



LOVELAND PLANNING COMMISSION MEETING

AGENDA

Monday, July 23, 2018

500 E. 3rd Street – Council Chambers

Loveland, CO 80537

6:30 PM

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LOVELAND PLANNING COMMISSIONERS: Carol Dowding (Chair), Pat McFall, Jamie Baker Roskie, Rob Molloy, Jeff Fleischer, Tim Hitchcock, Michael Bears, David Hammond, and Milo Hovland.

CALL TO ORDER

I. PLEDGE OF ALLEGIANCE

II. REPORTS:

a. Citizen Reports

This is time for citizens to address the Commission on matters not on the published agenda.

b. Current Planning Updates

1. Monday, July 30, 2018 - UDC Study Session at 6:00 pm at the DC

2. Monday, August 13, 2018 – Agenda Preview

i. Thornburg Hamilton Fourth Subdivision – PH – Preliminary Plat

3. Hot Topics:

- Survey responses requested
- Questions/comments about the UDC?

c. City Attorney's Office Updates:

d. Committee Reports

e. **Commission Comments**

III. APPROVAL OF MINUTES

Review and approval of the June 11, 2018 Meeting minutes

IV. REGULAR AGENDA:

1. Boyd Lake Place Addition – Public Hearing

This is a public hearing to consider requests for annexation and the establishment of Planned Unit Development (PUD) zoning for 27 acres of vacant land located along the west side of Boyd Lake Avenue to the south of Frank Road. The property is located to the west of High Plains School and west of the developing Lakes at Centerra community. Lakeview Holdings, LLC, is proposing the development of a 98-unit single-family residential community that will be marketed to an active adult demographic. The submitted Preliminary Development Plan (PDP) depicts detached, ranch-style homes located on individual lots that average 5,500 and would achieve a density of 3.6 units per acre. Designs include perimeter landscaping treatment and site amenities including walking trails, covered seating areas and a putting green.

Vehicular access to and from the site has been among the concerns expressed by area neighbors. Frank Road, which provides access to the existing Boyd Lake Shores neighborhood to the north, would provide full-movement access to the new development from Boyd Lake Avenue via the existing roundabout. Two additional right-in, right-out connections would also serve the project to the south of Frank Road. A forth access point is planned at the south end of the project site; however, future access to the Lost Creek Drive roundabout remains uncertain. City staff has determined that this forth access point is not necessary for the development to meet City requirements.

City development review staff is recommending approval of both the annexation and PUD zoning/Preliminary Development Plan. The site meets all requirements for annexation and the residential use and the proposed density aligns with Loveland's Comprehensive Plan. The role of the Planning Commission is to conduct a public hearing and forward its recommendations on to City Council for final action to be taken in a noticed public hearing.

V. ADJOURNMENT

SUPPLEMENTARY INFORMATION

Public Hearing Procedures

The purpose of a public hearing is for the Planning Commission (PC as used below) to obtain full information as to the matter under consideration. This includes giving all interested parties the opportunity to speak (provide testimony) at the hearing. The public hearing is a formal process. Below is the typical hearing sequence followed by the Planning Commission. *Annotations have been provided for clarity.*

1. **Agenda item is recognized and described by the Chair**
2. **Public hearing is opened***
3. **Staff presentation**
(Staff summarizes the application(s) and provides analysis based on established codes and standards. Clarifying questions to staff may be requested by Commissioners)
4. **Applicant presentation**
(The applicant is responsible for clearly describing their application(s) and justifying approval. Clarifying questions to applicant may be requested by Commissioners)
5. **Public comment**
(All public comment should be made from the podium upon the PC Chair acknowledging the citizen speaking. Citizens should provide their name and mailing address in writing at the podium, and introduce themselves. The PC may ask clarifying questions of the citizens. At a public hearing, the PC does not respond to questions from citizens; questions directed to the applicant or staff should be requested through the Chair.)
6. **Applicant response**
(The Chair typically requests that applicants respond to comments and questions raised during public comment)
7. **PC questions to staff, the applicant and possibly to citizens who presented**
(Commissioners may use this step in the process to gain a more detailed understanding of relevant information)
8. **Close public hearing**
(Unless specifically permitted by the Chair, further testimony is not allowed after the public hearing is closed)
9. **Motion**
(Motions are made by a PC member with possible conditions)
10. **Motion is seconded**
(A 2nd is required before the motion can be considered; a motion that fails to obtain a second dies)
11. **PC discussion**
(The PC discusses the application and whether it satisfies the required findings)
12. **PC Chair requests that the applicant agree to any conditions prior to a vote**
(If an applicant does not accept the proposed conditions, the PC may deny the application)
13. **Vote**
(The decisions of the PC must address relevant findings of fact. These findings are specified in adopted plans and codes, and serve to guide zoning and annexation decisions. Relevant findings are itemized in the Staff Report and referred to in the recommended motion.)

*** Note that the Planning Commission Chair may place time limits on presenters. All presenters should communicate clearly and concisely, refraining from duplicating detailed information that has been provided by others.**

CITY OF LOVELAND

PLANNING COMMISSION MINUTES

July 9, 2018

A meeting of the City of Loveland Planning Commission was held in the City Council Chambers on July 9, 2018 at 6:30 p.m. Members present: Chairwoman Dowding; and Commissioners Roskie, Fleischer, Molloy, McFall, Hitchcock, Bears, Hammond, and Hovland. Members absent: none. City Staff present: Bob Paulsen, Current Planning Manager; Laurie Stirman, Assistant City Attorney; Lisa Rye, Planning Commission Secretary.

These minutes are a general summary of the meeting. A complete video recording of the meeting is available for two years on the City's web site as follows: <https://loveland.viebit.com/>

CITIZEN REPORTS

There were no citizen reports.

CURRENT PLANNING UPDATES

1. **Robert Paulsen**, Current Planning Manager, noted that an upcoming Unified Development Code (UDC) study session is scheduled for Monday, July 16 at 6:00 pm at the Development Center. The third draft of the UDC will be printed and distributed to the Commissioners this week.
2. **Mr. Paulsen** reviewed the agenda item scheduled for the Monday, July 23, 2018 Planning Commission meeting. Boyd Lake Place Addition is an item on this agenda and will be a public hearing. It is anticipated that a large number of citizens will attend this meeting. It was asked that commissioners let staff know if they plan to be absent.
3. As a Hot Topic item, **Mr. Paulsen** introduced **Steve Adams**, City Manager, who spoke about the Boards and Commission Survey. At the request of City Council, **Mr. Adams** asked that all boards and commissions members complete the survey either electronically or by paper copy. A joint study session will be scheduled for a later date and will provide City Council and the Planning Commission the opportunity for a discussion regarding their roles. **Mr. Adams** assured the commissioners that the survey is confidential and results will be compiled and later shared, after all personal information is redacted.

CITY ATTORNEY'S OFFICE UPDATES

There was nothing to report from the City Attorney's office.

COMMITTEE REPORTS

There were no committee reports.

COMMISSIONER COMMENTS

There were no comments.

APPROVAL OF THE MINUTES

*Commissioner McFall made a motion to approve the **June 25, 2018** minutes; upon a second from Commissioner Bears, the minutes were unanimously approved. Commissioner Molloy abstained.*

CONSENT AGENDA

There were no items scheduled on the Consent Agenda

REGULAR AGENDA

1. New Vision Charter School – Location & Extent Review

Project Description: New Vision Charter School is seeking a new location for their elementary and middle school programs at 287 North Denver Avenue. The vacant, 10-acre site is located directly north of the new Larimer County office building that is being constructed at the corner of 1st Street and Denver. The phased 2-story project would house grades 3 through 8 at full build out and include typical site amenities such as parking; queuing, drop-off and pick-up facilities; athletic fields and landscaping. The first phase of construction would include a 55,000 square building with a planned 20,000 square foot future expansion.

The property is zoned I-Developing Industrial. Public schools are permitted by right in all zones. The review and permitting of public schools is primarily a function of the State of Colorado as specified in State Statutes. However, local planning commissions have statutory authority to review public school location and comment on their development plans. As the school will connect to City utilities and infrastructure, the school facility must conform to City engineering standards, including compliance with transportation requirements. Plans for the school facility are currently under review by the City. Staff believes that all pertinent requirements can be met and is supporting the project.

David Eisenbraun, Strategic Planning, presented the New Vision Charter School project and described the site, which is located north of the East 1st Street and North Denver Avenue intersection. The property is zoned industrial and according to the land-use map is designated as employment. The site plan shows two phases of development on 10 acres of vacant land. The first phase would consist of a 55,000 square feet area built-out with a two-story building, and the second phase would consist of a 20,000 square feet addition including a second playfield and additional drive isles.

Mr. Eisenbraun explained that according to State statute, the project must be presented to the Planning Commission for review. Following staff's review of a formal Site Development Plan

(SDP) and Public Improvement Construction Plans (PICPs), the applicant's intention is to break ground in the fall of 2018. Comments made by the Commission will be presented to the school district.

Mr. Tim Bishop, principal of New Vision Charter School, introduced himself. He explained the existing school is at full capacity and has been for several years. Several hundred prospective students are waitlisted and the school desires to increase enrollment. The school plans to keep the current campus location for grades K-2, and the new location would be for grades 3 - 8. The school currently has approximately 500 students enrolled, and with the prospective new campus location, the combined enrollment would be at approximately 900 students.

Mr. Ken Cooper, facilities director for Larimer County, asked for comments from staff or Mr. Bishop specific to parking and to the entering and exiting the school property by vehicles, along with description of any possible road improvements that are planned.

Ms. Roxanne Hayes, traffic engineer to the applicant, discussed the analysis of the traffic study performed for the project. She described that the greatest issue will be at North Denver Avenue and Opal Court. A southbound right-turn deceleration is required at this intersection, which New Vision Charter School has agreed to build since this area will be affected by greater traffic. There have been issues on East 1st Street with traffic congestion. The proposed new site will remove most of the existing stacking on E. 1st Street associated with the existing school facility, along with other interior subdivision roads. A variance request will be proposed, requesting the delay of construction at E. 1st St. and Peridot Avenue., to provide adequate time for a westbound right turn lane to be built.

Commissioner Molloy asked if a school zone will be added to the area of the school.

Mr. Randy Maizland, Transportation Development Review, explained that the signalized intersection at North Denver Avenue and East 1st Street will be the safest place for students to cross. The school will utilize crossing guards to encourage crossing at appropriate areas. School zones on arterial streets have a significant impact on the purpose of those roadways; however, once the school is opened and if a separate signalized crossing is recognized as a needed option, it will be considered at that time.

Commissioner Molloy questioned how school start times will impact traffic flow. **Mr. Bishop** answered that there will be staggered start times for families with students at each campus. He also mentioned that the school's current start time is 8:00 am and ends at 3:30 pm; a later end time will occur at the new campus.

Commissioner Hitchcock expressed concern with traffic, and he is not hearing answers that are addressing the problem. **Ms. Hayes** explained that since the prospective site is a much larger property, there is more room to offer queuing than at the present location, which will prevent cars from stacking on the roadways.

Commissioner Fleischer questioned if there is a designated left turn lane to get onto the site and if there are stacking capabilities. **Mr. Maizland** confirmed that there will be dedicated left turn

facilities on North Denver Ave. and on East 1st Street. A queuing analysis was performed with the traffic study and it was determined that there will be adequate space to accommodate incoming traffic.

Commissioner Hitchcock asked that with as busy as North Denver Avenue is, how will you exit the site heading north without a traffic light. **Ms. Hayes** explained that adding a signal is not a possibility. She explained there are several alternate routes people can use to exit the site, including exiting from Peridot Avenue.

Commissioner Fleischer asked if the traffic study covered the burden of the Larimer County office building. **Ms. Hayes** confirmed that it covered this new facility, along with the area new self storage facility and coffee shop. She added that the worst-case traffic scenario was used in conducting the study.

Commissioner Roskie stated that a school zone was approved for an arterial roadway near Wilson Avenue. She questioned what the conditions were in getting that approved and how could this apply to North Denver Avenue if it is determined a school zone is required. **Mr. Maizland** explained considerations are made case by case, and that evaluations will occur after the school is opened. He added that a condition has been implemented, which requires the school to install a school zone if directed by the city engineer.

Commissioner McFall asked if the current school will be closed at any time in the future. He also asked for clarification on the number attending each school. **Mr. Bishop** answered there are no plans at this time to close the original school building. It is predicted that 275-300 students will remain at the exiting school, and approximately 300 students will be attending the newly proposed school. Each year, the new location would gain more students, reaching a maximum of 900 students in several years.

Commissioner Dowding asked **Mr. Cooper** if his questions had been answered. He responded that the County opens their building at 7:30 am, so this will be a busy time. He shared his concern about the long-term plan that the school has to increase enrollment and asked that staff consider the impact. **Mr. Maizland** added that the analysis done was based on 600 students, and then again at the full build-out of 900 students. It was determined that the new site does provide enough on-site stacking and queueing for the full build-out. He does expect to see access points stressed and standards exceeded when the school reaches maximum capacity.

Commissioner McFall moved to communicate to the Thompson School District R2-J Board of Education that the City of Loveland Planning Commission has reviewed the proposed location and associated site development plan for New Vision Charter School and has determined that said plan is in compliance with City standards for public utilities, storm water conveyance and roadway connections related thereto, recommends that the site be used as depicted and described on said plan, with Thompson School District's agreement to the recommended conditions in Section IX of the Staff Report. **Commissioner Roskie** seconded the motion.

Mr. Tim Bishop, as a representative of New Vision Charter School, accepted the conditions as stated in Section IX.

The motion was adopted with 7 ayes and 1 nay.

ADJOURNMENT

Commissioner McFall made a motion to adjourn. Upon a second by Commissioner Roskie, the motion was unanimously adopted.

Commissioner Dowding adjourned the meeting 7:25 pm.

Approved by: _____
Carol Dowding, Planning Commission Chair

Lisa Rye, Planning Commission Secretary.

July 12, 2018
BOYD LAKE PLACE

- A. Purpose of the Project
- B. Site Introduction
- C. Site Design Concepts
- D. Circulation
- E. Public and Private Utilities
- F. Landscaping and Fencing
- G. Architecture and Site Design
- H. Implementation
- I. Regulatory Procedures

Application

Owner:

Firm: Lakeview Holdings, LLC
Name: Marcus Palkowitsh
Address: 720 S. Colorado Blvd., Suite 940N Denver, CO 80246
Phone: 303.399.9804
Email: marcus@msspcompanies.com

Applicant:

Firm: Lakeview Holdings, LLC
Name: Chad Rodriguez
Address: 720 S. Colorado Blvd., Suite 940N Denver, CO 80246
Phone: 303.399.9804
Email: chad@msspcompanies.com

Land Planner:

Firm: Kephart
Name: Chris Grady
Address: 2555 Walnut St., Denver, CO 80205
Phone: 303.832.4474
Email: chrisg@kephart.com

Engineer:

Firm: Northern Engineering
Name: Cody Snowden

Address: 301 North Howes St., Suite 100, Fort Collins, CO 80521

Phone: 970.568.5409

Email: cody@northernengineering.com

Landscape Architect:

Firm: Consilium Design, Inc.

Name: Craig Karn

Address: 7353 South Alton Way, Suite 135, Centennial, Colorado 80112

Phone: 303.224.9520

Email: clkarn@consiliumdesign.com

Boyd Lake Place Description

Site Address: Located South of Frank Road, West of Boyd Lake Avenue and East of Boyd Lake in Larimer County, Loveland, Colorado.

Property Size: 24.69 Acres

Existing Land Use Designation: Unincorporated Larimer County / Agriculture

Existing Zoning: FA, Farming

Proposed Zoning: PUD

Proposed Land Use Designation: Low Density Residential

BOYD LAKE PLACE PUD PRELIMINARY DEVELOPMENT PLAN (PDP) NARRATIVE AND TEXT

A. Purpose of Project

The purpose of this Preliminary Development Plan (PDP) submittal is to provide the necessary information to allow the City of Loveland staff, Planning Commission and City Council to review the Preliminary Development Plan and resolve any planning issues to permit the project and to then proceed through Final Development Plan approvals.

The purpose of this PDP is to establish the goals, guidelines, and overall design concepts for the annexation and development of 24.69 acres that will comprise Boyd Lake Place, a single-family age-targeted active adult residential community.

The City's Future Land Use Plan designates this property as a site for low-density residential (LDR) development. The Boyd Lake Place annexation/PUD/PDP request is consistent with the City's Comprehensive Plan's strategic policies, supporting strategies and zoning regulations. Accordingly, the City will benefit from having this parcel incorporated into the City for the following reasons:

- The project offers an alternative housing product, which is vital for the City to preserve its position as an attractive place to live and support its economic and development goals.
- The project provides attractive streetscapes and trails to improve connectivity to surrounding areas and regional amenities through the completion of the west side of Boyd Lake Avenue, including bike lanes and pedestrian walkways.
- The property will be developed in a manner that will protect Boyd Lake and provide ample open space for all residents in the community.
- A variety of community amenities, recreational spaces and close proximity to high-quality healthcare options will attract active adults who in turn will help support existing regional retail and businesses along Highway 34.

The application request does not interfere with, prevent, or infringe upon any of the area's existing, planned, previously committed services or proposals for community facilities, or other specific public or private actions contemplated within the City's Comprehensive Plan. This application request compliments and completes portions of the area's existing and planned transportation system services as contemplated by the City Transportation Plan.

- The development will provide infrastructure to make all modes of transportation—whether vehicular, biking or walking—convenient and viable for all ages, abilities, and income levels.
- The development will establish and maintain convenient connections between neighborhoods and to surrounding streets and destinations.
- The project will provide for the completion of the roadway, bike lanes and pedestrian walkway improvements along the west side of the existing Boyd Lake Avenue adjacent to this project.

B. Site Introduction

Boyd Lake Place is a very attractive property overlooking Boyd Lake and offering beautiful mountain views. The property is located north of Highway 34 on the west side of Boyd Lake Avenue.

The geographical location of the site is in Section 8, Township 5 North, Range 68 West, County of Larimer, Colorado. This project is located on 24.69 acres. The property is owned by Lakeview Holdings, LLC.

The subject parcel is located at the southwest corner of the Frank Road and Boyd Lake Avenue intersection, approximately a half mile north of US Highway 34. The parcel is bordered to the north by Boyd Lake Shores subdivision, a single-family residential community located within the Larimer County jurisdiction. To the property's west is a linear narrow tract of land lying east of the Greeley-Loveland Irrigation Ditch and runs parallel to the property boundary. Beyond the ditch to the west is existing open land and Boyd Lake. The property is bordered on the east by Boyd Lake Avenue along with the newly constructed High Plains School (K-8) and a two to three story, multi-family, medium-density residential (MDR) community. The project limit to the south is the roundabout at Lost Creek Drive.

The project's plan includes landscaping of open areas with regionally appropriate plant materials. Landscaping along Frank Road and Boyd Lake Avenue will follow the City of Loveland's current corridor plan for the area. In open space areas and buffer yards, a naturalistic design approach will be incorporated into the design. This area will incorporate low-water, drought-tolerant, formal and informal plantings of trees and shrub beds using a Hydrozone Plan (Hydrozone Plan) as defined in the City of Loveland Municipal Code chapter 19.06. Entry project monumentation will be located at the entrances of the project on Frank Road and Boyd Lake Avenue.

The Boyd Lake Place Homeowner's Association ("the HOA") shall maintain the open space area plantings, entry wall plantings, and landscape areas adjacent to all arterial streets. In addition, recreation areas will be constructed by the Developer, and maintained by the HOA in conformance with the City of Loveland's requirements.

C. Site Design Concepts

1. Overall Design Concept

The goal of this development is to create an active adult community offering compatible housing within an area of mixed-use development. The development concept is to provide a high quality, sensitively designed single-family residential community within the City of Loveland to help meet the current and future housing market demand. The project will consist of 98 single-family residential lots and will not include any commercial, retail, or office uses. These lots will be targeted to an active adult demographic. The community will offer low-maintenance active adult living at an attractive value.

The development introduces a new kind of healthy and livable adult community within the existing regional context. This community will provide a safe, secure, walkable environment, housing options, opportunities for recreation, and is near high-quality health facilities. Ample open space and a trail/sidewalk network within the development and along Boyd Lake Avenue provide residents with a variety of active and passive recreational options. This new community provides appropriately designed homes in a socially and environmentally sustainable community for adults who are already living in the Loveland area, or who may be moving here in the coming years.

The Boyd Lake Place land use plan is based on the belief that active adults no longer need or want to maintain a large house and back yard; active adults would prefer to travel, learn, recreate, socialize, pursue hobbies, and give back to the community. The community plan features lots with minimal maintenance, allowing residents more time to enjoy the nearby and on-site amenities provided.

Boyd Lake Place homes are designed and planned for aging in place with universal (accessible) design concepts. All homes offered are primarily a single-level ranch-style product with an optional second story walk-up attic or a standard two-level product with additional bedroom and living space. The product types offered range

in a wide variety of types and sizes. These housing options allow Loveland's active adult residents to continue living in their community, as well as accommodate residents new to the area.

Boyd Lake Place affords a rare opportunity to create a socially, environmentally and economically sustainable residential community. The project's development concept provides a high quality, sensitively designed residential community within the City of Loveland to help meet the current and future market demand for housing in this area. Annexing and zoning the property as a PUD will blend the parcel within the existing context and fulfill the Comprehensive Plan's strategic plan policies and supporting strategies.

2. Parcel Concepts

The total development area of the project submitted as a Preliminary Development Plan is 24.69 acres. The total developed land, including Right-of-Way (ROW) areas is 27.3. The overall proposed maximum gross density of the site is 3.6 dwelling units per acre and the maximum net density is 4.0 dwelling units per acre.

3. Land Use Categories and Definitions

The land use category in this Plan is for single-family dwelling units. Other ancillary or accessory uses relating to single-family residential units will be allowed, including but not limited to garages, mail kiosks, community facilities and related recreational amenities and passive open space. The City of Loveland Zoning Code definitions of R-1 shall be applied for this project description in addition to the following:

Single family dwelling: A one family dwelling not attached to any other dwelling, located on individual fee-simple lots or in clusters. These homes may be of conventional construction.

Building coverage: The area of land that a built structure covers.

Gross Area: That area which includes all the land within the boundary of annexation.

Net Area: That area of land within the boundary of annexation and excluding any rights-of-ways and easements.

Infrastructure: Shall mean those man-made structures which serve the common needs of the population such as, but not limited to, potable water systems, non-potable water systems for irrigation, wastewater disposal systems, solid waste

disposal sites or retention areas, storm drainage systems, utilities, bridges, roadways, bicycle paths, and trails.

Open Space: The gross area of a lot or tract of land minus all streets, driveways, parking areas, and habitable building areas which is to be or has been landscaped or developed for use by the public or by the residents of the lot, tract of land, or neighborhood for private, common, or public enjoyment or recreational use.

Recreational Space: Shall mean space that is designed for active or passive recreational use for more than three (3) families.

Allowable Land Uses	Single-Family Residential
Existing Zoning.....	Larimer County – FA (Farming)
Legal Property Area	24.69 acres
Gross Area of Annexation.....	27.3 acres
Maximum Number of Dwelling Units	98 units
Maximum Gross Density	3.6 DU/AC
Maximum Net Density	4.0 DU/AC
Open Space.....	24%
Height Limitations	26 ft. primary dwelling units

Permitted Use

- a. Agricultural. Open land for the raising of crops, plants or flowers.
- b. Residential. Single family detached dwelling.

Density/Intensity

Single-Family detached dwellings. Gross density shall not exceed four (4) dwelling units an acre or 98 residential lots.

Dimensional Standards

- a. Single-Family Detached Dwellings: Average lot size of five thousand five hundred (5,500) square feet but not less than four thousand five hundred (4,500) square feet.
- b. Typical Lot Dimension: Fifty (50) feet by one hundred ten (110) feet. Minimum lot width shall be thirty (30) feet at the right-of-way for an irregular shaped lot.
- c. Building Structure Height. No buildings or structures shall exceed a height of twenty-six (26) feet.

- d. Building Coverage. Maximum building coverage shall not exceed fifty (50) percent of the total lot area.

4. Neighborhood Structures

As shown on the PDP site plans, all proposed land uses within the PDP are for single family residential. Residential streets and open space will generally bind the residential neighborhoods. Landscaped setbacks and buffer zones will be established along Boyd Lake Avenue. The number of dwelling units will be allowed as shown on the Preliminary Development Plan (PDP) and will be designed in accordance with the development standards outlined in this plan.

5. Wetlands and Wildlife

The existing land use is agricultural. The topography is primarily flat, sloping gently to the west. No trees or shrub communities, streams, creeks, ponds, or seeps are within the property boundaries except for a small wetlands area in the northwest corner within the Greeley/Loveland Irrigation Company canal. The wetlands are limited to the extreme northwest corner of the property. The Environmentally Sensitive Areas Report found that “these wetlands occur as a narrow, intermittent fringe along the Greeley-Loveland Irrigation Company canal, and total less than 0.005 acre.” There are no plans to disturb these wetlands as part of the proposed development. The Environmental Assessment Report found that there are “no known special habitat characteristics present,” and the property has “limited wildlife habitat value.” An environmental assessment and wetland survey, containing further details for the property, has been prepared by Western Environmental Consultants, and has been approved by the City of Loveland. U.S. Fish and Wildlife Service has confirmed it has no concerns regarding Preble’s Meadow Jumping Mouse habitat.

D. Circulation

1. Overall Concept

a. Vehicular

The site will have two access points from Boyd Lake Avenue, and one access point from Frank Road on the northern boundary of the site. A potential future roundabout connection at Lost Creek Drive and Boyd Lake Avenue will be provided along the southern boundary of the site. The

neighborhood circulation will consist of public local internal streets and drives.

A summary of the proposed street improvements are as follows:

Street	Width FL-FL	Width Row	Classification	Curb	Sidewalk	Bike
Boyd Lake Avenue	Varies	120'	4 Lane Arterial	Vertical 2'-6"	6' Detached	7'
Frank Road	29'	50'	Residential Local	Roll-over 2'-6"	5' Detached (south side only)	N/A
Interior Roads	34'	50'	Residential Local	Roll-over 2'-6"	5' Detached	N/A

The street improvements are designed to facilitate vehicular, pedestrian and bicycle movements through the area. The existing Boyd Lake Avenue 4-lane roadway cross section varies in width. Boyd Lake Avenue is constructed as a 4-lane arterial street with a 120' full width Right-of-Way (ROW) section (60' half ROW) to replace the existing roadway from Frank Road to the southern boundary of the site. Coordination of this work effort will be completed as part of the project development process. This portion of Boyd Lake Avenue is proposed to consist of: four 12' wide travel lanes, a raised center median, the Larimer County Urban Area Street Standards required auxiliary lane(s), and 7' bicycle lanes on both sides. A 6' detached meandering sidewalk separated a minimum of 10' from the curb will provide for safe pedestrian and bicycle circulation. Frank Road is currently a Larimer County local street approximately 30' wide with a 2'- 6" wide valley pan on the north edge and no valley pan on the south edge. A 2'- 6" roll-over curb and a 5' detached sidewalk, will be added to the south side of the road. The total width of the roadway including curb and gutters will be 29'. This will facilitate driveways for residential lots which will front on to the street. The street section will provide for two 12' travel lanes with combined parking and bicycle lanes on both sides. All interior streets will be within a public right-of-way.

Pedestrian and Bicycle Paths

The residential neighborhood will provide an internal pedestrian circulation system in conjunction with the internal public streets, and a pedestrian walkway along the neighborhood's western boundary shall connect with the public walkway adjacent to Boyd Lake Avenue. The project provides attractive streetscapes with detached sidewalks and trails to improve connectivity to surrounding areas and regional amenities through the completion of the west side of Boyd Lake Avenue, including bike lanes and pedestrian walkways.

b. Public Transportation

No public transportation is planned to enter onto the site. Future bus stops may be adjacent to this site, as planned by the City transportation system.

E. Public and Private Utilities

1. Water and Sewer

Water and sewer service will be provided by the City of Loveland. The water service for this site will be provided by 8" and 16" diameter waterlines looped through the site connected to a 16" and 24" diameter waterline in Boyd Lake Avenue. The sanitary sewer for this site will be provided by 8" and 15" diameter sanitary sewer mains through the site connected to an existing 10" diameter sanitary sewer that is located at the northwest corner of the site, adjacent to the Greeley-Loveland Irrigation Canal.

2. Storm Drainage and Site Grading

This project is within the Boyd Lake Drainage Basin and the Outlet Mall Drainage Basin according to the 1987 City of Loveland Master Drainage Plan. Storm drainage for this site will channel to two separate detention ponds located adjacent to the western property line. These ponds will provide adequate detention for the 100-year storm levels with water quality included and will release at the two-year historic flows. Flows generating from some areas around the perimeter of the site will have to drain undetained and therefore will be kept to a minimum. Rooftop, yard, and common area surface flows will be directed towards adjacent streets or adjacent green space where it will be collected by either a flared end section or a sumped inlet. There will be one ultimate release point for the entire site at the northwest corner of the property where

the site will release runoff into the existing drainage lateral that runs north to south along the western property line.

3. Electric

Electric service will be provided by the City of Loveland Light and Power Department. Easements will be provided for the installation of underground electric, gas and other utility services within the project. One light fixture will be installed at the intersection of Frank Road and will include a full cut-off fixture to direct all light in a downward direction.

4. Gas

Natural gas will be provided by Xcel Energy Company. Easements will be provided for the installation of underground electric, gas and other utility services within the project.

5. Telephone

Telephone service will be provided by CenturyLink and other providers as available.

6. Cable

Cable services will be provided by Comcast and other providers as available.

7. Police and Fire

The development will be serviced by the Loveland Fire and Police Departments with service areas as determined to be appropriate. Street widths are designed to accommodate emergency vehicles. The Boyd Lake Place street system is designed to provide interconnectivity to the adjacent streets in the existing subdivisions. This interconnectivity of streets is important to the Loveland Fire and Police Departments' ability to provide quick response times within the area.

8. Schools

The development will be serviced by the Thompson R2-J School District, specifically the following schools:

Mountain View High School
High Plains School K-8

9. Amenity Areas

The development does not include any public park facilities. Open space and recreation areas will be provided for residents of the Boyd Lake Place community at a minimum ratio of one acre for every 100 dwelling units. A community trail system will loop the entire development connecting it with the passive and active recreation areas throughout the Boyd Lake Place development.

Amenity areas include:

- A primary gathering space with covered seating and views to Boyd Lake and the Rocky Mountains, bocce ball lawn, horseshoes pit, and open space. The primary gathering space will have viewing scopes for birdwatching, informative signage, and grills.
- A secondary gathering space with a putting green and open space. The secondary gathering space will include a large seating area.
- A respite with seating and landscaping
- Picnic tables with charcoal grills and trash receptacles
- A cantilever shelter for school kids and the crossing guard
- A lean-to shelter within open lawn and shade trees.
- Doggie pots throughout the site for convenience and to promote a clean environment.
- Open space areas can be used for a variety of activities such as:
 - o Badminton
 - o Tai Chi
 - o Volleyball
 - o Frisbee
 - o Catch
 - o Picnics
 - o Birthday parties and celebrations
 - o Dog walking

- Reading a book
- Tanning or relaxing
- Visiting with friends/family/neighbors
- Cricket
- Football
- Soccer
- Enjoying the views, observing wildlife
- Flying a kite
- Bird Watching
- Sketching/painting
- Photography
- Arts and Crafts, and much more.

F. Landscaping and Fencing

1. Overall Concept

The landscaping will be designed with regionally appropriate low-water, drought tolerant plant materials using a Hydrozone Plan. In open space and buffer yards, a naturalistic approach will be incorporated into the design. These areas will incorporate informal plantings of trees and shrub beds. The open space area plantings and entry wall plantings shall be maintained by the HOA. Stone masonry or stucco entry features will be located at the entrance to Boyd Lake Residences on Boyd Lake Avenue. Other site items such as mailboxes and street markers will adhere to a similar design theme as the entry features.

The Boyd Lake Place landscape plan is based on lake and mountain view living, connection and visual unity throughout the project. Fencing will not detract from the overall design, but rather will be used as a design element where desired, and as a screen for any element which would detract from the overall image of the project.

The landscape design intent for Boyd Lake Place takes inspiration from regionally appropriate plant materials. The plant palette and implementation will include uplands prairie plants. There are unobstructed views to the west over Boyd Lake within the development allowing for open vistas. Landscaping adjacent to roadways will be less naturalistic, and more formal with pine and shade trees, and groupings of shrub masses. The main entry into Boyd Lake Place will include entry signs, accompanied with ornamental and evergreen trees, and plantings of annuals, perennials and ornamental grasses. Natural areas to be reestablished will receive seed mixes and be allowed to naturalize again. Areas designated for recreation and adjacent to roadways will have a higher level of maintenance, using trees, shrubs, perennials, mulches, and grasses with in-ground automated Hydrozone irrigation system.

The Developer will install all common area landscaping and perimeter and common area fencing per the approved Landscape Plan for Boyd Lake Place.

2. Edge Treatment

The edge treatment of the project shall be designed to be in conformance with the required buffers and landscape design as per the City of Loveland Site Development Performance Standards. Special attention will be given to the area adjacent to Boyd Lake Avenue. Fencing with landscaping along Boyd Lake Avenue will be used to buffer the development as well as enhance the visual appearance along this roadway.

3. Open Space Edge Treatments

The edge treatment and open space areas adjacent to lots will be treated as an enhanced native landscape. Native tree, shrub, grass and forb species will be emphasized in the western open space areas adjacent to Boyd Lake, to reduce water usage and improve habitat for wildlife. The planting will be an uplands prairie plant palette. The areas of open space along Boyd Lake within the development allow for open vistas, and the opportunities to provide recreational areas. Throughout the landscaped areas some additional tree planting will be supplemented with the native shrub plantings to provide shade, habitat, and visual appeal to the open areas. The edges of the area adjacent to the west boundary will have a two-rail vinyl fence. Fences will be restricted to a height of 3'-0" and will be regulated by Final Development Plan and HOA design guidelines.

4. Buffer Yards/Arterial and Collector Streetscape

A Type D buffer yard will be provided along Boyd Lake Avenue. This buffer yard will be a minimum of twenty feet (20') deep from the right-of-way. Lots which back to Boyd Lake Avenue will have a six-foot-high vinyl privacy fence and stone

columns along the rear property line of the adjacent lot. The buffer yard and right-of-way will be landscaped using shrub massing, grasses, and a variety of trees. The buffer yard will include naturalistic shrub massing which will provide seasonal color and interest to the landscape. Evergreen trees will be located throughout the grass areas, and flowering trees will accompany the shrub areas. The plant material densities will conform to the buffer yard requirements as outlined in the Site Development Standards.

5. Installation Schedule

The tree lawn landscaping for all single-family lots shall be installed by the developer. The landscaping for the lots shall have a minimum of one (1) tree spaced no more than 35 feet apart for interior lots located in the front yard and two (2) street trees for corner lots located in the front and street-side yards. Use of native plant material is encouraged. The landscaping requirement on each lot shall include the right-of-way and easements located on or adjacent to the lot which shall be maintained by the homeowner, subject to the right of the HOA to enter and perform any maintenance deemed necessary or desirable to maintain the high HOA standards established for Boyd Lake Place. Prior to the issuance of the first building permit, all common area landscaping, landscaping in the right-of-way and tree lawns, fencing, buffer yards, and amenities shall be either installed or financially secured with the city.

When reviewing landscaping and fencing plans, the HOA shall at a minimum adhere to the City of Loveland requirements and use the Final Development Plan and the Landscape and Fencing Plan approved by the City. Individual lot fencing is optional at the discretion of the lot owner. The FDP will contain a matrix detailing allowed fencing within the development.

6. Maintenance

The Boyd Lake Place Home Owners Association (HOA) shall be responsible for the maintenance and long-term care of the landscaping, entry features, perimeter fencing, common area fencing and amenities. Individual lot landscaping and fencing shall be installed and maintained by the individual lot owner. All tracts as shown on the approved Final Plat shall be owned and maintained by the HOA.

G. Architecture and Site Design

1. Overall Concept

The architecture of the project shall be designed as a cohesive neighborhood using a range of materials which will be chosen and implemented in such a way as to render the development a recognizable identity. The various home designs shall not compete for attention, but rather blend to create a sense of community. Consistency in design shall support a smooth transition between the proposed uses on the subject property and between those uses of adjacent properties. To help assure the consistency and adherence to guidelines of the PDP, the HOA shall review each submittal and provide a letter of approval before application to the City of Loveland Building Department.

No building or other structures shall be erected, altered, placed, or permitted to remain on any lot other than one (1) single-family residence per lot, with an attached garage. Exterior design for houses shall place equal importance for architectural features, exterior adornments and special visual treatments on both the front and any other side or rear of house that is facing Frank Road, Boyd Lake Avenue or any public street in the development. Please refer to sheets 13, 14, & 15 depicting proposed building elevations.

2. Architectural Character

Residential units offered are primarily a single-level ranch-style product with an optional second story walk-up attic or a standard two-level product with additional bedroom and living space. Multiple unit plans with a variety of elevations will be designed to provide a diversity of housing style options to residents, as well as a range in design, massing, and aesthetics desired by the City of Loveland.

The overall architectural character will be based on a traditional home style, with similarities to the neighboring single-family homes. The materials and colors to be used on the homes will incorporate pitched and gable roofs, varied building materials, and variations in textures, characteristically found in the Rocky Mountain region. The gabled and hipped roofing will incorporate an overhanging eave and/or other architectural feature and shall be a consistent type and color of roofing material. The structures shall incorporate a variety of colors and textures, using different material types, and emphasizing detail components. The palette of colors will be appropriate to a traditional neighborhood, using earth tones, and incorporating materials such as lap siding, hardboard shingle siding, manufactured stone siding, stucco, and asphalt composite shingles. Appurtenances such as eaves, overhangs, bay windows, fireplaces, pot shelves, and similar elements may extend into the setbacks up to two feet. The overall architectural concept will emphasize the following elements:

- a. Building form and character shall be of an appropriate scale. Design considerations shall include the mass, bulk, and proportion of the structure including roofs, walls and openings.
- b. All elevations of structures will be detailed to provide visual interest and avoid unattractive views.
- c. Roofing materials, windows, buildings materials and finish will be coordinated to achieve a cohesive appearance.

The PDP submittal for this area includes information demonstrating detailed layouts of the proposed area, including street layouts, landscaping, building elevations, and site amenities. Architectural design guidelines are also established within the PDP.

3. General Notes

All property depicted on the PDP shall be governed by the requirements, restrictions and standards of the City of Loveland Municipal Code, unless otherwise set forth on this plan. Residents shall at all times comply with the requirements, restrictions and prohibitions set forth in the PDP. Residences shall have exterior elevations, roofs, and details that are coordinated and consistent in their architectural treatment. At least three different types of housing models shall be provided. Each housing model shall have at least three characteristics which clearly and obviously distinguish it from the other housing models, which characteristics may include, without limitation, differences in exterior materials, changes in roof lines, garage placement, placement of the footprint on the lot, and/or building fenestration.

The following anti-monotony standard applies to “proximate lots,” which are lots that adjoin or share side property lines, or lots that, although separated by a street, face each other such that the extension of side lot lines across the street creates an overlap along the front line of more than 30 percent.

- a. *General Standards.* No two buildings of similar front elevation shall be constructed or located on more than three proximate lots. Front elevations shall be deemed to be similar if there is no substantial difference in the:
 1. Roof lines;
 2. Fenestration; and
 3. Color or type of cladding materials.

- b. *Roof Lines.* The following differences in roof lines that are visible on the front building elevation are sufficient to meet the requirements above:
 1. Changing gable roofs to hip roofs or vice-versa;
 2. Providing an intersecting roof ridge or dormer on the main roof, provided that the height of the intersecting roof or dormer is at least 50 percent of the height of the main room;
 3. Rotating gable roof systems 90 degrees; or
 4. Other comparable changes that are approved by the Director.
- c. *Fenestration.* The following differences in the size, location, or type of windows that are visible on the front building elevation are sufficient to meet the requirements above:
 1. Changing from single windows to a multiple window arrangement or vice-versa;
 2. Changing the types of windows (e.g., casement to double hung); or
 3. In the event that because of its size, location or design, one window is the predominant window on the front building elevation, if the size, location, or type of that window is changed, then no other window need be changed.
- d. *Cladding and Color.* The following differences in the size, location, or type of windows that are visible on the front building elevation are sufficient to meet the requirements above
 1. *Changes in cladding materials throughout the façade or elevation for a minimum of one story in height.*
 2. *Changes in color (rather than merely shade).*

4. Typical Residential Neighborhood Architectural Styles

See sheets 13, 14, & 15 for illustrations on proposed single-family architectural styles.

5. Setbacks

Single-family building setbacks will be regulated as follows:

Building Setbacks	Front* (Building)	Front** (Garage)	Side	Rear
-------------------	----------------------	---------------------	------	------

	12 ft.	20 ft.	5 ft. (internal lot line) 15 ft. (from street)	12 ft.
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*Measured from the back of sidewalk or property line, whichever produces a greater setback

**Measured from garage door to back of sidewalk

6. Height Limits

No building shall exceed 26' measured in accordance with the Loveland Municipal Code.

7. Parking

The number, size and design of parking spaces and lots shall correspond to the requirements of the City of Loveland Municipal Code and the Site Development Performance Standards and Guidelines. No on-street parking will be permitted on an arterial street. Parking is permitted on residential local streets as provided within the fifty-foot right-of-way.

8. Garages

Garages within Boyd Lake Place shall abide by the following standards below:

- On at least 75 percent of the dwelling units on a block face, garage doors that face the street shall be recessed a minimum of 1 ½ feet behind either the living portion of the house or from the front of a covered porch measuring at least six feet in depth by eight feet in length.
- No more than 25% of the dwelling units on a block face shall have garage doors that face the street that extend in front of the living portion of the house or a covered porch. Garages meeting this standard shall comply with the following standards:
 - The maximum extension shall be 2 feet in front of the living portion of the house or a covered porch; and
 - No two homes with garage doors facing the street that extend in front of the living portion of the house or a covered porch shall be constructed on adjacent lots that adjoin or share side property lines.
- Garage doors that are adjacent to the street but that have side-facing garage doors (perpendicular to the street) shall be clad with the same predominant

materials, architectural detail, and windows that mimic the features of the living portion of the dwelling on the side of the garage facing the street.

- Garage door openings shall not comprise more than 47 percent of the front yard building facade.
- All garages shall be attached to the dwelling unit. No detached garages or detached accessory structures shall be permitted within the PUD.
- Alternate overhead garage door treatments may be approved by the Director if the proposed design substantially meets the intent of these standards to line streets with active living spaces, create pedestrian-oriented streetscapes, and provide variety and visual interest in the exterior design of residential buildings.

9. Windows

Wood or vinyl windows shall be required. No metal windows shall be permitted.

10. Roof

The roof of each single-family residence shall have a minimum 6/12 pitch and a minimum overhang of twelve (12) inches at every point around the roofline of the residence. The roof shall be constructed of asphalt, composition, clay or concrete tile, slate, or other roofing materials permitted by the City of Loveland.

11. Front Siding

A minimum of 25 percent for any single-family house walls facing the street, including the doors and windows, but excluding the roof areas, shall be masonry, such as brick, stone, artificial stone or stucco, as allowed by the HOA. Outside corners of street-facing elevations shall include a minimum of four feet (4') full wall height wrap with similar masonry material, inclusive of garage walls or the same total square footage of wrap in the form of a wainscoting with similar masonry material, inclusive of garage walls.

12. Siding Other than Masonry

When wood or Masonite or other types of approved lap siding is used on that part of the single-family residence not required to be sided with masonry, then siding which shall be in a horizontal pattern with a maximum width of eight (8) inches with no more than a seven (7) inch exposure shall be approved.

H. Implementation

1. Overall Concept

The development of the project is scheduled to commence in 2018 and be completed by the end of 2021. Construction of the project will be done in one phase.

2. Estimate of Build Out Time Period

Construction and sales are projected to commence in 2019 and be completed by the end of 2021.

I. Regulatory Procedures

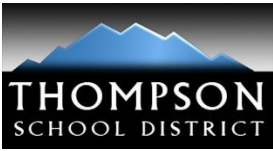
All property depicted on this plan shall be governed by the requirements, restrictions, and standards of the Boyd Lake Place annexation agreement, PUD, and PDP. All residential areas shall be governed by the provisions of Chapter 18.12 of the Loveland Municipal Code, as amended, unless specifically set forth in the Boyd Lake Place annexation agreement, PUD, and PDP.

Developers, contractors, property owners and residents shall comply at all times with the requirements, restrictions and prohibitions set forth in the annexation agreement, the PUD, and the PDP in this narrative. The owner of a property shall maintain the elevation, grading, and drainage plan approved by the governmental authority and the utility providers having jurisdiction. The owner of a property shall not impede or hinder in any way the water falling on the property from reaching the drainage courses established for the property and the common areas. The site elevation cannot be lower than shown on the approved construction plans without approval of the City of Loveland. Prior to the issuance of a Certificate of Occupancy from the City of Loveland, the owner shall deliver to the City a certificate from the owner's professional engineer certifying that the grading for the residence, as constructed and in place, complies in all material respects with all approved drainage plans.

The storm run-off water quality treatment ponds depicted on the approved drainage plans for this development shall be governed and monitored by the property owner in conjunction with the cooperative monitoring by the Greeley-Loveland Irrigation

Company and shall be subject to all approved constructions plans, rules, and regulations of the City of Loveland.

The Boyd Lake Place PDP is designed as a single-family neighborhood to provide contiguity to the surrounding neighborhoods to allow variations in building types and styles and to provide consistency in design throughout the development. A reference shall be included in the covenants within the PDP stating that said parcel is subject to zoning regulations established in the Boyd Lake Place PUD.



800 South Taft Avenue • Loveland, CO 80537 • Office (970) 613-5017

June 15, 2018

Referral Comments

Boyd Lake Place (25 May 2018)

Pursuant to Colorado Revised Statutes, the Thompson R2-J School District submits comments describing projected impacts of proposed developments submitted by statutory authority. Comments reflect information and projections available at time of referral.

Project Overview

The above-named project will consist of 99 single-family homes, located within the attendance boundaries of High Plains School and Mountain View High School, and within the city limits of Loveland, Colorado. Construction is expected to commence within 12-18 months, with buildout anticipated to occur approximately 5 years later.

Impact Overview

Resident populations are approaching capacity at the elementary level, and within capacity at secondary levels. The District anticipates that the overall impact from this project, when viewed from a feeder-system perspective, will be minimal. Given the type of product and the target market, the normal yield ratios will very likely not be attained.

Elementary Impact

At build out, this development is projected to produce a growth in enrollment of up to approximately 19 students over the six elementary grades, based on current yield ratios, without taking into account the product and target market. At current resident student population levels, this increase will bring elementary population closer to capacity. As this development meets the walk distance criteria as determined by the board of education, transportation will not be provided.

Middle School Impact

At build out, this development is projected to produce a growth in enrollment of up to approximately 12 students over the three middle school grades. At current resident student population levels, these additional students will have a minimal impact on capacity in the feeder system. As this development meets the walk distance criteria as determined by the board of education, transportation will not be provided.

High School Impact

At build out, this development is projected to produce a growth in enrollment of up to approximately 16 students over the four high school grades. At current resident student population levels, these additional students will not affect capacity in the feeder system. As this development meets the walk distance criteria as determined by the board of education, transportation will not be provided.

Additional Comments

It is important to remember that this development is a very small project, given the amount of development planned/approved in the feeder system boundaries. Additionally, the product and target market are not conducive to meeting average yield ratios, tending to make projections on the high side.

The District's citizen-based Master Plan Committee (MPC) monitors capacity levels at each of the district schools and makes annual recommendations for responding to projected enrollment changes. The selection and timing of the most appropriate tool(s) will be determined by the Board of Education.

The size and location of this development indicate that the school district land dedication requirement should be satisfied in the form of payment in lieu of land dedication (PILO) as per our intergovernmental agreement.

Boyd Lake Place
Southwest Quadrant of Boyd Lake Avenue/Frank Road
Loveland, CO

Traffic Impact Study

KE Job #2018-036

Prepared for:

Lakeview Holdings, LLC
720 S. Colorado Blvd., Suite 940-N
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June 12, 2018

Sean K. Kellar, PE, PTOE

This document, together with the concepts and recommendations presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization from Kellar Engineering LLC shall be without liability to Kellar Engineering LLC.

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1.0 Introduction

This Traffic Impact Study (TIS) is for the proposed Boyd Lake Place project located at the southwest quadrant of Boyd Lake Avenue and Frank Road in Loveland, CO. The purpose of this TIS is to identify project traffic generation characteristics, to identify potential traffic related impacts on the adjacent street system, and to develop mitigation measures required for identified traffic impacts.

Kellar Engineering LLC (KE) has prepared the TIS to document the results of anticipated traffic conditions in accordance with the City of Loveland's requirements and to identify any projected impacts to the transportation system. The project is anticipated to generate approximately 942 total average daily weekday trips, 75 total AM peak hour trips, and 99 total PM peak hour trips. See Table 1: Trip Generation.

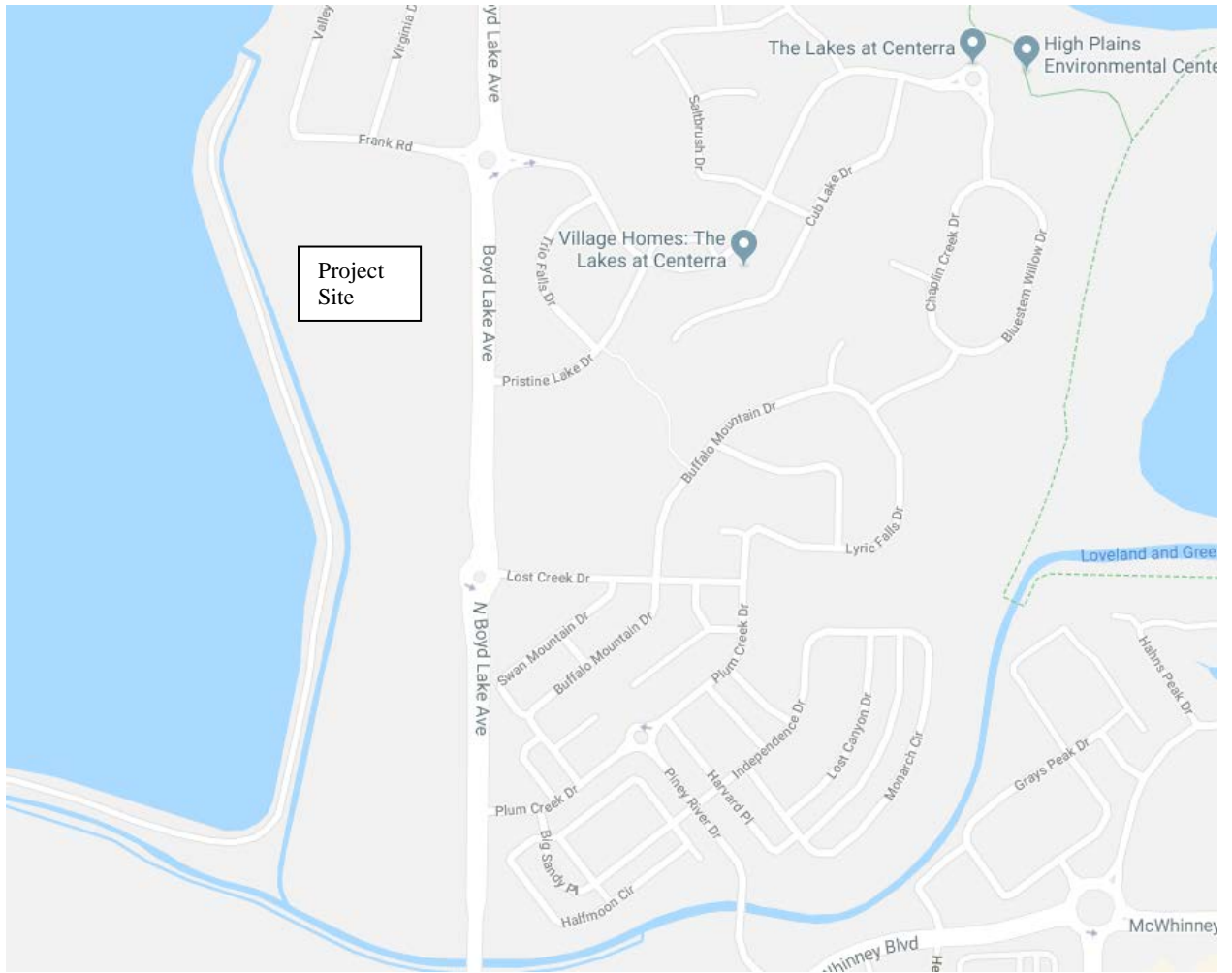
2.0 Existing Conditions and Roadway Network

The project site is located at the southwest quadrant of Boyd Lake Place and Frank Road in Loveland, CO. Boyd Lake Avenue is an existing north/south 4-lane arterial roadway with a posted speed of 35 mph adjacent to the project site. Boyd Lake Avenue has a raised center median, 4 travel lanes, and bike lanes adjacent to the project site. A detached sidewalk also exists along the east side of Boyd Lake Avenue adjacent to the project site. Multi-lane roundabouts have been constructed at the intersections of Boyd Lake Avenue/Frank Road and Boyd Lake Avenue/Lost Creek Drive. Frank Road is an existing east/west local residential street with a posted speed of 25 mph adjacent to the project site.

2.1 Recent Traffic Volumes

Recent peak hour traffic volume counts were conducted using data collection video cameras. The traffic counts were conducted in 15-minute intervals on Tuesday, 3/6/18 from 7:00 AM to 9:00 AM and 4:00 PM to 6:00 PM. The traffic counts are shown in Figure 3 with the count sheets provided in Appendix A.

Figure 1: Vicinity Map



Google Maps

Figure 2: Site Plan



Illustrative Site Plan

BOYD LAKE PLACE
LOVELAND, CO



KA# 215026
0 60 120

Lake View Holdings LLC

5.08.2018



3.0 Pedestrian Facilities

Currently bike lanes exist along Boyd Lake Avenue adjacent to the project site. A detached sidewalk also exists along the east side of Boyd Lake Avenue adjacent to the project site. The proposed project will be responsible for designing and constructing the adjacent curb, gutter, and sidewalk facilities along Boyd Lake Avenue. These bike lane and sidewalk facilities will provide adequate and safe pedestrian and bicycle facilities.

4.0 Proposed Development

The proposed project consists of approximately 99 single family homes. See Table 1: Trip Generation and Figure 2: Site Plan.

4.1 Trip Generation

Site generated traffic estimates are determined through a process known as trip generation. Rates and equations are applied to the proposed land use to estimate traffic generated by the development during a specific time interval. The acknowledged source for trip generation rates is the *Trip Generation Report* published by the Institute of Transportation Engineers (ITE). ITE has established trip generation rates in nationwide studies of similar land uses. For this study, KE used the *ITE 9th Edition Trip Generation Report* average trip rates. The proposed project is anticipated to generate approximately 942 total average daily weekday trips, 75 total AM peak hour trips, and 99 total PM peak hour trips. See Table 1: Trip Generation.

4.2 Trip Distribution

Distribution of site traffic on the street system was based on the area street system characteristics, existing traffic patterns and volumes, anticipated surrounding development areas, and the proposed access system for the project. The directional distribution of traffic is a means to quantify the percentage of site generated traffic that approaches the site from a given direction and departs the site back to the original source. Figure 5 illustrates the trip distribution used for the project's analysis.

4.3 Traffic Assignment

Traffic assignment was obtained by applying the trip distributions to the estimated trip generation of the development. Figure 6 shows the site generated peak hour traffic assignment.

4.4 Short Range Total Peak Hour Traffic

Site generated peak hour traffic volumes were added to the background traffic volumes to represent the estimated traffic conditions for the short range 2021 horizon. These background (2021) and short range (2021) total traffic volumes are shown in Figure 4 and Figure 7 respectively. The short range analysis year 2021 includes the proposed full development for the project plus background traffic (Year 2021 + Phase 1 Site Peak Hour Traffic Volumes from the City approved *The Lakes at Centerra North Project Revised Traffic Impact Study* – Millennium NW 11th & 12th Subdivision dated November 13, 2017 prepared by John Britton, P.E. with Permontes Group; growth rates from the NFRMPO (North Front Range Metropolitan Planning Organization).

5.0 Traffic Operation Analysis

KE's analysis of traffic operations in the site vicinity was conducted to determine the capacity at the identified intersection. The acknowledged source for determining overall capacity is the 2010 Edition of the Highway Capacity Manual.

5.1 Analysis Methodology

Capacity analysis results are listed in terms of level of service (LOS). LOS is a qualitative term describing operating conditions a driver will experience while traveling on a particular street or highway during a specific time interval. LOS ranges from an A (very little delay) to an F (long delays). A description of the level of service (LOS) for signalized and unsignalized intersections from the 2010 Highway Capacity Manual are provided in Appendix D.

5.2 Intersection Operational Analysis

Operational analysis was performed for the short range 2021 horizon. The calculations for this analysis are provided in Appendix F. Table 5 shows the short range total traffic LOS results. See Table 5: 2021 Short Range Total Peak Hour Operation.

5.3 Adequate Community Facilities (ACF) Ordinance Criteria Link Volumes

The 2021 short range total peak hour link volumes were compared with the ACF Traffic Thresholds in Table 2 to verify that the streets within the project area meet the link volume criteria in the Adequate Community Facilities (ACF) Ordinance. As shown in Table 2, the street links will meet the ACF Ordinance criteria for year 2021 total traffic.

Table 1: Trip Generation (ITE Trip Generation, 9th Edition)

ITE Code	Land Use	Size	Average Daily Trips		AM Peak Hour Trips						PM Peak Hour Trips					
			Rate	Total	Rate	% In	In	% Out	Out	Total	Rate	% In	In	% Out	Out	Total
210	Single Family Homes	99 DU	9.52	942	0.75	25%	19	75%	56	75	1.00	63%	62	37%	37	99
Total				942			19		56	75			62		37	99

DU = Dwelling Units

Figure 3: Recent Peak Hour Traffic

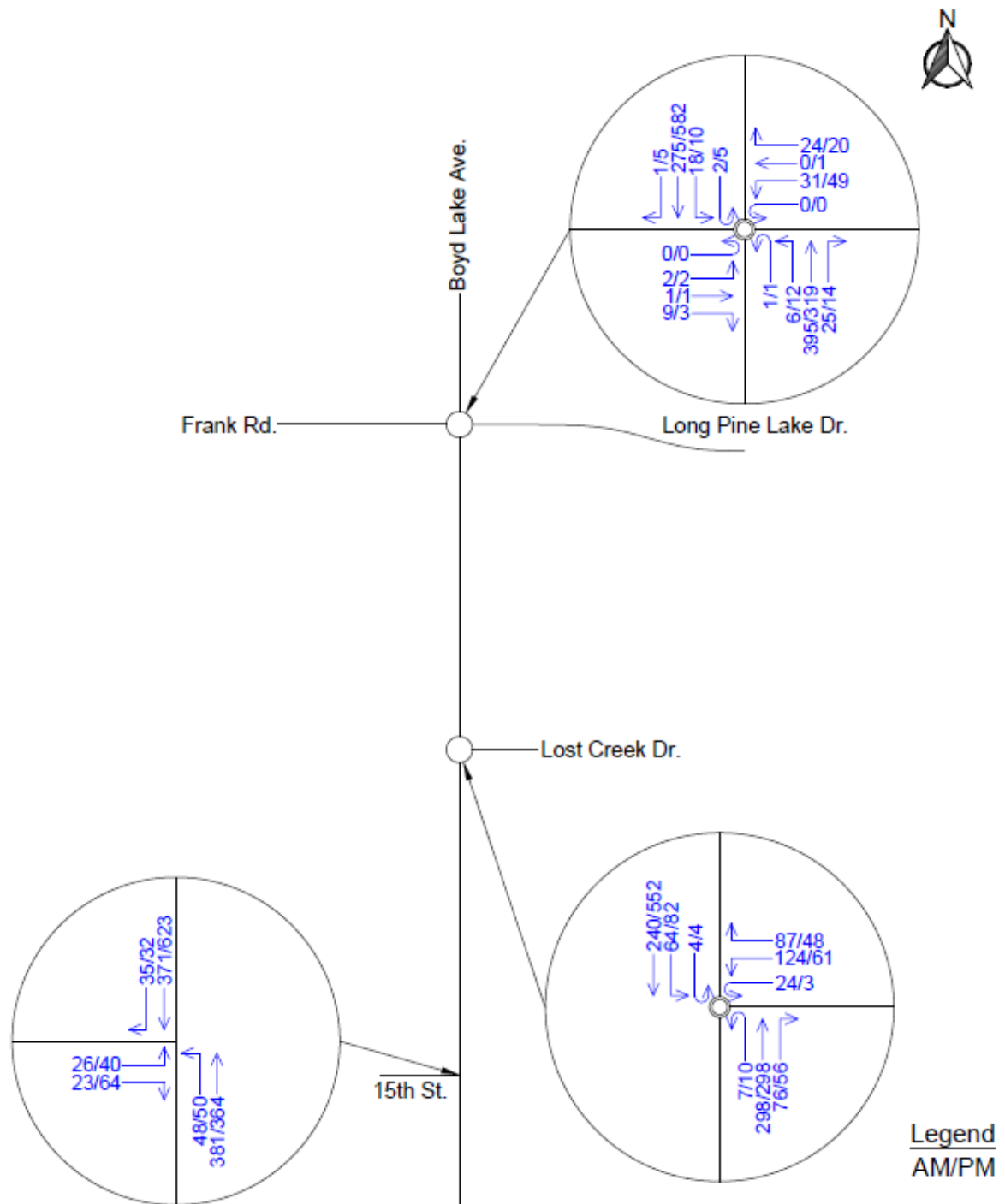


Figure 4: 2021 Background Traffic

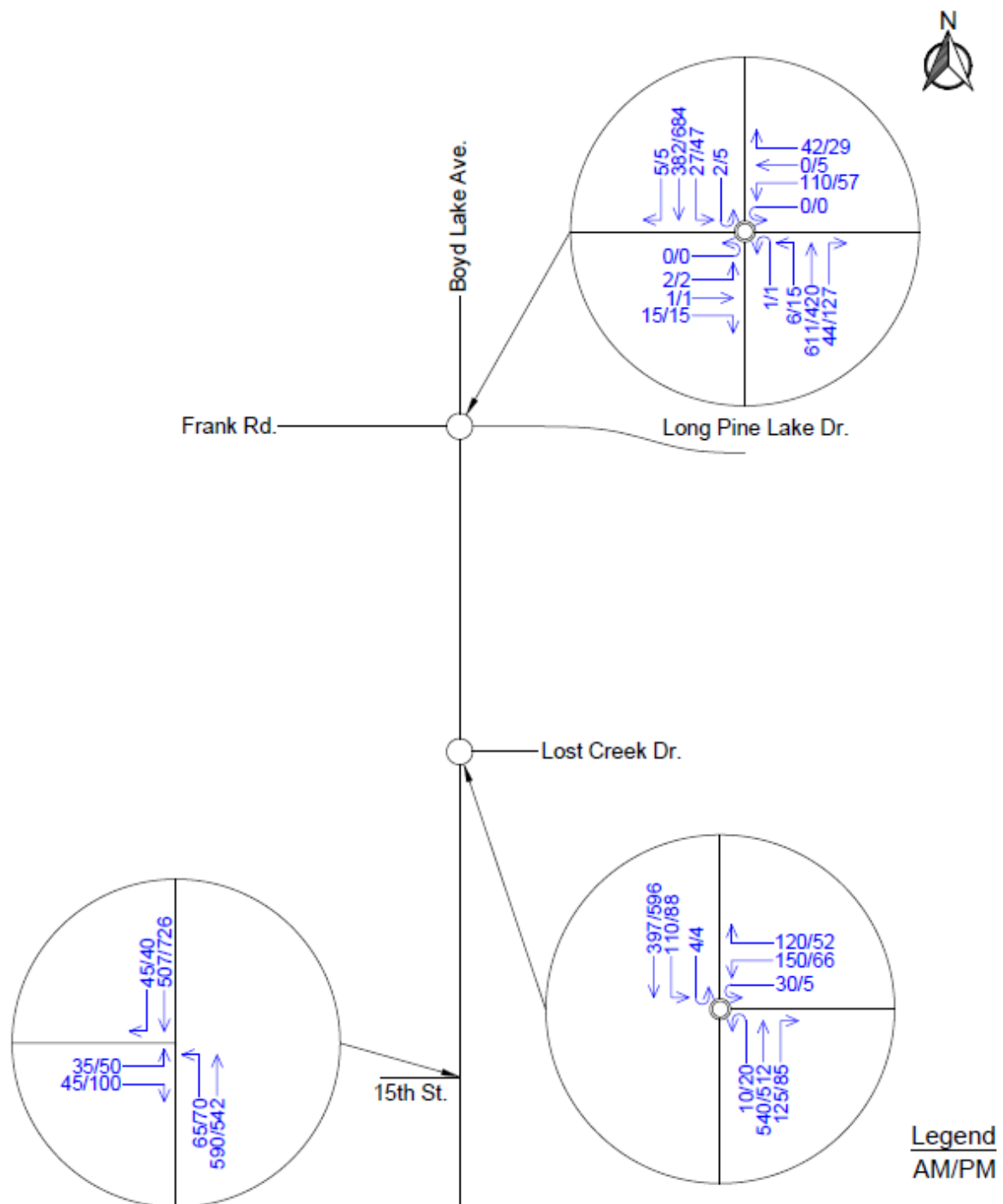


Figure 5: Trip Distribution

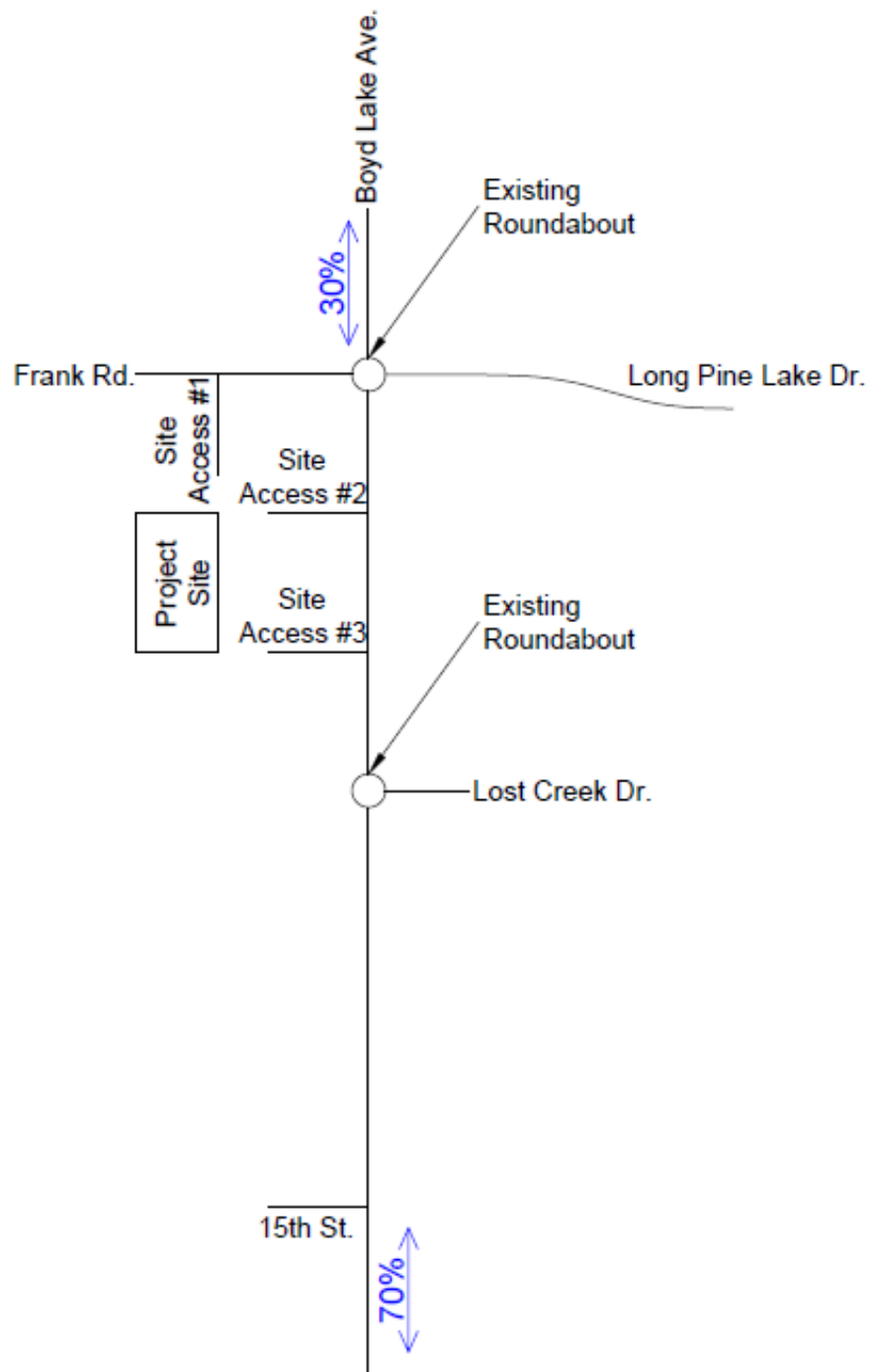


Figure 6: Site Generated Traffic

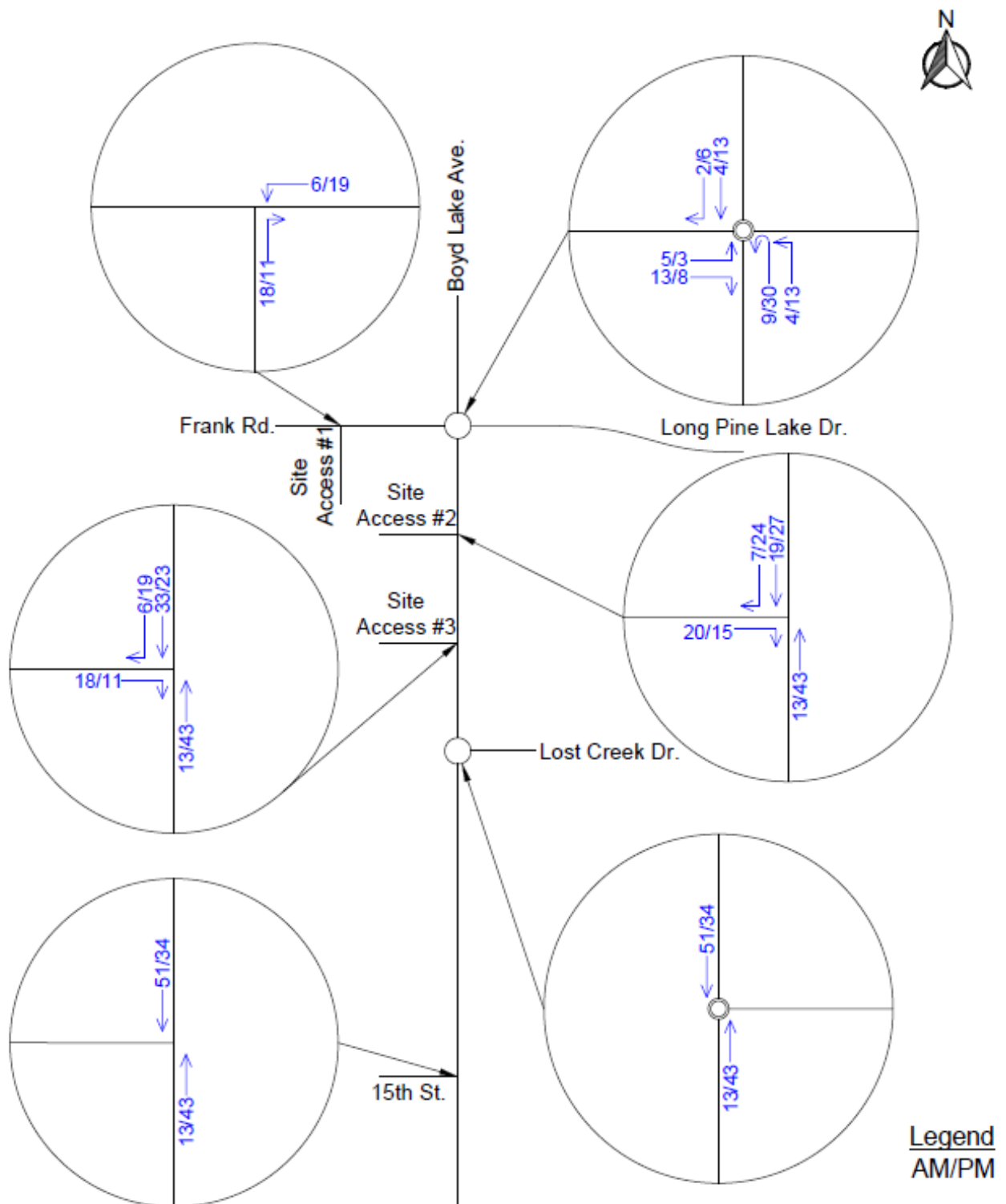


Figure 7: 2021 Short Range Total Traffic

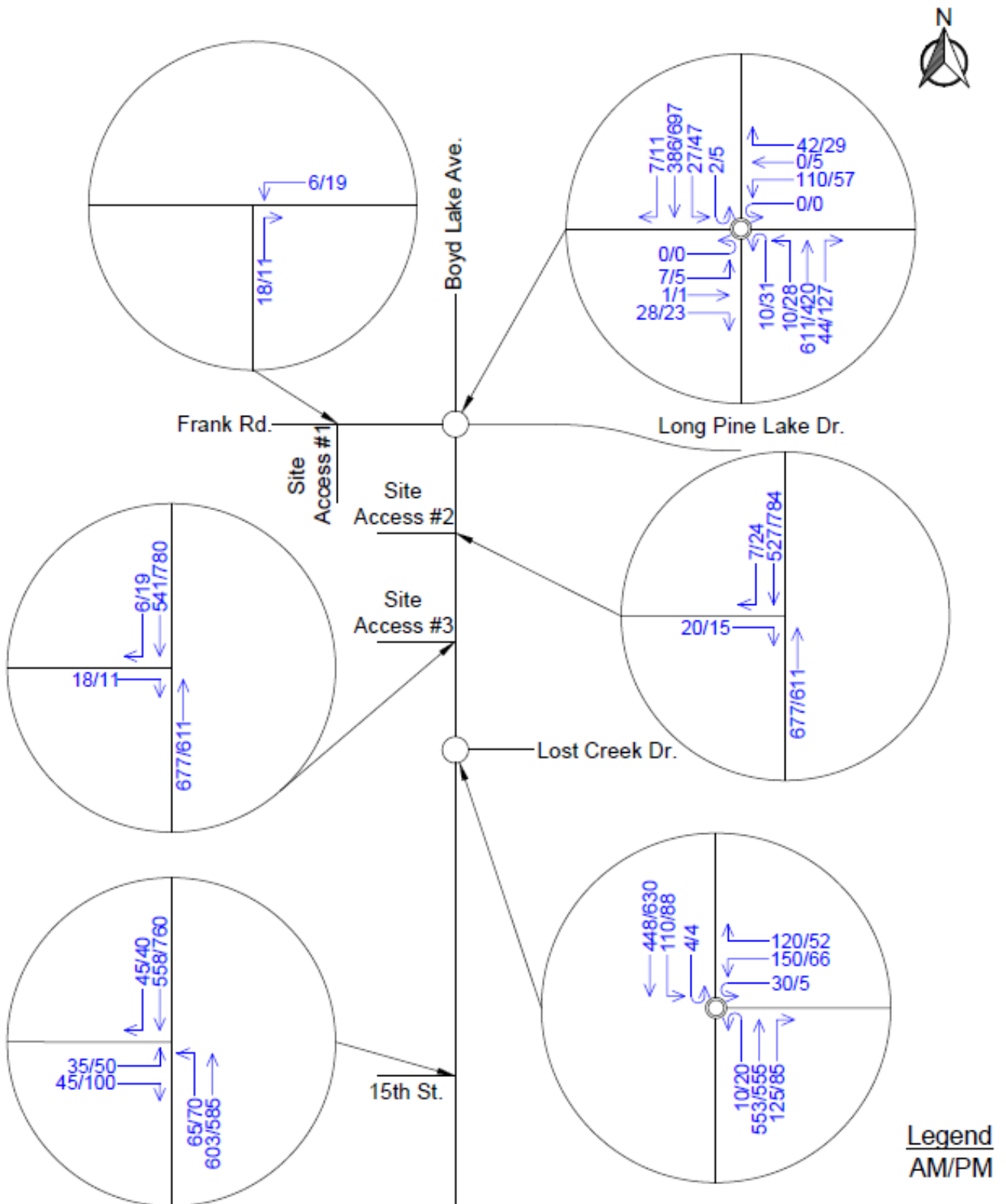


Table 2: Short Range Total Peak Hour Link Volumes

Street Segment	Direction	Existing Traffic Volume AM/PM	Date of Traffic Counts	2021 Background Traffic	Site Generated Traffic	2021 Total Traffic AM/PM	ACF Traffic Threshold	ACF Compliance
Boyd Lake Avenue south of Frank Road	NB	427/346	March 2018	662/563	13/43	675/606	965 (4-lane arterial)	Y/Y
	SB	316/635	March 2018	508/757	26/51	534/808	965 (4-lane arterial)	Y/Y
Frank Road west of Boyd Lake Avenue	EB	12/7	March 2018	18/18	18/11	36/29	60 (local residential)	Y/Y
	WB	7/18	March 2018	11/25	6/19	17/44	60 (local residential)	Y/Y

Table 3: Recent Peak Hour Operations

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./Frank Rd.	EB Approach	A	A	Y
	WB Approach	A	A	Y
	NB Approach	A	A	Y
	SB Approach	A	B	Y
	Overall	A	B	Y

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./Lost Creek Dr.	WB Approach	A	A	Y
	NB Approach	A	A	Y
	SB Approach	A	C	Y
	Overall	A	B	Y

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./15th St.	EB Left	C	D	Y
	EB Right	B	C	Y
	EB Approach	C	C	Y
	NB Left	A	A	Y
	NB Thru	A	A	Y
	NB Approach	A	A	Y
	SB Thru	A	A	Y
	SB Right	A	A	Y
	SB Approach	A	A	Y
	Overall	A	A	Y

Table 4: 2021 Background Peak Hour Operations

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./Frank Rd.	EB Approach	A	A	Y
	WB Approach	B	A	Y
	NB Approach	B	A	Y
	SB Approach	A	C	Y
	Overall	B	C	Y

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./Lost Creek Dr.	WB Approach	C	A	Y
	NB Approach	B	B	Y
	SB Approach	C	C	Y
	Overall	C	C	Y

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./15th St.	EB Left	E	F	N
	EB Right	B	C	Y
	EB Approach	D	E	N
	NB Left	A	B	Y
	NB Thru	A	A	Y
	NB Approach	A	A	Y
	SB Thru	A	A	Y
	SB Right	A	A	Y
	SB Approach	A	A	Y
	Overall	A	B	Y

Table 5: 2021 Short Range Total Peak Hour Operations

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./Frank Rd.	EB Approach	A	B	Y
	WB Approach	B	A	Y
	NB Approach	B	A	Y
	SB Approach	B	D	Y
	Overall	B	C	Y

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./Lost Creek Dr.	WB Approach	C	B	Y
	NB Approach	B	B	Y
	SB Approach	C	C	Y
	Overall	C	C	Y

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./15th St.	EB Left	E	F	N
	EB Right	B	C	Y
	EB Approach	D	E	N
	NB Left	A	B	Y
	NB Thru	A	A	Y
	NB Approach	A	A	Y
	SB Thru	A	A	Y
	SB Right	A	A	Y
	SB Approach	A	A	Y
	Overall	A	B	Y

Table 5: 2021 Short Range Total Peak Hour Operations (continued)

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Frank Rd./Site Access #1	EB Thru/Right	A	A	Y
	EB Approach	A	A	Y
	WB Thru/Left	A	A	Y
	WB Approach	A	A	Y
	NB Left/Right	A	A	Y
	NB Approach	A	A	Y
	Overall	A	A	Y

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./Site Access #2	EB Right	B	B	Y
	EB Approach	B	B	Y
	NB Thru	A	A	Y
	NB Approach	A	A	Y
	SB Thru/Right	A	A	Y
	SB Approach	A	A	Y
	Overall	A	A	Y

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./Site Access #3	EB Right	B	B	Y
	EB Approach	B	B	Y
	NB Thru	A	A	Y
	NB Approach	A	A	Y
	SB Thru/Right	A	A	Y
	SB Approach	A	A	Y
	Overall	A	A	Y

6.0 Findings

Based upon the analysis in this study, the proposed Boyd Lake Place project located at the southwest quadrant of Boyd Lake Avenue and Frank Road in Loveland, CO will be able to be demonstrate compliance with the City of Loveland's requirements for traffic at the time of full development with the following improvements.

The findings of the TIS are summarized below:

- The proposed project is anticipated to generate approximately 942 total average daily weekday trips, 75 total AM peak hour trips, and 99 total PM peak hour trips.
- The proposed project complies with the Adequate Community Facilities (ACF) Ordinance for traffic, the City of Loveland 2035 Transportation Plan, and the Larimer County Urban Area Street Standards (LCUASS).
- Except for the intersection of Boyd Lake Avenue/15th Street, the study intersections will operate acceptably and comply with the intersection levels of service (LOS) requirements in the LCUASS with the development of the Boyd Lake Place project and background traffic in the 2021 Short Range Total future.
- The Boyd Lake Avenue/15th Street intersection will not meet LCUASS LOS requirements with the existing geometry and intersection control in the 2021 Short Range Total future. Therefore, as proposed the future roundabout improvements at the Boyd Lake Avenue/15th Street intersection will comply with the intersection LOS requirements in the LCUASS with the development of the Boyd Lake Place project and background traffic in the 2021 Short Range Total future. It is therefore recommended that this roundabout be constructed by year 2021 and prior to full development of the Boyd Lake Place project.
- Once the proposed Boyd Lake Avenue/15th Street roundabout is constructed by others, the Boyd Lake Place project will comply with all of the LCUASS LOS requirements.
- Auxiliary lanes are not warranted at the proposed site access points to Boyd Lake Avenue or Frank Road.
- Traffic signal warrants are not anticipated to be warranted at the study intersections with the development of the Boyd Lake Place project and background traffic in the 2021 Short Range Total future.
- Per the Larimer County Urban Area Street Standards (LCUASS), the proposed development is responsible for the public street improvements adjacent to the property, unless designed and constructed by others.

- The following are the street classifications for the streets adjacent to and within the project site:
 - a. Boyd Lake Avenue = 4 Lane Arterial Street
 - b. Frank Road = Local Residential Street
 - c. Street A = Local Residential Street
 - d. Street B = Local Residential Street
 - e. Street C = Local Residential Street
 - f. Street D = Local Residential Street
 - g. Street E = Local Residential Street
 - h. Street F = Local Residential Street

Boyd Lake Place
Southwest Quadrant of Boyd Lake Avenue/Frank Road
Loveland, CO

Traffic Memorandum

KE Job #2018-036

Prepared for:

Lakeview Holdings, LLC
720 S. Colorado Blvd., Suite 940-N
Denver, CO 80246

Prepared by:



KELLAR ENGINEERING

www.kellarengineering.com
970.219.1602 phone



July 10, 2018

Sean K. Kellar, PE, PTOE

This document, together with the concepts and recommendations presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization from Kellar Engineering LLC shall be without liability to Kellar Engineering LLC.

Introduction

The following Traffic Memorandum (Traffic Memo) is an additional analysis to the *Boyd Lake Place Traffic Impact Study* (TIS) prepared by Kellar Engineering LLC (KE) dated June 12, 2018.

Kellar Engineering LLC (KE) has prepared this Traffic Memo to provide the 2021 Short Range Total LOS findings of the intersection of E. 15th Street/Boyd Lake Avenue with a right-in/right-out (RIRO) geometry. This Traffic Memo also provides a route analysis/comparison to address cut through traffic concerns for the subdivision north of Frank Road.

Existing Conditions and Roadway Network

The project site is located at the southwest quadrant of Boyd Lake Place and Frank Road in Loveland, CO. Boyd Lake Avenue is an existing north/south 4-lane arterial roadway with a posted speed of 35 mph adjacent to the project site. Boyd Lake Avenue has a raised center median, 4 travel lanes, and bike lanes adjacent to the project site. A detached sidewalk also exists along the east side of Boyd Lake Avenue adjacent to the project site. Multi-lane roundabouts have been constructed at the intersections of Boyd Lake Avenue/Frank Road and Boyd Lake Avenue/Lost Creek Drive. Frank Road is an existing east/west local residential street with a posted speed of 25 mph adjacent to the project site.

Figure 1: Vicinity Map

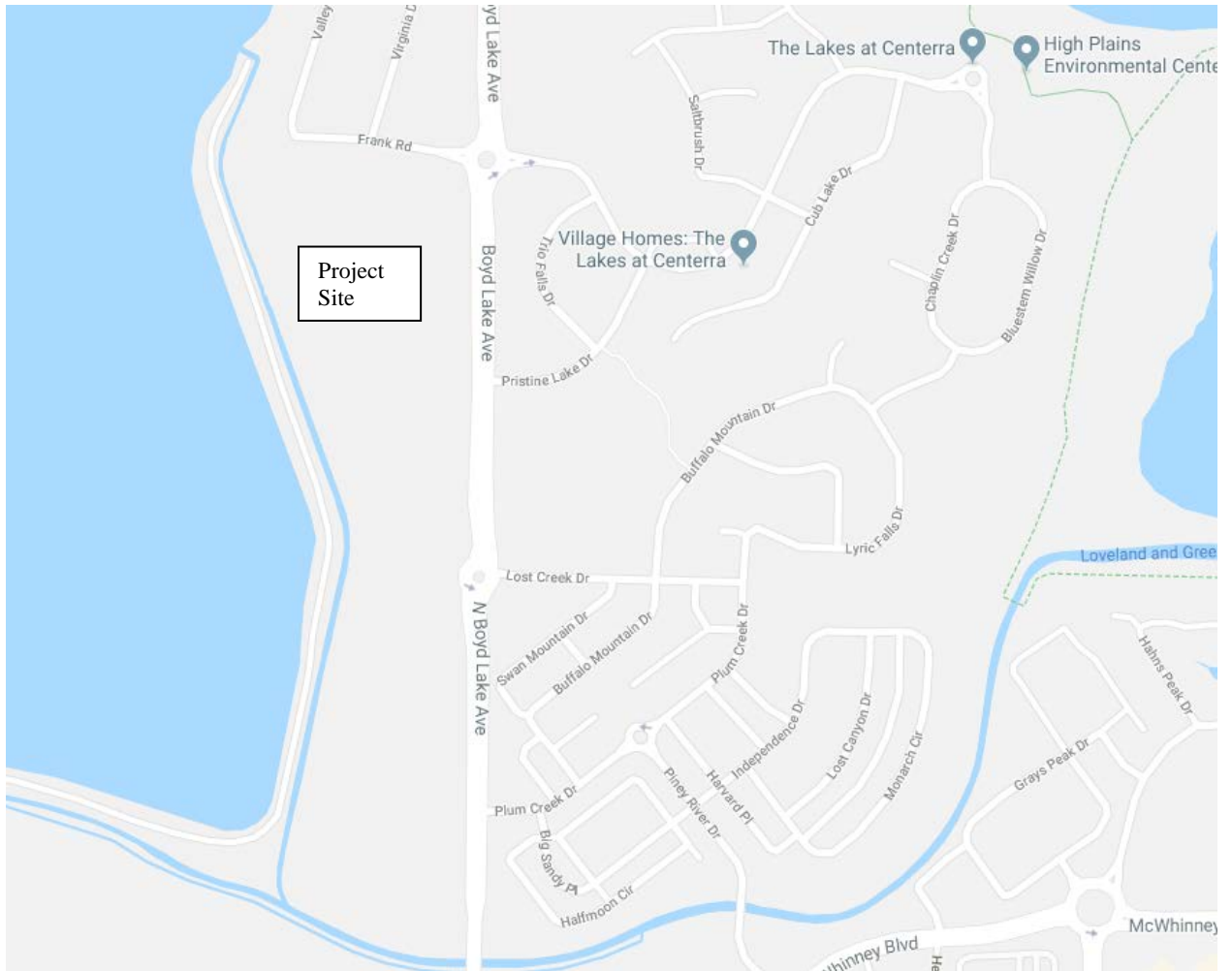


Figure 2: Site Plan



Illustrative Site Plan

BOYD LAKE PLACE
LOVELAND, CO



KA# 215026
0 60 120

Lake View Holdings LLC

5.08.2018



Pedestrian Facilities

Currently bike lanes exist along Boyd Lake Avenue adjacent to the project site. A detached sidewalk also exists along the east side of Boyd Lake Avenue adjacent to the project site. The proposed project will be responsible for designing and constructing the adjacent curb, gutter, and sidewalk facilities along Boyd Lake Avenue. These bike lane and sidewalk facilities will provide adequate and safe pedestrian and bicycle facilities.

Proposed Development

The proposed project consists of approximately 99 single family homes. See Table 1: Trip Generation and Figure 2: Site Plan.

Trip Generation

Site generated traffic estimates are determined through a process known as trip generation. Rates and equations are applied to the proposed land use to estimate traffic generated by the development during a specific time interval. The acknowledged source for trip generation rates is the *Trip Generation Report* published by the Institute of Transportation Engineers (ITE). ITE has established trip generation rates in nationwide studies of similar land uses. For this study, KE used the *ITE 9th Edition Trip Generation Report* average trip rates. The proposed project is anticipated to generate approximately 942 total average daily weekday trips, 75 total AM peak hour trips, and 99 total PM peak hour trips. See Table 1: Trip Generation.

Table 1: Trip Generation (ITE Trip Generation, 9th Edition)

ITE Code	Land Use	Size	Average Daily Trips		AM Peak Hour Trips						PM Peak Hour Trips					
			Rate	Total	Rate	% In	In	% Out	Out	Total	Rate	% In	In	% Out	Out	Total
210	Single Family Homes	99 DU	9.52	942	0.75	25%	19	75%	56	75	1.00	63%	62	37%	37	99
Total				942			19		56	75			62		37	99

DU = Dwelling Units

Figure 3: 2021 Short Range Total Peak Hour Traffic (RIRO)

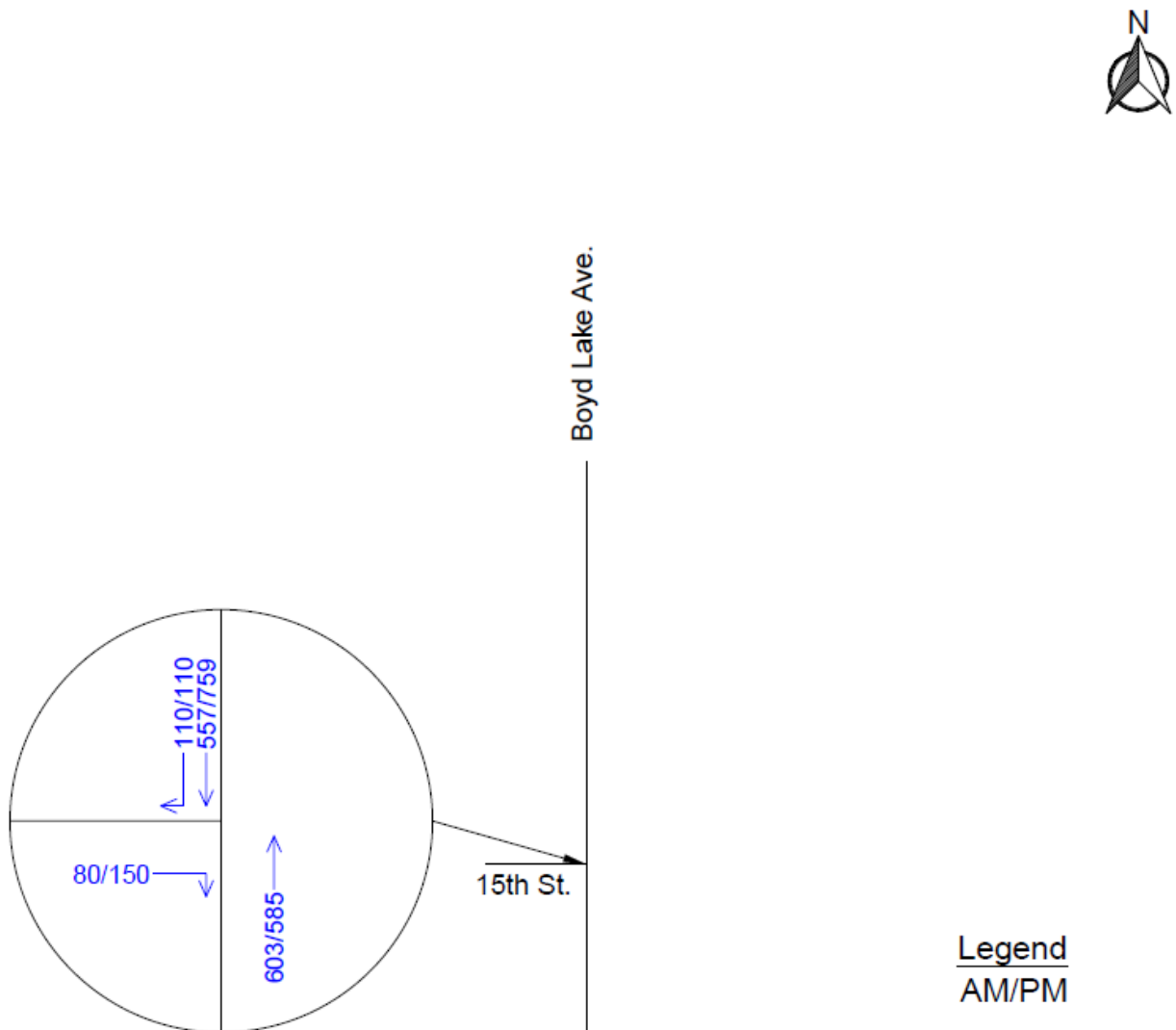


Table 2: 2021 Short Range Total Peak Hour Operations (RIRO)

Intersection	Movement	Level of Service (LOS)		ACF Compliance
		AM	PM	
		LOS	LOS	
Boyd Lake Ave./15th St.	EB Right	B	D	Y
(RIRO)	EB Approach	B	D	Y
	NB Thru	A	A	Y
	NB Approach	A	A	Y
	SB Thru	A	A	Y
	SB Right	A	A	Y
	SB Approach	A	A	Y
	Overall	A	B	Y

Route Analysis Comparison

A route analysis/comparison was conducted to assess cut through traffic potential for the existing subdivision to the north of Frank Road. Looking at the road network, Boyd Lake Avenue is an existing north/south 4-lane arterial with a posted speed of 35 mph adjacent to the project site. Valley Oak Drive is an existing north/south minor collector roadway with a posted speed of 25 mph. Also, the roadway geometry and residential characteristics of Valley Oak Drive provide for a low speed design. Boyd Lake Avenue's design and geometry are consistent with the arterial street standards in the LCUASS. Based upon the design, the travel time on Boyd Lake Avenue will be less than the travel time on Valley Oak Drive. Additionally, the travel time on Boyd Lake Avenue will be less than the travel time on any of the existing local residential streets within the subdivision north of Frank Road. In determining traffic distribution and trip distribution potential, traffic engineering principles are focused on delay that a driver will experience when traveling on a particular street or highway during a specific time interval. Based upon the design geometry and design speed, the majority of vehicles will utilize Boyd Lake Avenue because the travel time delay will be less. Drivers tend to take the quickest route. Based upon the road network, Boyd Lake Avenue will be the route that the highest percentage of traffic will travel upon. Using the 2021 Short Range Total traffic volumes, and looking at the travel time that it would take for a vehicle to travel north on Valley Oak Drive from Frank Road to Carrie Lane; Carrie Lane to Boyd Lake Avenue; and then turn left to head north on Boyd Lake Avenue; this route would take approximately 82 seconds during the AM peak hour (approximately 2,250 LF ~ 60 seconds of travel, plus ~ 21.9 seconds of intersection delay) and approximately 99 seconds during the PM peak hour (approximately 2,250 LF ~ 60 seconds of travel, plus ~ 39.3 seconds of intersection delay). Looking at the travel time that it would take for a vehicle to travel north on Boyd Lake Avenue from the Frank Road roundabout to Carrie Lane; it would take a vehicle approximately 50 seconds during the AM peak hour (approximately 1,950 LF ~ 38 seconds of travel, plus ~ 12.2 seconds of intersection delay) and approximately 59 seconds during the PM peak hour (approximately 1,950 LF ~ 38 seconds of travel, plus ~ 21.3 seconds of intersection delay). Therefore, based upon travel time calculations, the majority of the project's site generated traffic will utilize Boyd Lake Avenue to access the project site instead of traveling through the existing subdivision north of Frank Road.

Findings

The findings of the Traffic Memo are summarized below:

- The Boyd Lake Avenue/15th Street intersection will meet LCUASS LOS requirements with a right-in/right-out (RIRO) geometry.
- Based upon travel time calculations, the majority of the project's site generated traffic will utilize Boyd Lake Avenue to access the project site instead of traveling through the existing subdivision north of Frank Road.

APPENDICES:

Level of Service (LOS) Table

Level of Service Definitions

Level of Service	Signalized Intersection	Unsignalized Intersection
(LOS)	Average Total Delay	Average Total Delay
	(sec/veh)	(sec/veh)
A	≤ 10	≤ 10
B	> 10 and ≤ 20	> 10 and ≤ 15
C	> 20 and ≤ 35	> 15 and ≤ 25
D	> 35 and ≤ 55	> 25 and ≤ 35
E	> 55 and ≤ 80	> 35 and ≤ 50
F	> 80	> 50

LCUASS Table 4-2

Table 4-2
Loveland (GMA and City Limits)
Motor Vehicle LOS Standards (Intersections)

Intersection Component	Major Intersection ¹	Minor Intersection ²	Driveway
Overall (City Limits)	LOS C	LOS C	No Limit
Overall (GMAs)	LOS D	LOS D	No Limit
Any Leg	LOS D	LOS E	No Limit
Any Movement	LOS E	LOS F	No Limit
¹ Includes all signalized and unsignalized arterial/arterial and arterial/ major collector intersections. ² Includes all unsignalized intersections (except major intersections) and high volume driveways ³ There are no LOS standards for I-25 Interchanges. ⁴ On State Highways, overall LOS D is acceptable.			





2021 Short Range Total AM Peak Hour RIRO
1: Boyd Lake Ave & 15th St

Kellar Engineering LLC

07/10/2018

Intersection

Int Delay, s/veh 0.9

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	0	80	0	603	557	110
Future Vol, veh/h	0	80	0	603	557	110
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	94	0	709	655	129

Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	- 655	- 0	- 0
Stage 1	- -	- -	- -
Stage 2	- -	- -	- -
Critical Hdwy	- 6.22	- -	- -
Critical Hdwy Stg 1	- -	- -	- -
Critical Hdwy Stg 2	- -	- -	- -
Follow-up Hdwy	- 3.318	- -	- -
Pot Cap-1 Maneuver	0 466	0 -	- -
Stage 1	0 -	0 -	- -
Stage 2	0 -	0 -	- -
Platoon blocked, %		- -	- -
Mov Cap-1 Maneuver	- 466	- -	- -
Mov Cap-2 Maneuver	- -	- -	- -
Stage 1	- -	- -	- -
Stage 2	- -	- -	- -

Approach	EB	NB	SB
HCM Control Delay, s	14.7	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT EBLn1	SBT	SBR
Capacity (veh/h)	- 466	- -	- -
HCM Lane V/C Ratio	- 0.202	- -	- -
HCM Control Delay (s)	- 14.7	- -	- -
HCM Lane LOS	- B	- -	- -
HCM 95th %tile Q(veh)	- 0.7	- -	- -

2021 Short Range Total PM Peak Hour RIRO
1: Boyd Lake Ave & 15th St

Kellar Engineering LLC

07/10/2018

Intersection						
Int Delay, s/veh	2.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		↗		↖	↖	↗
Traffic Vol, veh/h	0	150	0	585	759	110
Future Vol, veh/h	0	150	0	585	759	110
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	85	85	95	95	85	85
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	176	0	616	893	129
Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	893	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.22	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.318	-	-	-	-
Pot Cap-1 Maneuver	0	340	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	340	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB	NB	SB			
HCM Control Delay, s	26.5	0	0			
HCM LOS	D					
Minor Lane/Major Mvmt	NBT	EBLn1	SBT	SBR		
Capacity (veh/h)	-	340	-	-		
HCM Lane V/C Ratio	-	0.519	-	-		
HCM Control Delay (s)	-	26.5	-	-		
HCM Lane LOS	-	D	-	-		
HCM 95th %tile Q(veh)	-	2.8	-	-		

ENVIRONMENTALLY SENSITIVE AREAS REPORT

**24.696 Acres within Section 8,
Township 5 North, Range 68 West
Larimer County, Colorado 80538**

**Western Environment and Ecology, Inc.
Project Number: 754-001-01**

PREPARED FOR:

**Lakeview Holdings, LLC
720 South Colorado Boulevard, Suite 940N
Denver, Colorado 80246**

June 29th, 2017

Prepared by:



Brendan Calonge
Senior Staff Scientist

Reviewed by:



Greg D. Sherman P.G.
President

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WESTERN ENVIRONMENT
AND ECOLOGY, INC.
2217 West Powers Avenue
Littleton, Colorado 80120

Figure 3 - Boyd Lake Buffer Map
24.696 Acres within Section 8,
Township 5 North, Range 68 West,
Larimer County, Colorado
PC Attachment 5

5.0 CONCLUSIONS AND RECOMMENDATIONS

At the time of the survey, no threatened or endangered species or their obvious habitat were seen on the subject site. The Greeley-Loveland Irrigation Company Canal is likely Waters of the U.S. subject to regulation under Section 404 of the Clean Water Act occur on the project. **Any impact to the Greeley-Loveland Irrigation Company Canal during development will require consultation with the Corps.**

- Riparian habitat marginally suitable to Preble's Meadow Jumping Mouse (*Zapus hudsonius preblei*) was observed along the Greeley-Loveland Irrigation Company Canal. However, based upon the poor quality of habitat and the upland habitat on the site, Western Environment does not consider the project to be Preble's habitat. On June 28th, 2017, Western Environment submitted a request for a "Finding of No Effect" letter (attached) to the U.S. Fish and Wildlife Service, for concurrence that development of this property would not adversely affect Preble's.
- The extreme northwest corner of the property and portions of the western border are within the Loveland Parks & Recreation Master Plan 2014 (Master Plan) 300 foot buffer. However, poor natural area habitat present on the site as well as the development adjacent to the north represent justification for reduction of buffer consideration to 75 feet.

No other Ecological Issues were found.

6.0 LITERATURE CITED

WESTERN ENVIRONMENT AND ECOLOGY, INC.

CEDAR CREEK
ASSOCIATES, INC.

916 Willshire Ave. • Fort Collins, Colorado 80521 • (970) 493-4394



P37-11-13D

DEC 28 1998

Mr. Matt Dulcich
Current Planning Division
City of Loveland
500 East Third Street
Loveland, Colorado 80537

December 23, 1998

Re: Environmentally Sensitive Areas Report for the Lakeview Estates Project

Dear Mr. Dulcich:

This letter is submitted on behalf of MSP Companies and addresses the characteristics of a parcel of land (study area), known as Lakeview Estates, lying to the south of the intersection of County Road 9 and Frank Road near Loveland, Colorado. The study area lies in the east 1/2 of Section 8, T5N, R68W and consists of approximately 34.0 acres. Current land use consists of a farming operation in row crop and hay production. The study area is bounded on the north by the Boyd Lake Shores Third Filing, on the east by private property composed of cropland, on the south by the Waterfall Subdivision, and on the west by property adjoining Boyd Lake. The maps included with this submittal depict the boundaries of the Lakeview Estates study area, known previously as the Frank Subdivision.

The data and associated text presented in this report are based on a detailed reconnaissance of the study area completed on July 27, 1998 and a follow-up site review conducted on December 23, 1998.

Summary of Study Area Characteristics

The topography of the 34-acre study area is uniform, with a nearly level to gently (<5 percent) sloping westerly aspect. There are no topographic breaks, draws, or other similar features present. Surficial drainage is generally to the west. No streams, creeks, ponds, or seeps occur on within the boundaries of the study area. The Greeley-Loveland Irrigation Canal parallels the west and south study area boundaries and crosses the extreme northwest corner of the study area. In addition, small feeder ditches cross the subject property from east to west.

The soils overlying the study area consist of the Fort Collins, Nunn, and Ulm series. These soils are deep, well drained, slowly to moderately permeable, and have a high water holding capacity. They are not subject to flooding and have water tables deeper than 6.0 feet. None of these soils is considered to be hydric by the Natural Resources Conservation Service (Natural Resources Conservation Service, 1980, Soil Survey of Larimer County Area, Colorado). Given the slope of the study area and surface soil textures, the soils are considered to have a low to moderate erosion potential in their natural state.

In terms of vegetation, the study area consists of agricultural fields characterized by wheat stubble, corn, and alfalfa plantings. No trees or shrub communities were observed within the study area boundaries. Land use surrounding the study area consists of a housing development to the north, a recreational development to the south, agricultural fields to the east, and agricultural fields and Boyd Lake to the west.

Inventory of Environmentally Sensitive Areas

Mature Stands of Vegetation

The study area consists entirely of a farming operation characterized by row crops and hay production. There are no mature shrub or tree vegetation communities within the study area boundaries. Several *Elaeagnus angustifolia* trees, ranging from 10 to approximately 20 feet tall, surround the house on County Road 9 which was excluded from the study area boundaries.

Legal or Jurisdictional Wetlands as Defined by the U.S. Army Corps of Engineers

Wetlands are limited to the extreme northwest study area boundary. These wetlands occur as a narrow, intermittent fringe along the Greeley-Loveland Irrigation Company canal and total less than 0.005 acre. The dominant species characterizing these wetlands is *Phalaris arundinacea*. Formal mapping of these wetlands has not been completed to date. It is unlikely that these wetlands would qualify as jurisdictional given that they lie along the inslope of the irrigation canal. Such wetlands are typically excluded from jurisdictionality by the Corps of Engineers. There are no plans to disturb this canal or the associated wetlands as a part of the proposed development.

These wetlands do not appear to qualify as habitat for the threatened species *Spiranthes diluvialis*.

Wildlife Habitat Areas and Corridors

The study area supports a cropland vegetation community association as described above. There are no mature vegetation stands present and the topography is nearly level. As such, the project area has a limited wildlife habitat value and there are no known special habitat characteristics present which would render this 34.0-acre study area unique in terms of wildlife habitat. The study area, being part of an active farming operation, does not appear to qualify as a wildlife corridor as such, though small mammals and avian species undoubtedly pass through or over the area to reach Boyd Lake.

There does not appear to be any habitat suitable for *Zapus hudsonius preblei* present within the boundaries of the study area.

Natural Areas Identified in the City of Loveland *Natural Areas Inventory Study*

Boyd Lake is the only natural area near the Lakeview Estates study site which is listed in the *Natural Areas Inventory Study* document. This water body has been given an overall habitat rating of 8, a "medium" enhancement potential, and is an important habitat component for migrating waterfowl from late fall through early spring. The undeveloped lake margins and marshy areas provide additional habitat for a variety of songbirds, raptors, waterfowl, and shorebirds. Waterfowl and shorebird use of the lake and shoreline areas, however, is generally restricted by intensive recreational boating use of the lake. The Lakeview Estates study area boundary is typically over 200 feet to the east of the "approximate edge of Boyd Lake" (see accompanying maps).

Physical Linkages to Other Natural Areas or Open Spaces

The study area is physically linked to Boyd Lake in that the area between Boyd Lake and the study area remains undeveloped except for the constructed irrigation canal. Open spaces, in the form of agricultural fields, occur to the east of the study area. The study area would therefore serve as a physical linkage between these agricultural fields and Boyd Lake.

Land Within or Affecting Floodway and Flood Fringe Boundaries

The study area drains to the west. No creek, streams, or ponds occur within the study area boundaries which would be subject to flooding. The only water body near the study area is Boyd Lake. The study area does not appear to be within a flood zone or flood fringe boundary associated with this water body by virtue of the soil and vegetation characteristics observed.

Irrigation Canals and Ditches

The Greeley-Loveland Irrigation Company Canal parallels the west and south study area boundaries and crosses the extreme northwest corner of the study area. In addition, small feeder ditches cross the subject property from west to east as a part of the farming operation. No other canals or ditches are associated with the Lakeview Estates site.

Water Courses, Stream Corridors, or Estuaries

Aside from the irrigation structures noted above, no other water courses or estuaries occur within the study area boundaries.

Existing Slopes Over Twenty Percent

The study area is characterized by a nearly level to gently sloping topography having slopes less than approximately 5 percent. No slopes over 20 percent occur within the study area boundaries except those associated with the irrigation canal inslopes located in the extreme northwest corner of the property.

Soils Having a High Water Table, Being Highly Erodible, Subject to Erosion, or Highly Acidic

Soil map units overlying the study area include the Fort Collins loam, 1 to 3 percent slopes; Nunn clay loam 0 to 1 percent slopes; Nunn clay loam, 1 to 3 percent slopes; Ulm clay loam, 0 to 3 percent slopes; and the Ulm clay loam, 3 to 5 percent slopes as delineated by the Natural Resources Conservation Service. These map units are rated as having wind and water erosion hazards ranging from slight to moderate. The depth to a seasonal water table for all listed soils is greater than 6.0 feet and there is no potential for flooding for any of these map units. No highly acid soils overly the study area. The pH values for the typical profiles of the soils listed above range from 6.6 to 8.4 (Natural Resources Conservation Service, 1980, Soil Survey of Larimer County Area, Colorado).

Operating High Water Line, Aquifer Recharge and Discharge Areas, Land Incapable of Meeting Percolation Requirements, Land Formerly Used for Landfill Operations or Hazardous Industrial Use, and Fault Areas

These topics were not included as part of the work assigned to Cedar Creek. No discussions related to these topics are presented in this letter report.

Assessment of Potential Impacts of Proposed Development

In terms of direct impacts, few environmentally sensitive areas listed as of concern by the City occur within the boundaries of the study area. The wetlands established in the extreme northwest portion of the study area, as well as the canal along which they are located, will be avoided during the development of this parcel of land. No direct impacts to these sensitive areas are anticipated. Any physical linkage the study area provides between the agricultural fields (open space) to the east and Boyd Lake to the west would be eliminated. Such a linkage is likely limited to the occasional movement of small mammals and furbearers from east to west and vice versa, and would not constitute a "wildlife corridor" of any consequence. Development would not disrupt this linkage for avian species.

With respect to indirect impacts, housing construction might temporarily interrupt wildlife use of the *E. angustifolia* trees associated with the house located along County Road 9. Avian species utilizing these trees are likely accustomed to human activity given the juxtaposition of the existing house to the trees and County Road 9, as well as the land use characteristics of the surrounding area. It can reasonably be assumed that the avian species common to the area would return and continue to use this resource in the future.

The study area, as noted above, typically lies over 200 feet from the approximate edge of Boyd Lake. This distance exceeds 500 feet on average along the central and southern boundaries of the property. Given that the canal along the northwest property boundary will not be disturbed as a result of development, a buffer zone in excess of 200 feet east of the lake edge would be more than adequate to limit indirect impacts to Boyd Lake. Where this distance closes to under 200 feet in the northwest, the deep, steep-sided Greeley-Loveland Irrigation Company canal would serve as a barrier to casual lake access.

A wetland of varying width and composition has become established between the approximate lake edge and the canal. The distance from the edge of this wetland to the property boundary generally ranges from 300 to 475 feet in the central and southern portions of the study area. Along the northern one-third of the property this distance narrows to from 100 to 150 feet excepting for the extreme northwestern corner of the property. Overall, such distances will provide a natural "buffer" more than adequate for protecting this medium quality wetland from the indirect affects of this project. Where more narrow distances prevail to the north, the canal referred to above would provide an additional buffering affect. It should also be noted that the widest and potentially most valuable wetlands between Boyd Lake and the study area parallel the central and southern study area boundaries where the "buffer zone" distances are the greatest. As a consequence, the potential indirect impacts of this proposed development to these existing wetlands are minimal.

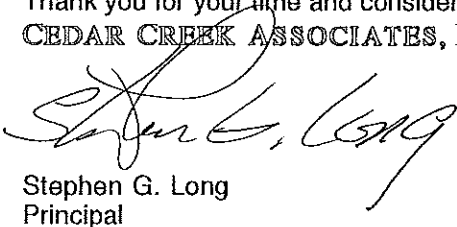
Recommendation: Protection Measures, Mitigation, Enhancement

The wetland/canal complex in the northwest corner of the study area will be avoided during development. Therefore, there will be no direct impacts to environmentally sensitive areas as a result of this project. Indirect impacts are few and considered to be limited in affect. The naturally occurring buffer zone between the study area and Boyd Lake, consisting of undeveloped land and the Greeley-Loveland Irrigation Company canal, would serve to minimize, or eliminate, any indirect impacts to this water resource. In light of this analysis, no additional protection or mitigation measures are considered to be necessary.

One measure could be employed as a means of enhancing the wildlife characteristics of the buffer zone near the northwest corner of the property near Frank Road. Where the distance between the study area boundary and the edge of Boyd Lake narrows to less than 250 feet, shrub plantings could be made to provide a vegetation component not present on or near the study area. The species selected for planting should be drought-tolerant, valuable to wildlife in terms of food and cover, and be capable of a high rate of establishment with a minimum of supplemental water. It may be appropriate to avoid tree plantings in this area since this could result in a "snow fence effect" unacceptable to future landowners.

I trust this letter report will provide you with the information you need for your analysis. If you have any questions regarding the contents of this letter, please call (493-4394) at your convenience. Should you require information concerning the development project specifically, please call Mr. Marcus Palkowitsh at 303-399-9804.

Thank you for your time and consideration;
CEDAR CREEK ASSOCIATES, INC.


Stephen G. Long
Principal

*Marcus: I sent my resume and two maps with
the copy to Matt Dulcich of Current Planning.*

From: Nanci LaBianca
To: [Kerri Burchett](mailto:Kerri.Burchett@mspcompanies.com); chad@mspcompanies.com
Subject: Proposed Project Boyd Lake Road
Date: Wednesday, July 11, 2018 10:05:47 AM

Hi Kerri,

I just received the new and amended development plan for the area east of Boyd lake, off Frank Road and by High Plains School.

I was at the initial meeting at the Great Western where we filled the room with over 200 people. We voiced our concerns about the high density apartments and the many issues we felt would occur with the developers' plans at that time.

After reading the new plans, I am pleased that the developers actually listened to us. The new single family homes proposed to be built there are more conducive to our area and needs. I am pleased with the efforts the developers went through to make sure that our concerns were met.

Thank you for your part in helping to get these amendments made and for listening to the people living in that area.

Sincerely,
Nanci LaBianca
1935 Big Sandy Place
Loveland, CO 80538

From: No.Reply
To: [Kerri Burchett](#)
Subject: Email contact from City of Loveland
Date: Thursday, June 7, 2018 7:47:37 AM

Message submitted from the <City of Loveland> website.

Site Visitor Name: Diane Duelfer
Site Visitor Email: dzdmotion@gmail.com

Hello Kerri Burchett -

I am writing regarding the building of new homes at the Southwest Corner of Frank Road and Boyd Lake Avenue. Whatever happened to Boyd Lake Open Space? I was under the impression that did not allow for any more land development around Boyd Lake.

Loveland is experiencing a huge amount of new construction taking away land that is necessary environmentally to allow water to soak into the ground and not simply run-off into gutters. in addition wildlife have decreased access to natural habitat forcing changes in natural landscape contributing to unnecessary crowding for birds, wildlife and aquatic species.

It is my hope that you and the City of Loveland's planning division will re-evaluate the building of these 100 single family residential units.

Thank you in advance to your time and attention to this matter.

Sincerely,
Diane Z. Duelfer
dzdmotion@gmail.com

From: John Isaak
To: ["Hoyle Curtis"](#)
Cc: [Kerri Burchett](#)
Subject: May 24 MSP Meeting Thoughts
Date: Friday, May 25, 2018 9:10:28 AM

Kerri Burchett, Hoyle & BLS 3rd filing Board Members: Below are some of the take-a ways that summarize our thoughts on last night's MSP update regarding Lakeview Holding LLC development plans. Let us further suggest that our BLS 3rd filing board should work closely with the City of Loveland regarding our neighborhood concerns. Overall it seems to us that this High Density single family MSP proposed development is being well received by the City of Loveland and the chance of eventual approval could occur within the next few months because of the single family plan correlates with the City of Loveland's overall master development plans. As such, BLS and the City of Loveland now collectively have to develop plans on how we can best be involved in making sure our concerns get addressed.

- A. **Frank Road Concern:** The traffic on Frank Road will be significantly increased because of 2 factors. 1st, the Frank Road exit is the only one on the North end of the MSP development. In fact, Kerri Burchett of City of Loveland indicated that this road would become part of the City of Loveland. Does this imply that the City will then be responsible for the road surface and gutter maintenance? 2nd, the proposed MSP development plan positions 12 homes with access to their homes *directly off of Frank Road*. This added congestion on Frank Road is excessive and could be a major walking and driving safety issue.
- B. **Valley Oak Drive Concerns:** The traffic on Valley Oak Drive will also be significantly increased. When the Boyd Lake Road 4-lane expansion is completed then all BLS residence that want to travel north will have to access Boyd Lake Avenue at the Frank Road or White Bark round-a-bout intersections. This change will increase the traffic on Valley Oak to the point that our 1st, 2nd, and 3rd BLS will not be able to financial support the surface and gutter maintenances without significant special assessments or annual HOA fee increases. Kerri, you highlighted the City's master plan to have communities connected so if MSP is approved can BLS assume the City of Loveland will also maintain Valley Oak Drive similar to their plans for Maintaining Frank Road?
- C. **55+ Miss-representation:** The 55+ Friendly characterization of these high density single family homes is a miss-representation of the overall plans. In fact, Chad Rodrigues stated that there will be NO age restrictions. This means that the proposed 99 homes will be a normal single family neighborhood with a mix of all age groups. So, there most certainly will be school age children that will be impacting the Loveland school district and we should make sure that the school board is aware of this fact. If this truly would be a 55+ community then there would be a club house (possible pool) for meeting and recreation purposes that senior citizens enjoy and would use often. Further the overall traffic impact would be reduced and there would be NO impact on the school systems of Loveland. The Loveland and Fort Collins cities are frequently listed nationally as prime retirement areas so

we suggest and encourage the City of Loveland to only approve this development requiring 55+ Ownership. Seniors are most open to small lots and smaller living quarters as they down size when retiring. We believe a 55+ criteria would actually be very well received and sales would thrive because it would be different than all current area home residential communities. Such a 55+ requirement would actually be a win for the City of Loveland, the Loveland School District, and all of Boyd Lake Shores. And, MSP would realize the same financial results and be viewed as a good and responsive neighbor.

- D. **South Round-A-Bout Access:** MSP and McWhinney need to resolve the existing South round-a-bout access issue. It seems that Chad Rodrigues is telling us that they have not been able to make resolution progress but just last week McWhinney stated that they were open to such a discussion. Our board via the City of Loveland should request assistance in facilitating open discussions between the two developers. This south access is a KEY issue and if approved would significantly reduce the traffic congestion on Frank Road and Valley Oak Drive.

Thanks for letting us share and do believe that this is a most key time for BLS and know collectively our board and the City of Loveland will take the appropriate actions regarding our concerns and interests. Kerri, you are a key player in directing this development and ask that you give serious consideration to the thoughts of a community that has been in the area since the early 1970's. We are aware of the tremendous growth in our immediate area and ask you to make sure our concerns and suggestions are taken into consideration in the approval of the development during the resolution process.

-

Jean & John Isaak
3205 Valley Oak Drive

From: Larry Pepek
To: [Kerri Burchett](#); [Lizzy Ginger](#); [Rick Parker](#); [tate lundy](#); [Darren Duerksen](#); [Beth Sedillo](#)
Subject: Re: Meeting for the development south of Frank road
Date: Friday, May 18, 2018 12:25:09 PM

Kerri,

We received a paper copy of the latest sketch plan in the mail a day or so after I contacted you. I took a look at the new site plan and it is huge improvement over the first submittal. I do see some issues that I would like to pass along that I feel will need to be addressed.

1) The houses on Frank Ave. north of Virginia Dr. will have one heck of a time backing out of their driveways. There are already concerns from the existing neighbors dealing with cars exiting the roundabout at high speeds. Being that Frank Road is the only access that can be used to go north I would expect that there will be increased traffic on Frank Road thus making it even more difficult to exit their driveways. The removal of the five proposed houses east of Virginia Dr. would help to make the situation safer.

2) It appears that the planned neglected to label the road that ties into Frank Road as a point of access. Not only is it a point of access it is the access that will be primarily used to go north on Boyd Lake Ave. after leaving the subdivision.

3) We have always argued that the developer should use the roundabout to the south, however the developer maintains that McWhinney will not give him access to the right of way. I find this very hard to believe and would like to see a letter from McWhinney stating that under no circumstances will they give access to this project. My guess the developer can obtain access if they are willing to pay for it.

4) It appears that the right of way width for Frank Road is small considering the increased traffic. I would like to see the developer dedicate additional right of way to make it wider. Making the road wider will make it easier for the existing houses to enter Frank Road.

In conclusion, this plan is much better than the previous however it does not address all the traffic and safety concerns along Frank Road. If I had it my way no additional homes would back out onto Frank Road and an additional fork in the road would be built to divert the additional traffic off of Frank Road into the proposed subdivision. The first row of homes on the north side of the subdivision would back up to Frank Road but would not have direct access to it. Making these simple changes to site plan would hardly impact the lot count and would make the situation much safer. You and I exchanged a couple drawings that showed a fork of some sort that would limit the exposure of the existing homes on Frank Road to all the additional traffic. I would like to see the developer follow up with this idea. If you could provide me with a CAD file of the submittal I would be more than happy to show how this would work. If you have any questions please let me know.

Thank you,
Larry

On Thu, May 10, 2018 at 4:01 PM, Kerri Burchett <Kerri.Burchett@cityofloveland.org> wrote:

Hi Larry,

From: Nancy M Houghton
To: [Kerri Burchett](#)
Subject: Boyd Lake Place
Date: Saturday, May 12, 2018 10:39:19 AM

Dear Ms. Burchett,

I received the developer, Lakeview Holdings, LLC updated plans. They look very nice. I am happy with the changes that were made. I hope to come to the next meeting to learn more details about the development.

Sincerely,

Nancy Houghton
2551 Cub Lake Ct.
Loveland

Sent from my iPhone

From: Chad Rodriguez
To: [Kerri Burchett](#)
Subject: FW: Proposed project on Frank Rd.
Date: Tuesday, May 29, 2018 9:21:59 AM

Chad Rodriguez
MSP Companies
Suite 940N
720 S. Colorado Blvd.
Denver, CO 80246
303.399.9804
303.399.3631 Fax
303.570.3708 Mobile

From: Filler, Paul (PA) <Paul.Filler@us.sasol.com>
Sent: Monday, May 28, 2018 8:25 AM
To: Chad Rodriguez <chad@mspcompanies.com>
Subject: Proposed project on Frank Rd.

Chad,

I read your notice and discussed this with a few others and I want to say we were very happy with your new plan.

Wanted to let you know.



Sasol (USA) Corporation
12120 Wickchester Lane
Houston, Texas 77079
www.sasol.com

Paul Filler

Market Development Manager – Agrochemicals
Performance Chemicals

Mobile +1 337-764-3316
Fax +1 281-368-8735
E-mail Paul.Filler@us.sasol.com

NOTICE: Please note that this eMail, and the contents thereof, is subject to the standard Sasol

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If you cannot access the legal notice through the URL attached and you wish to receive a copy thereof please send an eMail to legalnotice@sasol.com

From: Douglas Webster
To: chad@msspcompanies.com
Cc: [Kerri Burchett](#)
Subject: Boyd Lake Development
Date: Thursday, May 24, 2018 7:12:10 PM

Chad,

We live at the corner of Frank Road and Boyd Lake Avenue. No one will be affected by the development south of Frank Road more than we will be. We do believe that traffic will be an issue until the round-a-bout is completed at the south of the property but we feel that this is an issue that is out of your hands,

That being said, we wish to express our appreciation for the major changes that you have proposed to the development in accordance to concerns expressed by the neighborhood. You exceeded our expectations for modifications and we anticipate this might have been at some financial loss to your company. The proposed single family units will be much more in keeping with the feeling of the community and despite the inevitable increase in traffic, the proposed decreased density is more than significant.

Thank you so much for being willing to work with the neighbors. We wish that there had been less negativity at the meeting but the reason that there was a lower turnout than the first meeting most likely represents the fact that many in the community were pleased with the plans.

We look forward to the development as planned and feel thankful that you have acted in good faith in response to residential concerns.

Respectfully,

Doug and Terry Webster

From: Bob Kaphammer
To: chad@msspcompanies.com
Cc: [Kerri Burchett](#); [Bob Kaphammer](#)
Subject: Boyd Lake Place Addition
Date: Monday, May 21, 2018 3:20:07 PM

Chad,

I am a home owner just a few houses north of this proposed development. I was at the meeting for the previous proposal and agreed with the majority of the audience that the multi-units proposed were not appropriate for this area next to the lake.

This new proposal with single-family units targeted at a 55+ demographic I think is a much better fit for the area and is more compatible with the existing community to the north.

That being said there are still some key areas of concern:

1. There is only one reasonable way out of the development if you are heading north and that is onto Frank Rd which gives them incentive to use Virginia or Valley Oak. I'm not counting the future roundabout connection at the south end because I don't think you will be able to force this requirement onto the future developer to the south. Also anyone returning into the development from the south has to go to the north end at Frank Rd the back south to their place. If this north access onto Frank was moved to the east, about halfway between the roundabout and Virginia, it would encourage more people to use the roundabout and not go down Virginia or Valley Oak.
2. In your description of the project you stated that "...The community will offer low-maintenance active adult living." I don't see any facilities for 'active adults' to be active in. There is no club house, swimming pool, pickle ball courts, not even a park with a bench or picnic table. The only thing I see is a multi-use trail with a little grass along it.

Thanks for your time,
Bob Kaphammer
2800 Valley Oak Dr.
970-619-8693

BOYD LAKE PLACE ADDITION
TO THE CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO BEING LOCATED IN THE EAST HALF
OF SECTION 8, TOWNSHIP 5 NORTH, RANGE 68 WEST OF THE 6TH PRINCIPAL MERIDIAN

DESCRIPTION:

A tract of land located in Section 8, Township 5 North, Range 68 West of the 6th P.M., County of Larimer, State of Colorado, and being more particularly described as follows:

Considering the East line of the Northeast Quarter of said Section 8 as bearing South 00° 24' 03" West and with all bearings contained herein relative thereto:

Commencing at the East Quarter Corner of said Section 8; thence, North 89° 35' 57" West, 30.00 feet to a point on the West right-of-way line of Boyd Lake Avenue as shown on Rocky Mountain Village Addition; thence along said West line, North 00° 24' 03" East, 97.78 feet to the **POINT OF BEGINNING**; thence, North 89° 35' 57" West, 30.00 feet; thence along a curve concave to the north having a central angle of 10° 50' 23" with a radius of 373.00 feet, an arc length of 70.57 feet and the chord of which bears South 66° 01' 14" West, 70.46 feet; thence, North 38° 30' 52" West, 635.29 feet; thence, North 16° 20' 52" West, 395.50 feet; thence, North 40° 17' 52" West, 300.00 feet; thence, North 10° 56' 52" West, 350.00 feet; thence, North 39° 00' 52" West, 460.00 feet; thence, North 21° 20' 08" East, 13.12 feet; thence, South 88° 04' 39" East, 314.14 feet; thence, North 01° 55' 15" East, 39.61 feet; thence along a curve concave to the east having a central angle of 00° 06' 40" with a radius of 230.00 feet, an arc length of 0.45 feet and the chord of which bears North 01° 58' 35" East, 0.45 feet; thence, South 88° 04' 39" East, 824.30 feet; thence, South 00° 24' 02" West, 25.74 feet; thence, South 88° 04' 39" East, 20.00 feet to the West right-of-way line of Boyd Lake Avenue as shown on Millennium Addition; thence along said West line, South 00° 24' 03" West, 1662.07 feet to the Point of Beginning.

The above described tract of land contains 1,190,623 square feet or 27.333 acres, more or less and is subject to all easements and rights-of-way now on record or existing.

ANNEXATION:

Total Perimeter 5,244 feet

Contiguous Boundary 1,812 feet

Minimum Contiguous Feet Required 874 feet

NOTES:

- This Annexation Map does not represent an actual field survey. It is made from recorded legal descriptions and platted subdivisions and it is not intended to be a land survey plat.
- The lineal unit of measurement for this map is U.S. Survey Feet.
- The Basis of Bearings is the East line of the Northeast Quarter of Section 8-5-68 as bearing North 00° 24' 03" East and monumented as shown hereon.
- For all information regarding easements, right-of-way, or title of record, Northern Engineering relied upon File No. 36200-17-01967, Revision No. 2, dated April 27, 2018, by North American Title Insurance Company.
- Subject parcel lies in FEMA FIRM Map 08069C1200F, not printed.
- This property is subject to an annexation agreement which has been recorded in the real property records of Larimer County.
- Property within this Addition is subject to an avigation easement beneficial to the Northern Colorado Regional Airport that provides for unobstructed use or airspace above said property. Restricted airspace reserved as part of this easement limits building heights to a maximum of fifty (50) feet above ground level. The easement limits remedy against noise, vibration, fumes, dust, particles and other effects incident to aviation use and operations at the Northern Colorado Regional Airport, and includes limitations on use of the property that could interfere with the safe navigation of aircraft through said space.
- Boyd Lake Water Surface Elevation Information:
 - Normal water surface elevation: 4959.59
 - City of Loveland 100-year storm water surface elevation: 4960.08
 - State Engineer's Office (SEO) maximum water surface elevation: 4962.00

Surveyor Certificate:

I, Robert C. Tessely, a registered Land Surveyor in the State of Colorado, do hereby certify that the annexation map shown hereon is a reasonably accurate depiction of the parcel of land legally described hereon and, to the extent described herein, is at least one sixth (1/6) of the peripheral boundary of said parcel is contiguous to the boundary of the City of Loveland, Colorado. The map was compiled using existing plats, deeds, legal descriptions, and other documents and is not based on a field survey nor should it be construed as a boundary survey.

For and on Behalf of Northern Engineering Services
Robert C. Tessely
Colorado Registered Professional
Land Surveyor No. 38470

The foregoing instrument was acknowledged before me this _____ day of _____, _____, by Robert C. Tessely.

Witness my hand and official seal.

My commission expires _____.

Notary Public

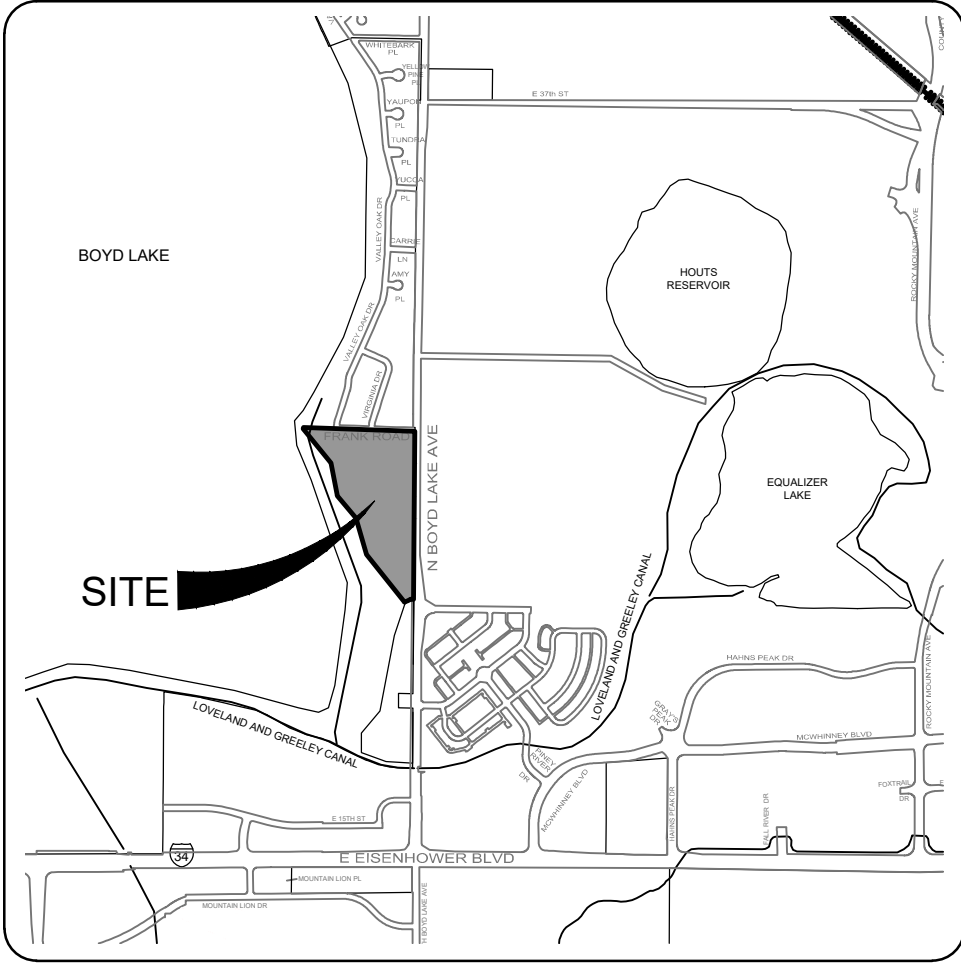
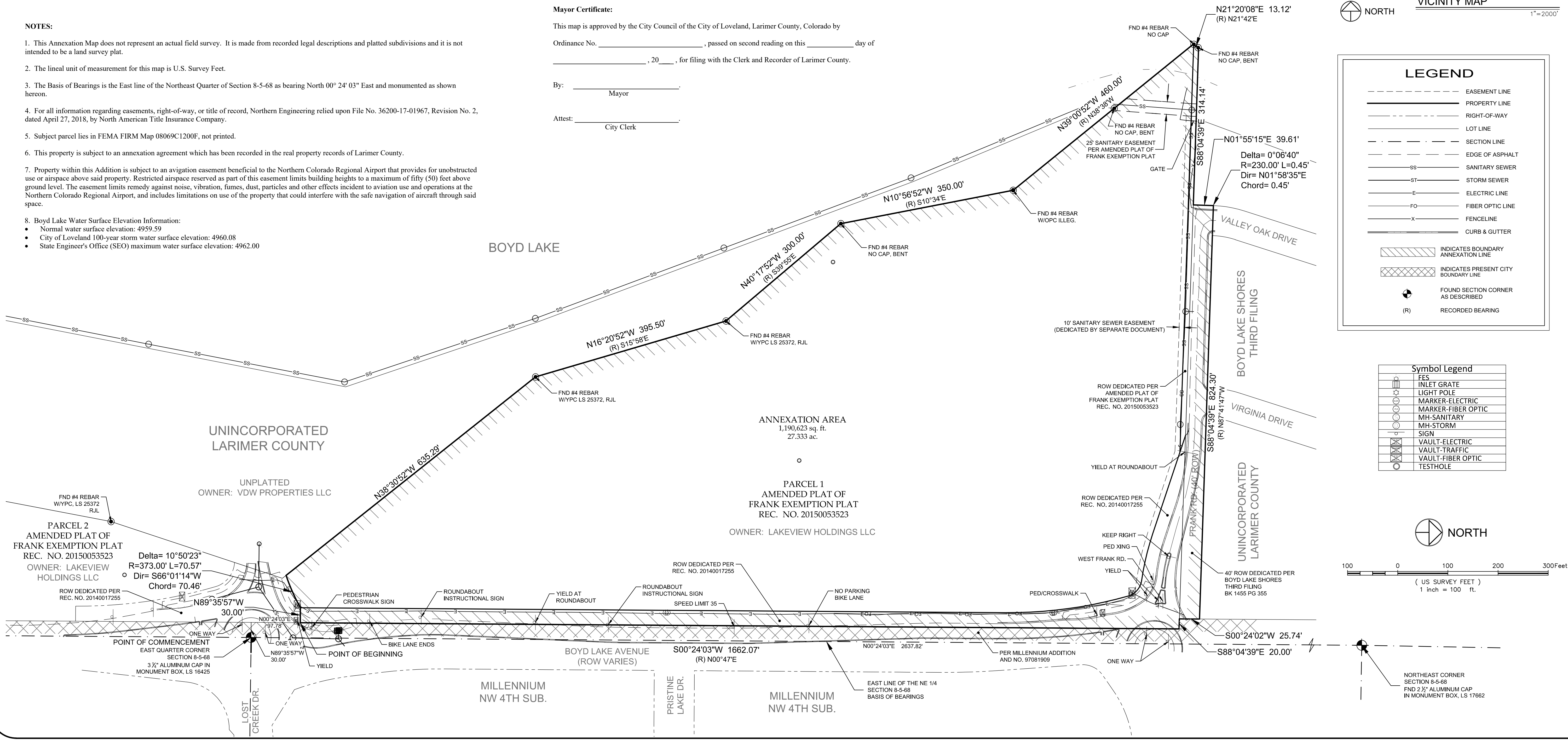
Mayor Certificate:

This map is approved by the City Council of the City of Loveland, Larimer County, Colorado by

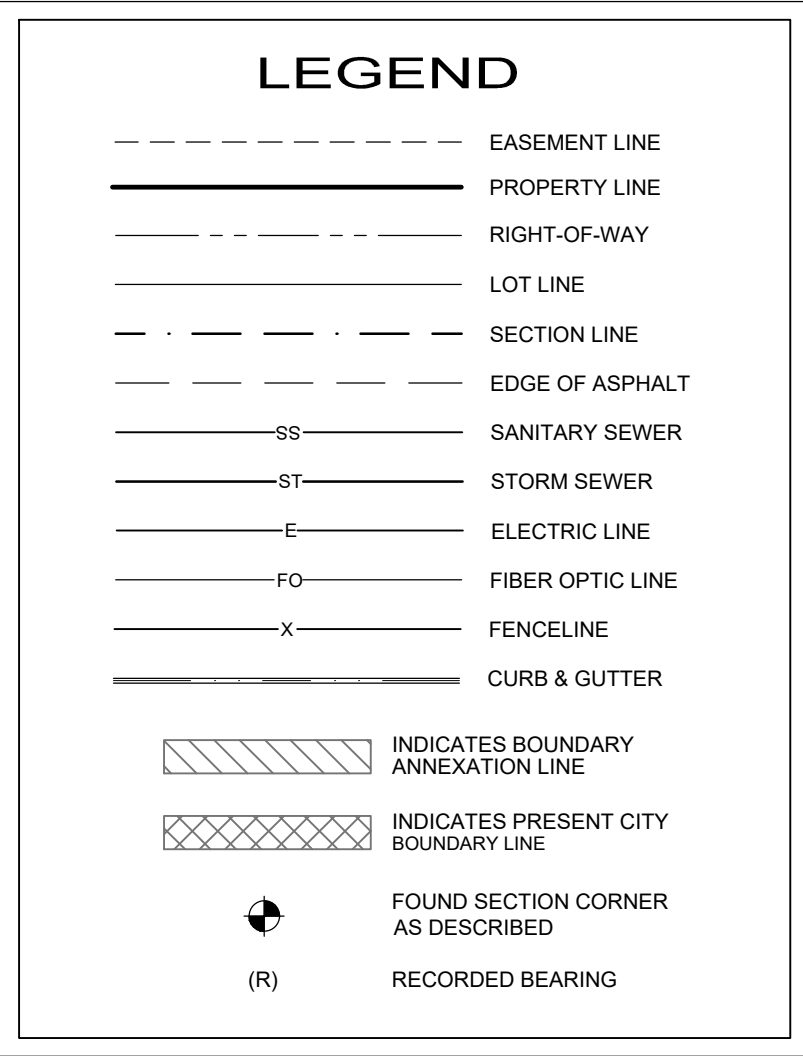
Ordinance No. _____, passed on second reading on this _____ day of _____, 20____, for filing with the Clerk and Recorder of Larimer County.

By: _____ Mayor

Attest: _____ City Clerk



NORTH
VICINITY MAP
1"=2000'



Symbol Legend	
	FES
	INLET GRATE
	LIGHT POLE
	MARKER-ELECTRIC
	MARKER-FIBER OPTIC
	MH-SANITARY
	MH-STORM
	SIGN
	VAULT-ELECTRIC
	VAULT-TRAFFIC
	VAULT-FIBER OPTIC
	TESTHOLE



100 0 100 200 300 Feet
(U.S. SURVEY FEET)
1 inch = 100 ft.

NOTICE:
According to Colorado law you must commence any legal action based upon any defect in this survey within three years after you discover such defect. In no event may any action based upon any defect in this survey be commenced more than ten years after the date of the certificate shown hereon.

SECTION:
8
TOWNSHIP:
5N
RANGE:
68 W of the 6th PM

**NORTHERN
ENGINEERING**

301 North Howe Street, Suite 100
Fort Collins, Colorado 80521
PHONE: 970.221.1158
www.northernengineering.com

DATE:
6/14/18
PROJECT:
884-006
DESIGNED BY:
L. Smith
DRAWN BY:
L. Smith
REVIEWED BY:
R. Tessely
SCALE:
1"=100'

BOYD LAKE PLACE ADDITION
SECTION 8, T5N, R68W
CITY OF LOVELAND, COLORADO

Sheet
1
Of 1 Sheet

BOYD LAKE PLACE PRELIMINARY DEVELOPMENT PLAN

PARCEL 1, AMENDED PLAT OF FRANK EXEMPTION PLAT ACCORDING TO THE PLAT THEREOF RECORDED ON AUGUST 13, 2015 AS RECEPTION NO. 20150053523 IN THE REAL ESTATE RECORDS OF LARIMER COUNTY, COLORADO.

Approval Signatures

- a. Approved this _____ day of _____, 2018__, by the Current Planning Manager of the City of Loveland, Colorado.
- _____
Current Planning Manager
- b. Approved this _____ day of _____, 2018__ by the City Engineer of the City of Loveland, Colorado.
- _____
City Engineer
- c. Approved as to form this _____ day of _____, 2018__ by the City Attorney of the City of Loveland, Colorado.
- _____
City Attorney
- d. Approved this _____ day of _____, 2018__ by the City Planning Commission of the City of Loveland, Colorado.
- _____
Chairperson
- e. Approved this _____ day of _____, 2018__ by the City Council of the City of Loveland, Colorado.
- _____
Mayor
- _____
Attest

Owners Certification

KNOW ALL PERSONS BY THESE PRESENTS THAT: Lakeview Holdings, LLC
A Colorado Limited Liability Company

Being all the lawful recorded owners of the property shown on this Boyd Lake Place
Preliminary Development Plan, except any existing public streets, roads, or highways, do hereby certify that I/we accept the conditions and restrictions set forth on said plan and in the conditions of approval by the City of Loveland, dated _____, and that I/we consent to the recordation of any information pertaining thereto.

Owner: Lakeview Holdings, LLC
A Colorado Limited Liability Company
Marcus Palkowitsh, Manager

STATE OF COLORADO)
)ss
COUNTY OF)

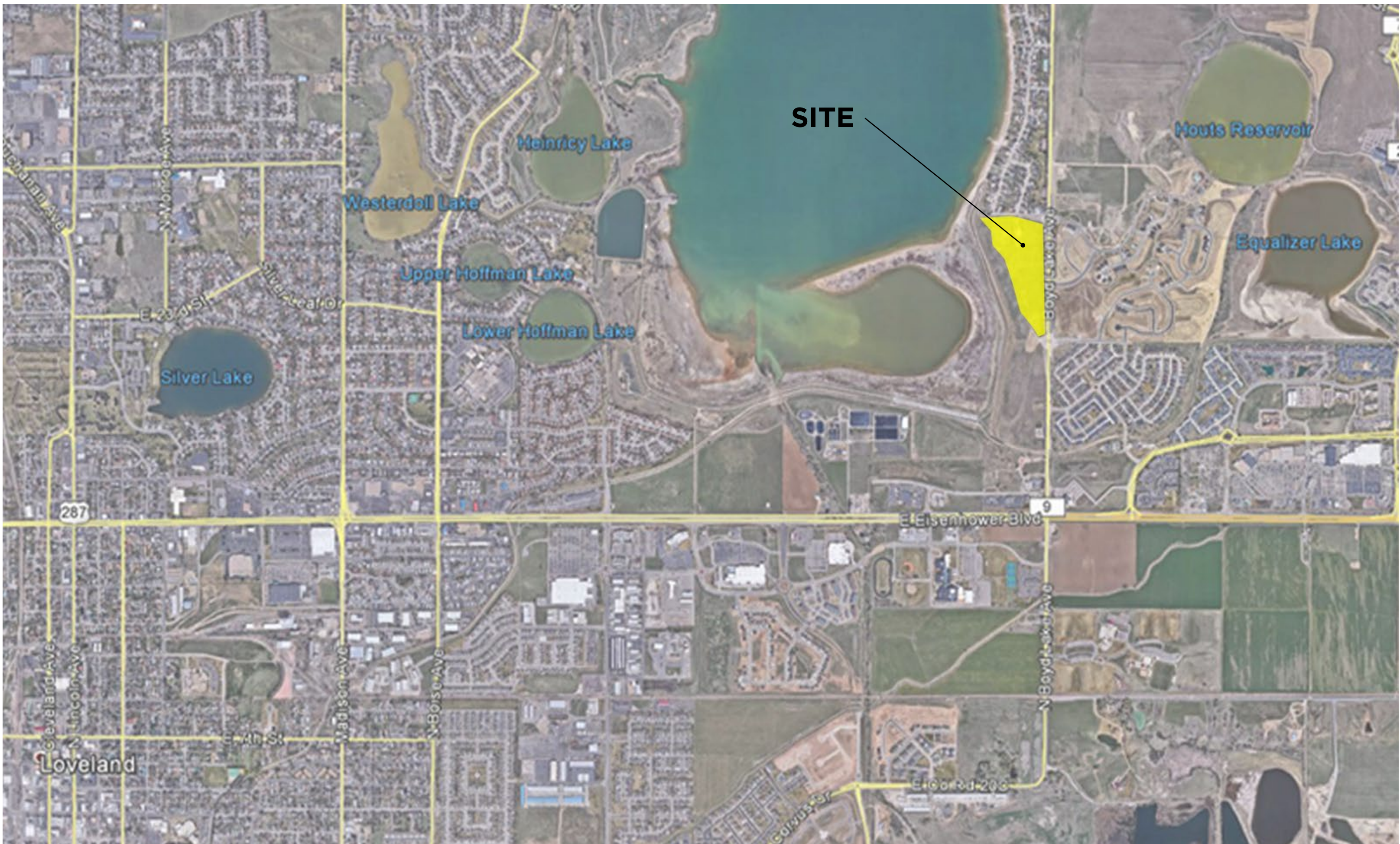
The foregoing instrument was acknowledged before me this _____ day of _____, 2018__, by _____.

Witness my hand and official seal.

My commission expires: _____

Notary Public

Vicinity Map





Property Size: 24.69 Acres
Total Area of Open Space: 5.93 Acres
Minus Detention Areas: .75 Acres
Minus Buffer Yard at Boyd Lake Ave.: .8 Acres
Open Space Amenity/Trail = 4.38 Acres
Requirement of 1 Acre/100 lots.
This site 4.38 Acres/98 Lots

- ★ Location of Amenity Features
- Possible Location for Mail Kiosk
- ✳ Possible Location for Doggie Pots
- ✳ Possible Location for Interpretive Signage

Fencing Legend:	
	Masonry Wall
	Synthetic Wood Fence
	Two-Rail Synthetic Wood Fence
	Masonry Column
	Synthetic Wood Fence Post

Note: Prior to the issuance of the first building permit, all common area landscaping, landscaping in the right-of-way and tree lawns, fencing, buffer yards, and amenities shall be either installed or financially secured with the city.

PDP Land Use Information/Area	
Gross Acreage	27.333 Acres
Net Acreage	24.696 Acres
Height Limitations	26'
Parking Requirements	2 Spaces Per Dwelling Unit
Maximum # of Units	98 Units
Lot Sizes	Average = 5500 SF
Gross Density	3.6 Units / Acre
Net Density	4.0 Units / Acre
% Open Space Provided	24%

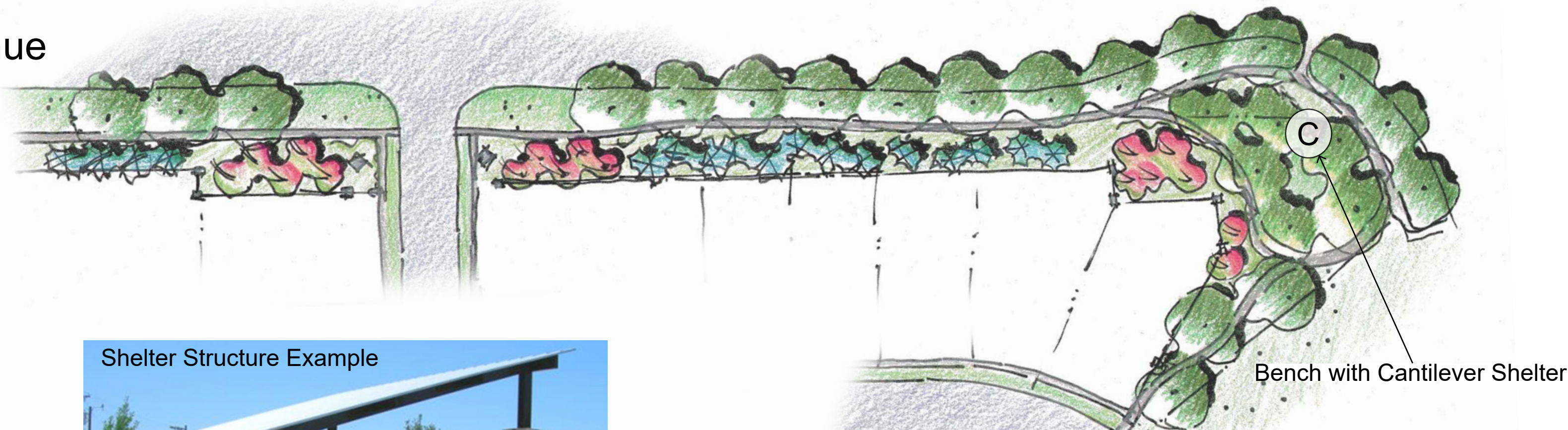


LAKEVIEW HOLDINGS, LLC.

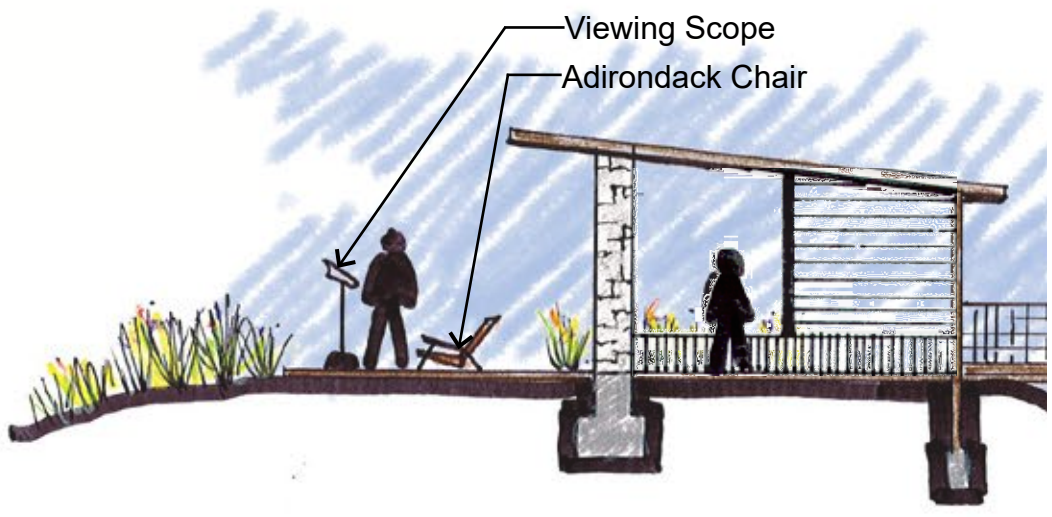
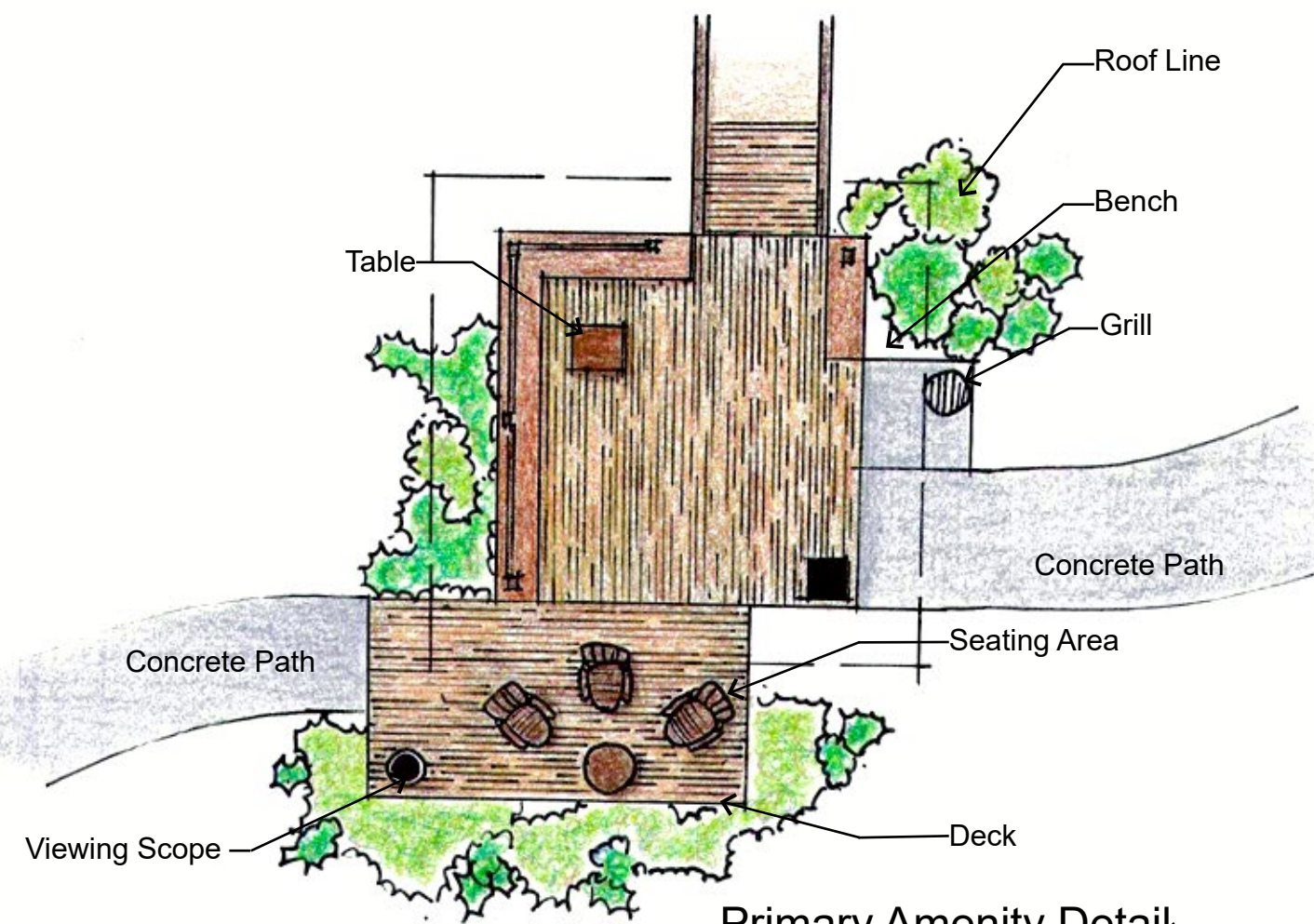
BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
CONCEPTUAL SITE PLAN

ISSUE DATE:	5/16/18	PROJECT #:	215026
DATE	REVISION	COMMENTS	
6/14/18	1st	Resubmittal	
7/12/18	2nd	Resubmittal	

Boyd Lake Avenue



Frank Road



Primary Amenity Cross-Section



Horseshoe Pit with Open Space

Putting Green

Bocce Lawn with Open Space

Concrete Path

Respite with Bench and Retaining Wall

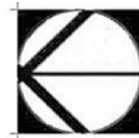
OPEN SPACE ACTIVITIES:

- Badminton
- Tai chi
- Volleyball
- Frisbee
- Catch (softball, baseball)
- Picnic
- Host birthday parties/celebrations
- Take the dog out
- Read a book
- Tan/lounge
- Visit with friends/family
- Cricket
- Walk/Jog
- Football
- Enjoy the views
- Observe wildlife
- Fly a kite
- Yoga
- Bird watching
- Sketching/painting
- Soccer
- Photography
- Arts and crafts
- Metal detecting
- Garden Party

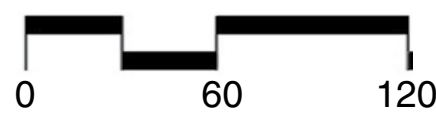
PERIPHERAL PATH DISTANCE: 1.0 Mile

PROGRAMED ACTIVITIES:

- Bocce Ball
- Horseshoes
- Putting Green
- Viewing Scope for Bird Watching
- Viewing Decks
- Covered Gathering Spaces
- Picnic Tables with Grill and Trash Receptacles
- Adirondack Chairs for Lounging and Viewing
- Walking/Jogging Path with Mile Markers
- Respite with Seating
- Framed Views of Mountains and Lake
- Educational Signage
- Bike Path



NORTH



LAKEVIEW HOLDINGS, LLC.

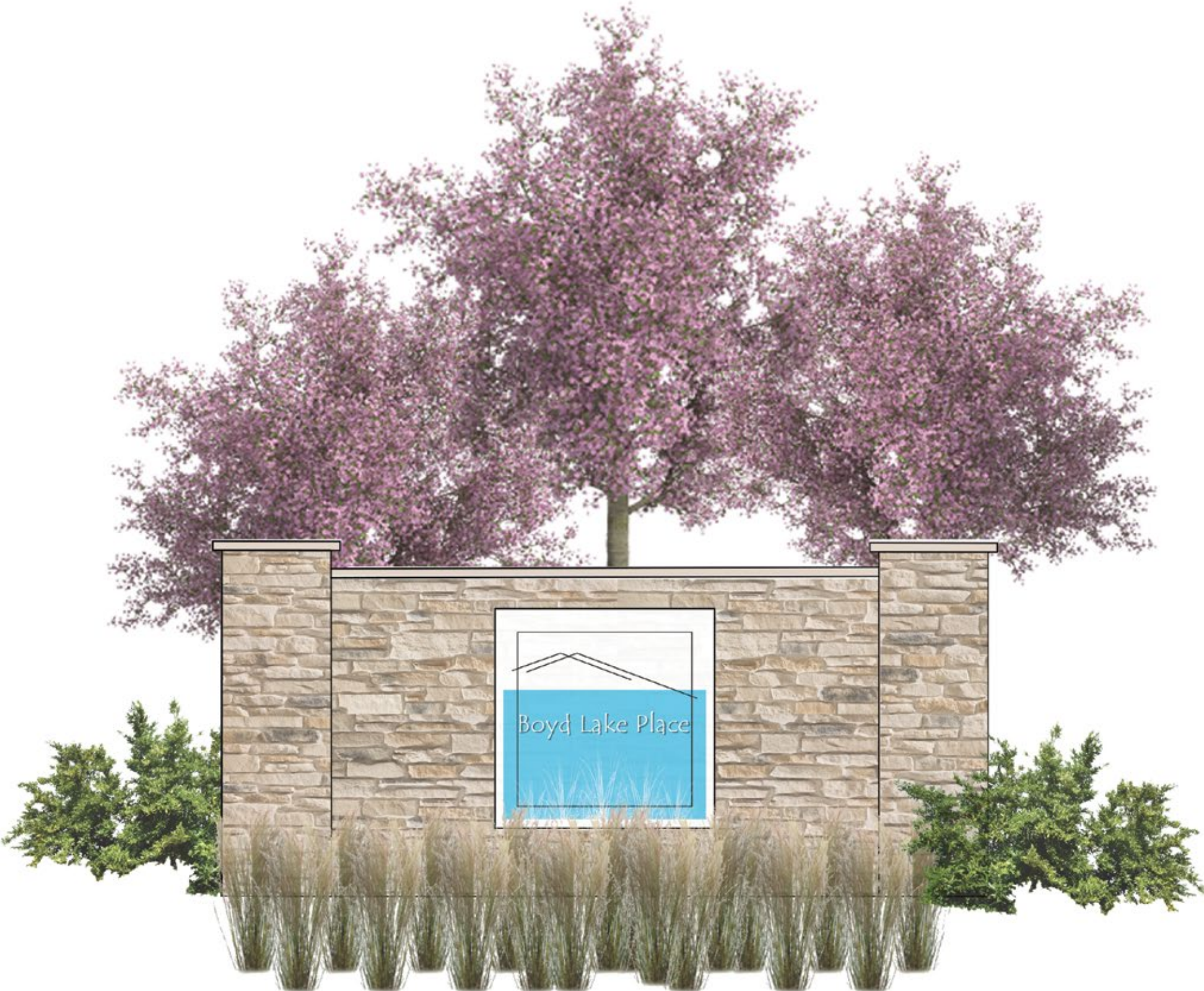
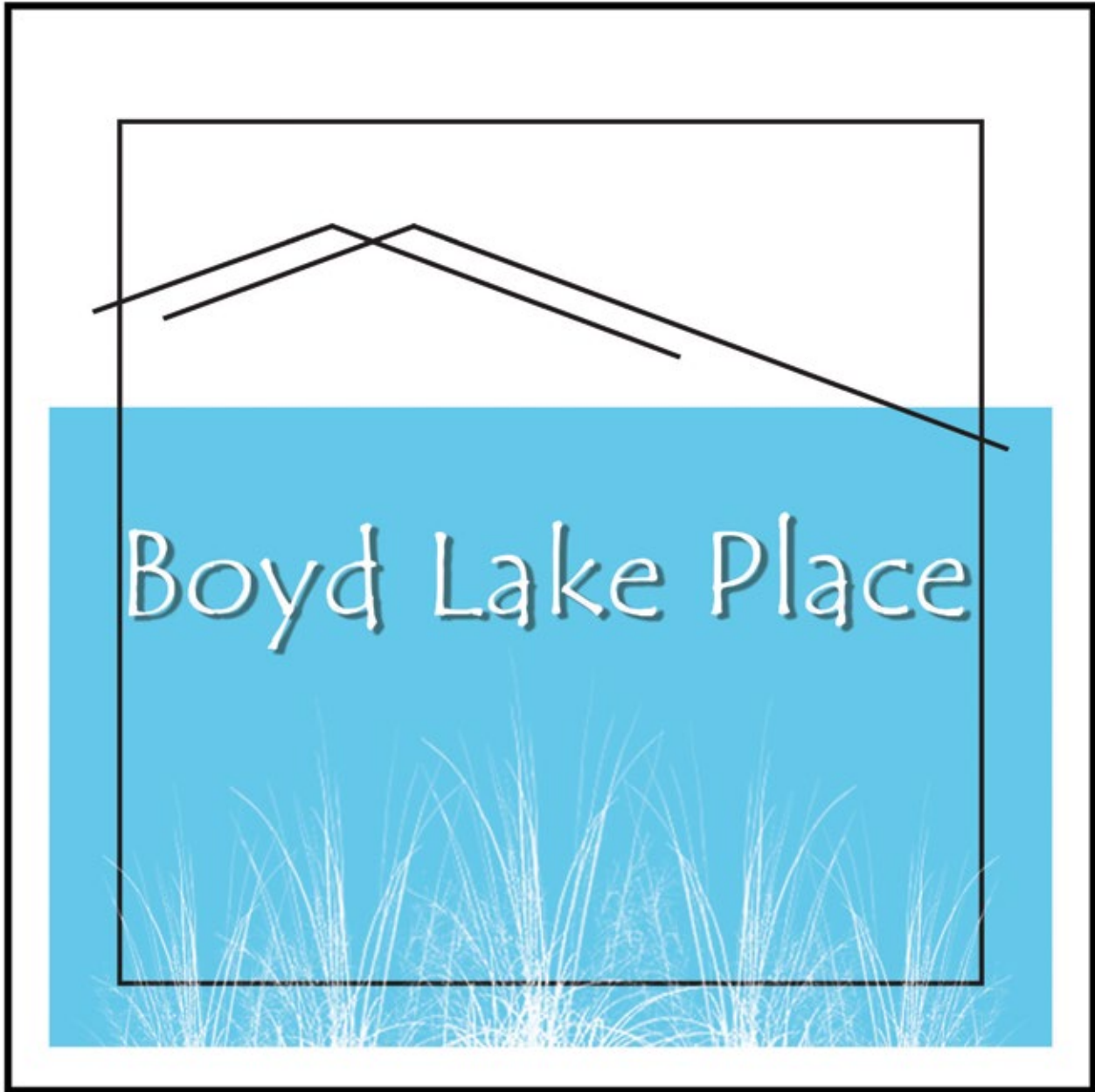
BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
SITE AMENITIES PLAN

DATE	REVISION COMMENTS
6/14/18	1st Resubmittal
7/12/18	2nd Resubmittal

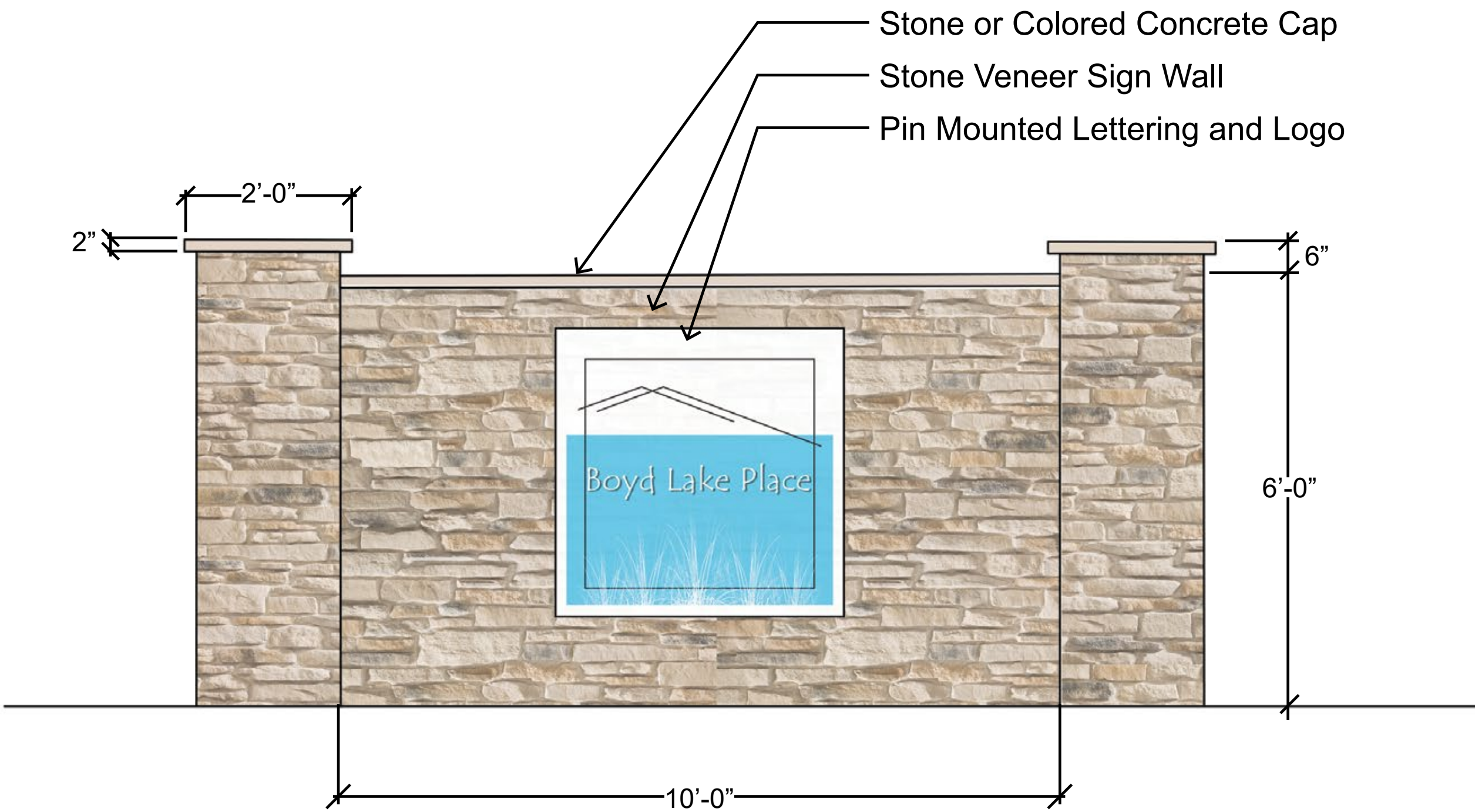
SHEET NO.
3
3 OF 15

Primary Entry Signage

Project Logo



Stone Veneer Signage Wall Detail



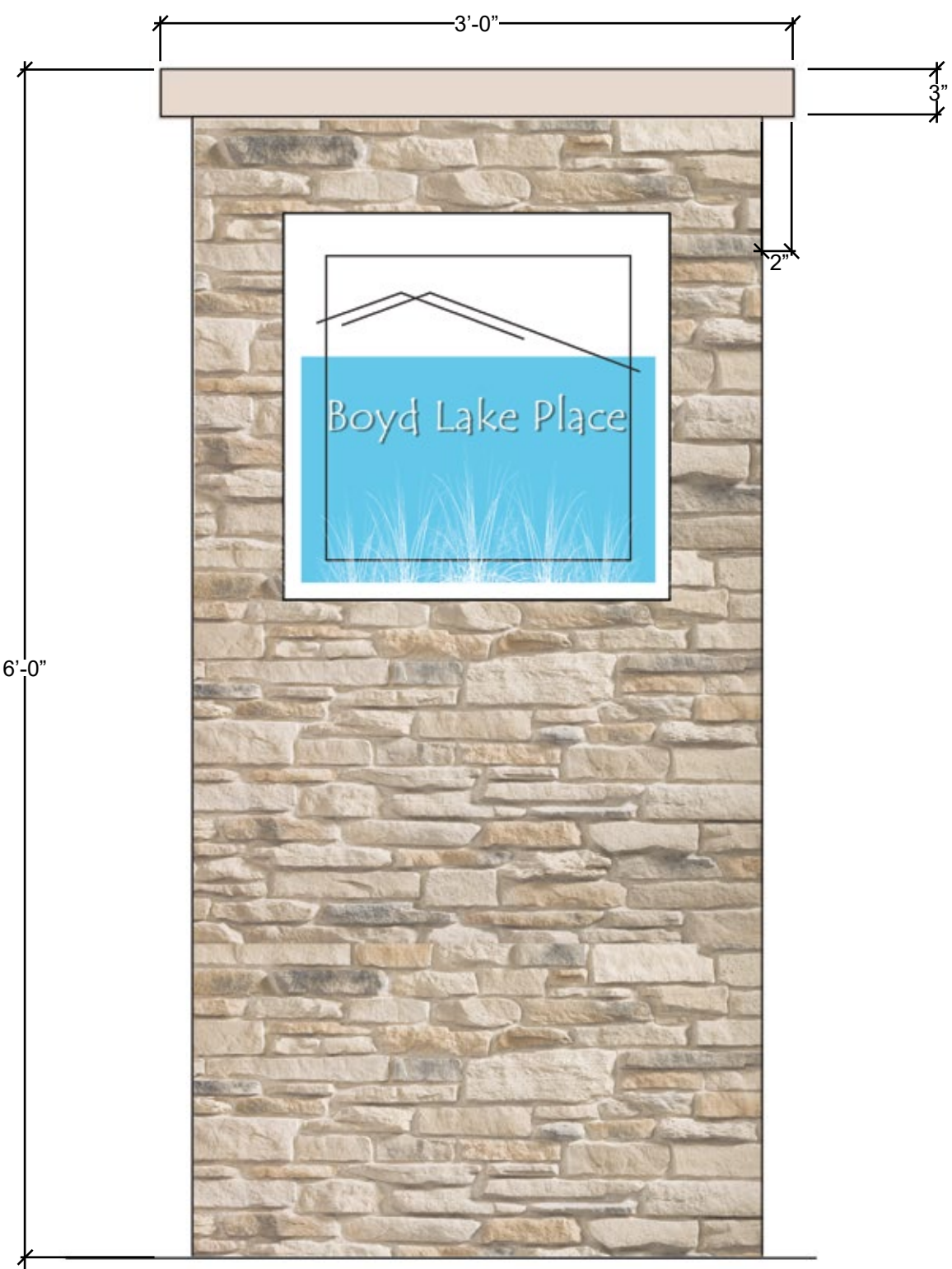
Perimeter Stone Column and Fencing



Two-Rail Fence Detail



Secondary Entry Signage



Project Narrative

A. Purpose of the Project

The purpose of this Preliminary Development Plan (PDP) submittal is to provide the necessary information to allow the City of Loveland staff, Planning Commission and City Council to review the Preliminary Development Plan and resolve any planning issues to permit the project and to then proceed through Final Development Plan approvals.

The purpose of this PDP is to establish the goals, guidelines, and overall design concepts for the annexation and development of 24.69 acres that will comprise Boyd Lake Place, a single-family age-targeted active adult residential community.

The City’s Future Land Use Plan designates this property as a site for low-density residential (LDR) development. The Boyd Lake Place annexation/PUD/PDP request is consistent with the City’s Comprehensive Plan’s strategic policies, supporting strategies and zoning regulations. Accordingly, the City will benefit from having this parcel incorporated into the City for the following reasons:

- The project offers an alternative housing product, which is vital for the City to preserve its position as an attractive place to live and support its economic and development goals.
- The project provides attractive streetscapes and trails to improve connectivity to surrounding areas and regional amenities through the completion of the west side of Boyd Lake Avenue, including bike lanes and pedestrian walkways.
- The property will be developed in a manner that will protect Boyd Lake and provide ample open space for all residents in the community.
- A variety of community amenities, recreational spaces and close proximity to high-quality healthcare options will attract active adults who in turn will help support existing regional retail and businesses along Highway 34.

The application request does not interfere with, prevent, or infringe upon any of the area’s existing, planned, previously committed services or proposals for community facilities, or other specific public or private actions contemplated within the City’s Comprehensive Plan. This application request compliments and completes portions of the area’s existing and planned transportation system services as contemplated by the City Transportation Plan.

- The development will provide infrastructure to make all modes of transportation—whether vehicular, biking or walking—convenient and viable for all ages, abilities, and income levels.
- The development will establish and maintain convenient connections between neighborhoods and to surrounding streets and destinations.
- The project will provide for the completion of the roadway, bike lanes and pedestrian walkway improvements along the west side of the existing Boyd Lake Avenue adjacent to this project.

B. Site Introduction / History

Boyd Lake Place is a very attractive property overlooking Boyd Lake and offering beautiful mountain views. The property is located north of Highway 34 on the west side of Boyd Lake Avenue.

The geographical location of the site is in Section 8, Township 5 North, Range 68 West, County of Larimer, Colorado. This project is located on 24.69 acres. The property is owned by Lakeview Holdings, LLC.

The subject parcel is located at the southwest corner of the Frank Road and Boyd Lake Avenue intersection, approximately a half mile north of US Highway 34. The parcel is bordered to the north by Boyd Lake Shores subdivision, a single-family residential community located within the Larimer County jurisdiction. To the property’s west is a linear narrow tract of land lying east of the Greeley-Loveland Irrigation Ditch and runs parallel to the property boundary. Beyond the ditch to the west is existing open land and Boyd Lake. The property is bordered on the east by Boyd Lake Avenue along with the newly constructed High Plains School (K-8) and a two to three story, multi-family, medium-density residential (MDR) community. The project limit to the south is the roundabout at Lost Creek Drive.

The project’s plan includes landscaping of open areas with regionally appropriate plant materials. Landscaping along Frank Road and Boyd Lake Avenue will follow the City of Loveland’s current corridor plan for the area. In open space areas and buffer yards, a naturalistic design approach will be incorporated into the design. This area will incorporate low-water, drought-tolerant, formal and informal plantings of trees and shrub beds using a Hydrozone Plan (Hydrozone Plan) as defined in the City of Loveland Municipal Code chapter 19.06. Entry project monumentation will be located at the entrances of the project on Frank Road and Boyd Lake Avenue. The Boyd Lake Homeowner’s Association (“the HOA”) shall maintain the open space area plantings, entry wall plantings, and landscape areas adjacent to all arterial streets. In addition, recreation areas will be constructed by the Developer, and maintained by the HOA in conformance with the City of Loveland’s requirements.

C. Site Design Concept

1. Overall Design Concept

The goal of this development is to create an active adult community offering compatible housing within an area of mixed-use development. The development concept is to provide a high quality, sensitively designed single-family residential community within the City of Loveland to help meet the current and future housing market demand. The project will consist of 98 single-family residential lots and will not include any commercial, retail, or office uses. These lots will be targeted to an active adult demographic. The community will offer low-maintenance active adult living at an attractive value.

The development introduces a new kind of healthy and livable adult community within the existing regional context. This community will provide a safe, secure, walkable environment, housing options, opportunities for recreation, and is near high-quality health facilities. Ample open space and a trail/sidewalk network within the development and along Boyd Lake Avenue provide residents with a variety of active and passive recreational options. This new community provides appropriately designed homes in a socially and environmentally sustainable community for adults who are already living in the Loveland area, or who may be moving here in the coming years.

The Boyd Lake Place land use plan is based on the belief that active adults no longer need or want to maintain a large house and back yard; active adults would prefer to travel, learn, recreate, socialize, pursue hobbies, and give back to the community. The community plan features lots with minimal maintenance, allowing residents more time to enjoy the nearby and on-site amenities provided.

Boyd Lake Place homes are designed and planned for aging in place with universal (accessible) design concepts. All homes offered are primarily a single-level ranch-style product with an optional second story walk-up attic or a standard two-level product with additional bedroom and living space. The product types offered range in a wide variety of types and sizes. These housing options allow Loveland’s active adult residents to continue living in their community, as well as accommodate residents new to the area.

Boyd Lake Place affords a rare opportunity to create a socially, environmentally and economically sustainable residential community. The project’s development concept provides a high quality, sensitively designed residential community within the City of Loveland to help meet the current and future market demand for housing in this area. Annexing and zoning the property as a PUD will blend the parcel within the existing context and fulfill the Comprehensive Plan’s strategic plan policies and supporting strategies.

2. Parcel Concepts

The total development area of the project submitted as a Preliminary Development Plan is 24.69 acres. The total developed land, including Right-of-Way (ROW) areas is 27.3 acres. The overall proposed maximum gross density of the site is 3.6 dwelling units per acre and the maximum net density is 4.0 dwelling units per acre.

3. Land Use Categories and Definitions

The land use category in this Plan is for single-family dwelling units. Other ancillary or accessory uses relating to single-family residential units will be allowed, including but not limited to garages, mail kiosks, community facilities and related recreational amenities and passive open space. The City of Loveland Zoning Code definitions of R-1 shall be applied for this project description in addition to the following:

- Single family dwelling: A one family dwelling not attached to any other dwelling, located on individual fee-simple lots or in clusters. These homes may be of conventional construction.
- Building coverage: The area of land that a built structure covers.
- Gross Area: That area which includes all the land within the boundary of annexation.
- Net Area: That area of land within the boundary of annexation and excluding any rights-of-ways and easements.
- Infrastructure: Shall mean those man-made structures which serve the common needs of the population such as, but not limited to, potable water systems, non-potable water systems for irrigation, wastewater disposal systems, solid waste disposal sites or retention areas, storm drainage systems, utilities, bridges, roadways, bicycle paths, and trails.
- Open Space: The gross area of a lot or tract of land minus all streets, driveways, parking areas, and habitable building areas which is to be or has been landscaped or developed for use by the public or by the residents of the lot, tract of land, or neighborhood for private, common, or public enjoyment or recreational use.
- Recreational Space: Shall mean space that is designed for active or passive recreational use for more than three (3) families.

Allowable Land Uses.....Single-Family Residential
Existing Zoning.....Larimer County – FA (Farming)
Legal Property Area.....24.69 acres
Gross Area of Annexation.....27.3 acres
Maximum Number of Dwelling Units.....98 units
Maximum Gross Density.....3.6 DU/AC
Maximum Net Density.....4.0 DU/AC
Open Space.....24%
Height Limitations.....26 ft. primary dwelling units

- Permitted Use:
- Agricultural. Open land for the raising of crops, plants or flowers.
 - Residential. Single family detached dwelling.

Density/Intensity
Single-Family detached dwellings. Gross density shall not exceed four (4) dwelling units an acre or 98 residential lots.

- Dimensional Standards
- a. Single-Family Detached Dwellings: Average lot size of five thousand five hundred (5,500) square feet but not less than four thousand five hundred (4,500) square feet.
 - b. Typical Lot Dimension: Fifty (50) feet by one hundred ten (110) feet. Minimum lot width shall be thirty (30) feet at the right-of-way for an irregular shaped lot.
 - c. Building Structure Height. No buildings or structures shall exceed a height of twenty-six (26) feet.
 - d. Building Coverage. Maximum building coverage shall not exceed fifty (50) percent of the total lot area.

4. Neighborhood Structures

As shown on the PDP site plans, all proposed land uses within the PDP are for single family residential. Residential streets and open space will generally bind the residential neighborhoods. Landscaped setbacks and buffer zones will be established along Boyd Lake Avenue. The number of dwelling units will be allowed as shown on the Preliminary Development Plan (PDP) and will be designed in accordance with the development standards outlined in this plan.

5. Wetlands and Wildlife

The existing land use is agricultural. The topography is primarily flat, sloping gently to the west. No trees or shrub communities, streams, creeks, ponds, or seeps are within the property boundaries except for a small wetlands area in the northwest corner within the Greeley/Loveland Irrigation Company canal. The wetlands are limited to the extreme northwest corner of the property. The Environmentally Sensitive Areas Report found that “these wetlands occur as a narrow, intermittent fringe along the Greeley-Loveland Irrigation Company canal, and total less than 0.005 acre.” There are no plans to disturb these wetlands as part of the proposed development. The Environmental Assessment Report found that there are “no known special habitat characteristics present,” and the property has “limited wildlife habitat value.” An environmental assessment and wetland survey, containing further details for the property, has been prepared by Western Environmental Consultants, and has been approved by the City of Loveland. U.S. Fish and Wildlife Service has confirmed it has no concerns regarding Preble’s Meadow Jumping Mouse habitat.

D. Circulation

1. Overall Concept

Vehicular: The site will have two access points from Boyd Lake Avenue, and one access point from Frank Road on the northern boundary of the site. A potential future roundabout connection at Lost Creek Drive and Boyd Lake Avenue will be provided along the southern boundary of the site. The neighborhood circulation will consist of public local internal streets and drives. A summary of the proposed street improvements are as follows:

Street	Width FL-FL	Width ROW	Classification	Curb	Sidewalk	Bike
Boyd Lake Avenue	Varies	120’	4 Lane Arterial	Vertical 2’-6”	6’ Detached	7’
Frank Road	29’	50’	Residential Local	Roll-over 2’-6”	5’ Detached (South Side Only)	N/A
Interior Roads	34’	50’	Residential Local	Roll-over 2’-6”	5’ Detached	N/A

The street improvements are designed to facilitate vehicular, pedestrian and bicycle movements through the area. The existing Boyd Lake Avenue 4-lane roadway cross section varies in width. Boyd Lake Avenue is constructed as a 4-lane arterial street with a 120’ full width Right-of-Way (ROW) section (60’ half ROW) to replace the existing roadway from Frank Road to the southern boundary of the site. Coordination of this work effort will be completed as part of the project development process. This portion of Boyd Lake Avenue is proposed to consist of: four 12’ wide travel lanes, a raised center median, the Larimer County Urban Area Street Standards required auxiliary lane(s), and 7’ bicycle lanes on both sides. A 6’ detached meandering sidewalk separated a minimum of 10’ from the curb will provide for safe pedestrian and bicycle circulation. Frank Road is currently a Larimer County local street approximately 30’ wide with a 2’- 6” wide valley pan on the north edge and no valley pan on the south edge. A 2’- 6” roll-over curb and a 5’ detached sidewalk will be added to the south side of the road. The total width of the roadway including curb and gutters will be 29’. This will facilitate driveways for residential lots which will front on to the street.The street section will provide for two 12’ travel lanes with combined parking and bicycle lanes on both sides. All

interior streets will be within a public right-of-way.

Pedestrian and Bicycle Paths: The residential neighborhood will provide an internal pedestrian circulation system in conjunction with the internal public streets, and a pedestrian walkway along the neighborhood’s western boundary shall connect with the public walkway adjacent to Boyd Lake Avenue. The project provides attractive streetscapes with detached sidewalks and trails to improve connectivity to surrounding areas and regional amenities through the completion of the west side of Boyd Lake Avenue, including bike lanes and pedestrian walkways.

Public Transportation: No public transportation is planned to enter onto the site. Future bus stops may be adjacent to this site, as planned by the City transportation system.

E. Public and Private Utilities

1. Water and Sewer

Water and sewer service will be provided by the City of Loveland. The water service for this site will be provided by 8” and 16” diameter waterlines looped through the site connected to a 16” and 24” diameter waterline in Boyd Lake Avenue. The sanitary sewer for this site will be provided by 8” and 15” diameter sanitary sewer mains through the site connected to an existing 10” diameter sanitary sewer that is located at the northwest corner of the site, adjacent to the Greeley-Loveland Irrigation Canal.

2. Drainage Concepts

This project is within the Boyd Lake Drainage Basin and the Outlet Mall Drainage Basin according to the 1987 City of Loveland Master Drainage Plan. Storm drainage for this site will channel to two separate detention ponds located adjacent to the western property line. These ponds will provide adequate detention for the 100-year storm levels with water quality included and will release at the 2-year historic flows. Flows generating from some areas around the perimeter of the site will have to drain undetained and therefore will be kept to a minimum. Rooftop, yard, and common area surface flows will be directed towards adjacent streets or adjacent green space where it will be collected by either a flared end section or a sumped inlet. There will be one ultimate release point for the entire site at the northwest corner of the property where the site will release runoff into the existing drainage lateral that runs north to south along the western property line.

3. Electric

Electric service will be provided by the City of Loveland Light and Power Department. Easements will be provided for the installation of underground electric, gas and other utility services within the project. One light fixture will be installed at the intersection of Frank Road and will include a full cut-off fixture to direct all light in a downward direction.

4. Gas

Natural gas will be provided by Xcel Energy Company. Easements will be provided for the installation of underground electric, gas and other utility services within the project.

5. Telephone

Telephone service will be provided by CenturyLink and other providers as available.

6. Cable

Cable services will be provided by Comcast and other providers as available.

7. Police and Fire

The development will be serviced by the Loveland Fire and Police Departments with service areas as determined to be appropriate. Street widths are designed to accommodate emergency vehicles. The Boyd Lake Place street system is designed to provide interconnectivity to the adjacent streets in the existing subdivisions. This interconnectivity of streets is important to the Loveland Fire and Police Departments’ ability to provide quick response times within the area.

8. Schools

The development will be serviced by the Thompson R2-J School District, specifically the following schools:

- Mountain View High School
- High Plains School k-8

9. Amenity Areas

The development does not include any public park facilities. Open space and recreation areas will be provided for residents of the Boyd Lake Place community at a minimum ratio of one acre for every 100 dwelling units. A community trail system will loop the entire development connecting it with the passive and active recreation areas throughout the Boyd Lake Place development.

Amenity areas include:

- A primary gathering space with covered seating and views to Boyd Lake and the Rocky Mountains, bocce ball lawn, horseshoes pit, and open space. The primary gathering space will have viewing scopes for birdwatching, informative signage, and grills.
- A secondary gathering space with a putting green and open space. The secondary gathering space will include a large seating area.
- A respite with seating and landscaping
- Picnic tables with charcoal grills and trash receptacles
- A cantilever shelter for school kids and the crossing guard
- A lean-to shelter within open lawn and shade trees.
- Doggie pots throughout the site for convenience and to promote a clean environment.
- Open space areas can be used for a variety of activities such as:
 - o Badminton
 - o Tai Chi
 - o Volleyball
 - o Frisbee
 - o Catch
 - o Picnics
 - o Birthday parties and celebrations
 - o Dog walking
 - o Reading a book
 - o Tanning or relaxing
 - o Visiting with friends/family/neighbors
 - o Cricket
 - o Football
 - o Soccer
 - o Enjoying the views, observing wildlife
 - o Flying a kite
 - o Bird Watching
 - o Sketching/painting
 - o Photography
 - o Arts and Crafts, and much more.



KEPHART
community ■ planning ■ architecture

LAKEVIEW HOLDINGS, LLC.

BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
NARRATIVE

ISSUE DATE: 5/16/18	PROJECT #: 215026
DATE	REVISION COMMENTS
6/14/18	1st Resubmittal
7/12/18	2nd Resubmittal

SHEET NO.

5

5 OF 15

E. Landscaping and Fencing

1. Overall Concept

The landscaping will be designed with regionally appropriate low-water, drought tolerant plant materials using a Hydrozone Plan. In open space and buffer yards, a naturalistic approach will be incorporated into the design. These areas will incorporate informal plantings of trees and shrub beds. The open space area plantings and entry wall plantings shall be maintained by the HOA. Stone masonry or stucco entry features will be located at the entrance to Boyd Lake Residences on Boyd Lake Avenue. Other site items such as mailboxes and street markers will adhere to a similar design theme as the entry features.

The Boyd Lake Place landscape plan is based on lake and mountain view living, connection and visual unity throughout the project. Fencing will not detract from the overall design, but rather will be used as a design element where desired, and as a screen for any element which would detract from the overall image of the project.

The landscape design intent for Boyd Lake Place takes inspiration from regionally appropriate plant materials. The plant palette and implementation will include uplands prairie plants. There are unobstructed views to the west over Boyd Lake within the development allowing for open vistas. Landscaping adjacent to roadways will be less naturalistic, and more formal with pine and shade trees, and groupings of shrub masses. The main entry into Boyd Lake Place will include entry signs, accompanied with ornamental and evergreen trees, and plantings of annuals, perennials and ornamental grasses. Natural areas to be reestablished will receive seed mixes and be allowed to naturalize again. Areas designated for recreation and adjacent to roadways will have a higher level of maintenance, using trees, shrubs, perennials, mulches, and grasses with inground automated Hydrozone irrigation system.

The Developer will install all common area landscaping and perimeter and common area fencing per the approved Landscape Plan for Boyd Lake Place.

2. Edge Treatment

The edge treatment of the project shall be designed to be in conformance with the required buffers and landscape design as per the City of Loveland Site Development Performance Standards. Special attention will be given to the area adjacent to Boyd Lake Avenue. Fencing with landscaping along Boyd Lake Avenue will be used to buffer the development as well as enhance the visual appearance along this roadway.

3. Open Space Edge Treatments

The edge treatment and open space areas adjacent to lots will be treated as an enhanced native landscape. The planting will be an uplands prairie plant palette. Native tree, shrub, grass, and forb species will be emphasized in the western open space areas adjacent to Boyd Lake, to reduce water usage and improve habitat for wildlife. The areas of open space along Boyd Lake within the development allow for open vistas, and the opportunities to provide recreational areas. Throughout the landscaped areas some additional tree planting will be supplemented with the native shrub plantings to provide shade, habitat, and visual appeal to the open areas. The edges of the area adjacent to the west boundary will have a two-rail vinyl fence. Fences will be restricted to a height of 3'-0" and will be regulated by Final Development Plan and HOA design guidelines.

4. Buffer Yards / Arterial and Collector Streetscape

A Type D buffer yard will be provided along Boyd Lake Avenue. This buffer yard will be a minimum of twenty feet (20') deep from the right-of-way. Lots which back to Boyd Lake Avenue will have a six-foot-high vinyl privacy fence and stone columns along the rear property line of the adjacent lot. The buffer yard and right-of-way will be landscaped using shrub massing, grasses, and a variety of trees. The buffer yard will include naturalistic shrub massing which will provide seasonal color and interest to the landscape. Evergreen trees will be located throughout the grass areas, and flowering trees will accompany the shrub areas. The plant material densities will conform to the buffer yard requirements as outlined in the Site Development Standards.

5. Installation Schedule

The tree lawn landscaping for all single-family lots shall be installed by the developer. The landscaping for the lots shall have a minimum of one (1) tree spaced no more than 35 feet apart for interior lots located in the front yard and two (2) street trees for corner lots located in the front and street-side yards. Use of native plant material is encouraged. The landscaping requirement on each lot shall include the right-of-way and easements located on or adjacent to the lot which shall be maintained by the homeowner, subject to the right of the HOA to enter and perform any maintenance deemed necessary or desirable to maintain the high HOA standards established for Boyd Lake Place. Prior to the issuance of the first building permit, all common area landscaping, landscaping in the right-of-way and tree lawns, fencing, buffer yards, and amenities shall be either installed or financially secured with the city.

When reviewing landscaping and fencing plans, the HOA shall at a minimum adhere to the City of Loveland requirements and use the Final Development Plan and the Landscape and Fencing Plan approved by the City. Individual lot fencing is optional at the discretion of the lot owner. The FDP will contain a matrix detailing allowed fencing within the development.

6. Maintenance

The Boyd Lake Place Home Owners Association (HOA) shall be responsible for the maintenance and long-term care of the landscaping, entry features, perimeter fencing, common area fencing and amenities. Individual lot landscaping and fencing shall be installed and maintained by the individual lot owner. All tracts as shown on the approved Final Plat shall be owned and maintained by the HOA.

G. Architecture and Site Design

1. Overall Concept

The architecture of the project shall be designed as a cohesive neighborhood using a range of materials which will be chosen and implemented in such a way as to render the development a recognizable identity. The various home designs shall not compete for attention, but rather blend to create a sense of community. Consistency in design shall support a smooth transition between the proposed uses on the subject property and between those uses of adjacent properties. To help assure the consistency and adherence to guidelines of the PDP, the HOA shall review each submittal and provide a letter of approval before application to the City of Loveland Building Department.

No building or other structures shall be erected, altered, placed, or permitted to remain on any lot other than one (1) single-family residence per lot, with an attached garage. Exterior design for houses shall place equal importance for architectural features, exterior adornments and special visual treatments on both the front and any other side or rear of house that is facing Frank Road, Boyd Lake Avenue or any public street in the development. Please refer to sheets 13, 14, & 15 depicting proposed building elevations.

2. Architectural Character

Residential units offered are primarily a single-level ranch-style product with an optional second story walk-up attic or a standard two-level product with additional bedroom and living space. Multiple unit plans with a variety of elevations will be designed to provide a diversity of housing style options to residents, as well as a range in design, massing, and aesthetics desired by the City of Loveland.

The overall architectural character will be based on a traditional home style, with similarities to the neighboring single-family homes. The materials and colors to be used on the homes will incorporate pitched and gable roofs, varied building materials, and variations in textures, characteristically found in the Rocky Mountain region. The gabled and hipped roofing will incorporate an overhanging eave and/or other architectural feature and shall be a consistent type and color of roofing material. The

structures shall incorporate a variety of colors and textures, using different material types, and emphasizing detail components. The palette of colors will be appropriate to a traditional neighborhood, using earth tones, and incorporating materials such as lap siding, hardboard shingle siding, manufactured stone siding, stucco, and asphalt composite shingles. Appurtenances such as eaves, overhangs, bay windows, fireplaces, pot shelves, and similar elements may extend into the setbacks up to two feet. The overall architectural concept will emphasize the following elements:

- Building form and character shall be of an appropriate scale. Design considerations shall include the mass, bulk, and proportion of the structure including roofs, walls and openings.
- All elevations of structures will be detailed to provide visual interest and avoid unattractive views.
- Roofing materials, windows, buildings materials and finish will be coordinated to achieve a cohesive appearance.

The PDP submittal for this area includes information demonstrating detailed layouts of the proposed area, including street layouts, landscaping, building elevations, and site amenities. Architectural design guidelines are also established within the PDP.

3. General Notes

All property depicted on the PDP shall be governed by the requirements, restrictions and standards of the City of Loveland Municipal Code, unless otherwise set forth on this plan. Residents shall at all times comply with the requirements, restrictions and prohibitions set forth in the PDP. Residences shall have exterior elevations, roofs, and details that are coordinated and consistent in their architectural treatment. At least three different types of housing models shall be provided. Each housing model shall have at least three characteristics which clearly and obviously distinguish it from the other housing models, which characteristics may include, without limitation, differences in exterior materials, changes in roof lines, garage placement, placement of the footprint on the lot, and/or building fenestration.

The following anti-monotony standard applies to "proximate lots," which are lots that adjoin or share side property lines, or lots that, although separated by a street, face each other such that the extension of side lot lines across the street creates an overlap along the front line of more than 30 percent.

- **General Standards.** No two buildings of similar front elevation shall be constructed or located on more than three proximate lots. Front elevations shall be deemed to be similar if there is no substantial difference in the:
 1. Roof lines;
 2. Fenestration; and
 3. Color or type of cladding materials.
- **Roof Lines.** The following differences in roof lines that are visible on the front building elevation are sufficient to meet the requirements above:
 1. Changing gable roofs to hip roofs or vice-versa;
 2. Providing an intersecting roof ridge or dormer on the main roof, provided that the height of the intersecting roof or dormer is at least 50 percent of the height of the main room;
 3. Rotating gable roof systems 90 degrees; or
 4. Other comparable changes that are approved by the Director.
- **Fenestration.** The following differences in the size, location, or type of windows that are visible on the front building elevation are sufficient to meet the requirements above:
 1. Changing from single windows to a multiple window arrangement or vice-versa;
 2. Changing the types of windows (e.g., casement to double hung); or
 3. In the event that because of its size, location or design, one window is the predominant window on the front building elevation, if the size, location, or type of that window is changed, then no other window need be changed.
- **Cladding and Color.** The following differences in the size, location, or type of windows that are visible on the front building elevation are sufficient to meet the requirements above:
 1. Changes in cladding materials throughout the façade or elevation for a minimum of one story in height.
 2. Changes in color (rather than merely shade).

4. Typical Residential Neighborhood Architectural Styles

See sheet 13, 14, & 15 of the PDP for illustrations on proposed single-family architectural styles.

5. Setbacks

Single-family building setbacks will be regulated as follows:

Building Setbacks	Front* (Building)	Front** (Garage)	Side	Rear
	12 ft.	20 ft.	5 ft. (internal lot line) 15 ft. (from street)	12 ft.

*Measured from the back of sidewalk or property line, whichever produces a greater setback

**Measured from garage door to back of sidewalk

6. Height Limits

No building shall exceed 26' measured in accordance with Loveland Municipal Code.

7. Parking

The number, size and design of parking spaces and lots shall correspond to the requirements of the City of Loveland Municipal Code and the Site Development Performance Standards and Guidelines. No on-street parking will be permitted on an arterial street. Parking is permitted on residential local streets as provided within the fifty-foot right-of-way.

8. Garages

Garages within Boyd Lake Place shall abide by the following standards below:

- On at least 75 percent of the dwelling units on a block face, garage doors that face the street shall be recessed a minimum of 1 feet behind either the living portion of the house or from the front of a covered porch measuring at least six feet in depth by eight feet in length.
- No more than 25% of the dwelling units on a block face shall have garage doors that face the street that extend in front of the living portion of the house or a covered porch. Garages meeting this standard shall comply with the following standards:
 - a. The maximum extension shall be 2 feet in front of the living portion of the house or a covered porch; and
 - b. No two homes with garage doors facing the street that extend in front of the living portion of the house or a covered porch shall be constructed on adjacent lots that adjoin or share side property lines.
- Garage doors that are adjacent to the street but that have side-facing garage doors (perpendicular to the street) shall be clad with the same predominant materials, architectural detail, and windows that mimic the features of the living portion of the dwelling on the side of the garage facing the street.
- Garage door openings shall not comprise more than 47 percent of the front yard building facade.
- All garages shall be attached to the dwelling unit. No detached garages or detached accessory structures shall be permitted within the PUD.
- Alternate overhead garage door treatments may be approved by the Director if the proposed design substantially meets the intent of these standards to line streets with active living spaces, create pedestrian-oriented streetscapes, and provide variety and visual interest in the exterior design of residential buildings.

9. Windows

Wood or vinyl windows shall be required. No metal windows shall be permitted.

10. Roof

The roof of each single-family residence shall have a minimum 6/12 pitch and a minimum overhang of twelve (12) inches at every point around the roofline of the residence. The roof shall be constructed of asphalt, composition, clay or concrete tile, slate, or other roofing materials permitted by the City of Loveland.

11. Front Siding

A minimum of 25 percent for any single-family house walls facing the street, including the doors and windows, but excluding the roof areas, shall be masonry, such as brick, stone, artificial stone or stucco, as allowed by the HOA. Outside corners of street-facing elevations shall include a minimum of four feet (4') full wall height wrap with similar masonry material, inclusive of garage walls or the same total square footage of wrap in the form of a wainscoting with similar masonry material, inclusive of garage walls.

12. Siding Other than Masonry

When wood or Masonry or other types of approved lap siding is used on that part of the single-family residence not required to be sided with masonry, then siding which shall be in a horizontal pattern with a maximum width of eight (8) inches with no more than a seven (7) inch exposure shall be approved.

H. Implementation

1. Overall Concept

The development of the project is scheduled to commence in 2018 and be completed by the end of 2021. Construction of the project will be done in one phase.

2. Estimate of Build Out Time Period

Construction and sales are projected to commence in 2019 and be completed by the end of 2021.

I. Regulatory Procedures

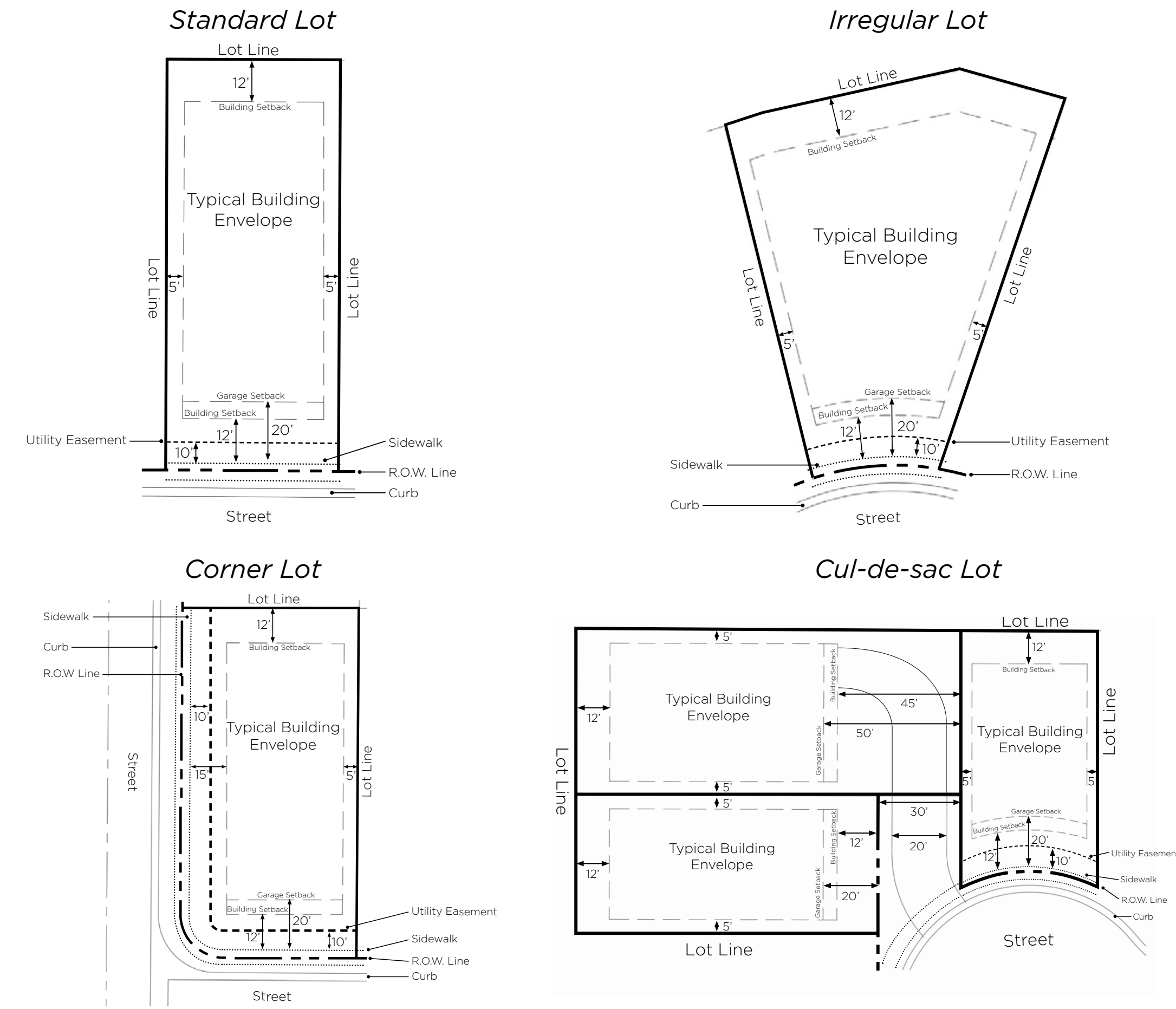
All property depicted on this plan shall be governed by the requirements, restrictions, and standards of the Boyd Lake Place annexation agreement, PUD, and PDP. All residential areas shall be governed by the provisions of Chapter 18.12 of the Loveland Municipal Code, as amended, unless specifically set forth in the Boyd Lake Place annexation agreement, PUD, and PDP.

Developers, contractors, property owners and residents shall comply at all times with the requirements, restrictions and prohibitions set forth in the annexation agreement, the PUD, and the PDP in this narrative. The owner of a property shall maintain the elevation, grading, and drainage plan approved by the governmental authority and the utility providers having jurisdiction. The owner of a property shall not impede or hinder in any way the water falling on the property from reaching the drainage courses established for the property and the common areas. The site elevation cannot be lower than shown on the approved construction plans without approval of the City of Loveland. Prior to the issuance of a Certificate of Occupancy from the City of Loveland, the owner shall deliver to the City a certificate from the owner's professional engineer certifying that the grading for the residence, as constructed and in place, complies in all material respects with all approved drainage plans.

The storm run-off water quality treatment ponds depicted on the approved drainage plans for this development shall be governed and monitored by the property owner in conjunction with the cooperative monitoring by the Greeley-Loveland Irrigation Company and shall be subject to all approved constructions plans, rules, and regulations of the City of Loveland.

The Boyd Lake Place PDP is designed as a single-family neighborhood, to provide contiguity to the surrounding neighborhoods to allow variations in building types and styles, and to provide consistency in design throughout the development. A reference shall be included in the covenants within the PDP stating that said parcel is subject to zoning regulations established in the Boyd Lake Place PUD.

Lot Typicals



community ■ planning ■ architecture

LAKEVIEW HOLDINGS, LLC.

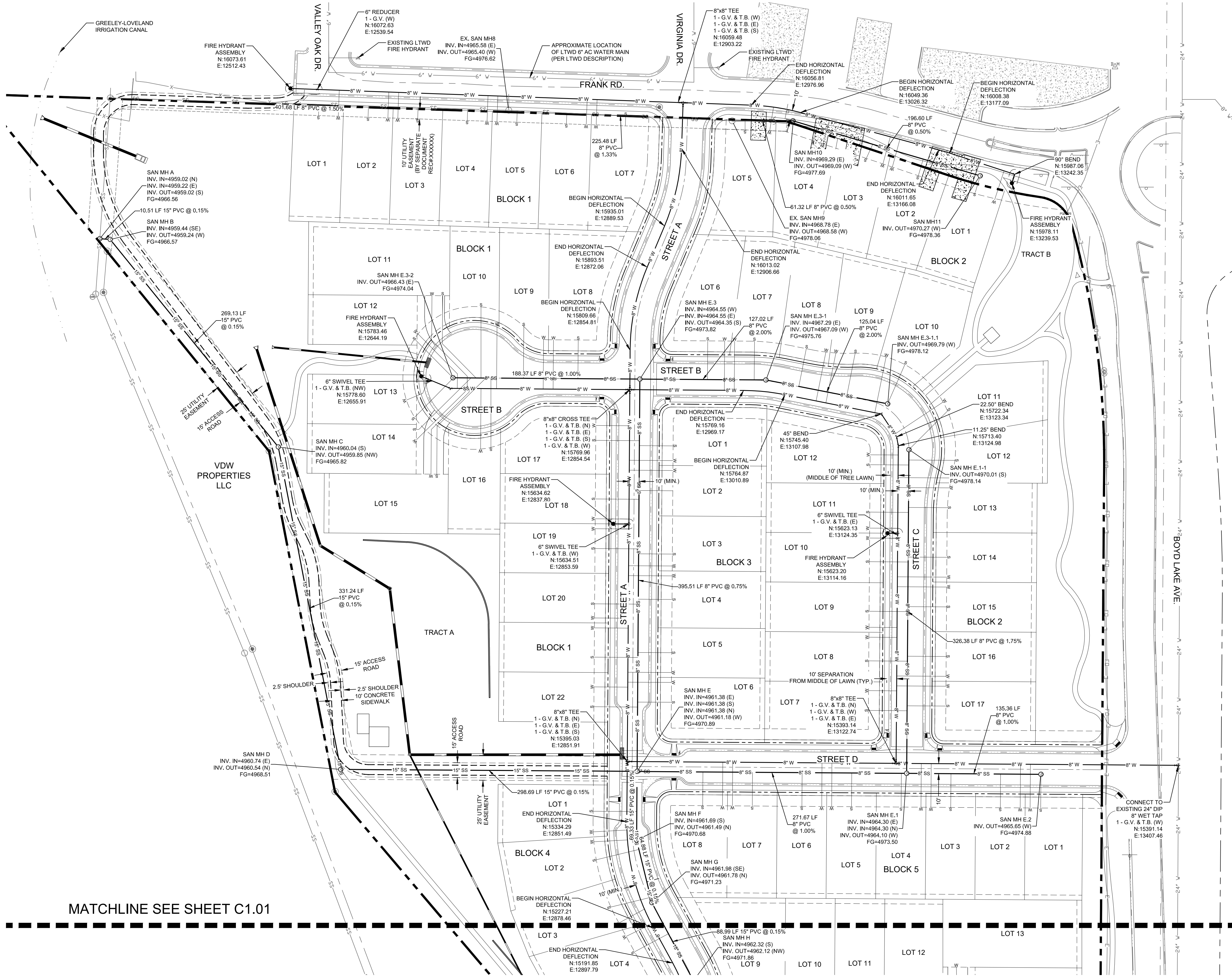
BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
DEVELOPMENT NARRATIVE AND
TYPICAL LOTS

ISSUE DATE: 5/16/18	PROJECT #: 215026
DATE	REVISION COMMENTS
6/14/18	1st Resubmittal
7/12/18	2nd Resubmittal

SHEET NO.

6

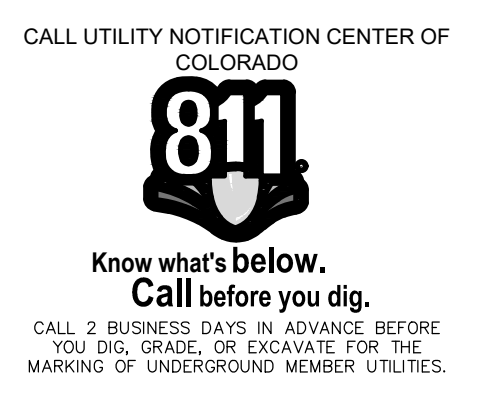
6 OF 15



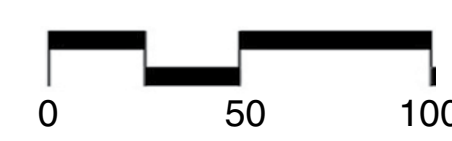
LEGEND:

PROJECT BOUNDARY	---
EXISTING CURB & GUTTER	---
PROPOSED CURB & GUTTER	---
FUTURE CURB & GUTTER	---
PROPOSED EASEMENT LINE	---
EXISTING EXISTING R.O.W	---
EXISTING BURIED ELECTRIC	---
EXISTING BURIED GAS	---
EXISTING WATER LINE	---
PROPOSED 8" WATER LINE	---
PROPOSED WATER SERVICE LINE	---
EXISTING SANITARY SEWER LINE	---
PROPOSED SANITARY SERVICE LINE	---
PROPOSED STORM SEWER	---
PROPOSED INLET	---
EXISTING STORM SEWER	---
PROPOSED GAS LINE	---
PROPOSED ELECTRIC LINE	---
EXISTING SANITARY MH	---
EXISTING GAS METER	---
EXISTING ELECTRIC METER	---
EXISTING ELECTRIC VAULT	---
EXISTING TELEPHONE PEDESTAL	---
EXISTING FIRE HYDRANT	---
EXISTING TRAFFIC VAULT	---

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 - UTILITY SERVICES ARE SHOWN IN A SCHEMATIC FASHION ONLY. EXACT LOCATIONS SHALL BE PER THE REQUIREMENTS OF THE RESPECTIVE UTILITY PROVIDERS, AND ARE SUBJECT TO CHANGE IN THE FIELD.
 - ALL WATER FITTINGS, VALVES, AND OTHER UTILITY APPURTENANCES ARE ONLY GRAPHICALLY REPRESENTED AND ARE NOT TO SCALE.
 - ALL EXISTING SANITARY SEWER SERVICES ARE APPROXIMATE AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION. ALL SEWER SERVICES TO BE UNUSED SHALL BE ABANDONED AT THE MAIN AND COORDINATED WITH FORT COLLINS UTILITIES (970-221-6700).
 - ALL EXISTING WATER SERVICES ARE APPROXIMATE AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION. ALL SEWER SERVICES TO THE UNUSED SHALL BE ABANDONED AT THE MAIN AND COORDINATED WITH THE CITY OF LOVELAND UTILITIES (970-982-3715).
 - PRIOR TO ORDERING MATERIAL AND MOBILIZING, CONTRACTOR SHALL FIELD LOCATE AND VERIFY ELEVATIONS OF ALL CONCRETE AND ASPHALT TIE-INS, EXISTING SEWER MAINS, WATER MAINS, AND ALL OTHER UTILITIES AT THE POINTS OF CONNECTION SHOWN ON THE PLANS. SPECIAL ATTENTION SHALL BE PAID TO ALL PROPOSED UTILITY CROSSINGS. IT IS RECOMMENDED THAT THE CONTRACTOR POT-HOLE SAID CROSSINGS PRIOR TO ORDERING MATERIAL.
 - LIMITS OF STREET CUT ARE APPROXIMATE. FINAL LIMITS ARE TO BE DETERMINED IN THE FIELD BY THE CITY ENGINEERING INSPECTOR. ALL REPAIRS TO BE IN ACCORDANCE WITH CITY STREET REPAIR STANDARDS.
 - CONTRACTOR IS RESPONSIBLE FOR ALL THE REPAIRS AND REPLACEMENTS OF ANY DAMAGED CURB AND GUTTER. ALL REPAIRS TO BE IN ACCORDANCE WITH CITY STREET REPAIR STANDARDS.



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NE NORTHERN ENGINEERING
FORT COLLINS: 301 North Howes Street, Suite 100, 80521 970.221.4158
GREELEY: 820 8th Street, 80531 northernengineering.com

LAKEVIEW HOLDINGS, LLC.

**BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
OVERALL UTILITY PLAN**

ISSUE DATE:	5/16/18	PROJECT #:	215026
DATE	REVISION	COMMENTS	
6/14/18	1st	Resubmittal	
7/12/18	2nd	Resubmittal	

SHEET NO.
7
7 OF 15

MATCHLINE SEE SHEET C1.00

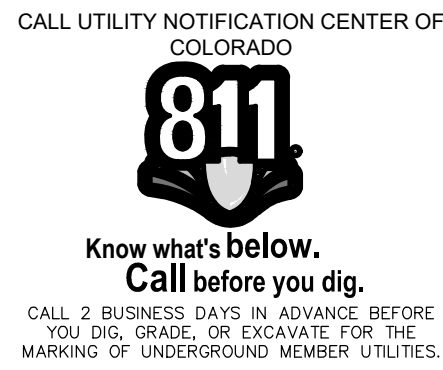


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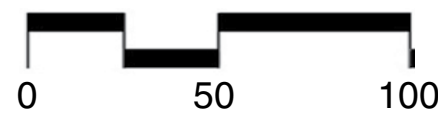
- PROJECT BOUNDARY
EXISTING CURB & GUTTER
PROPOSED CURB & GUTTER
FUTURE CURB & GUTTER
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EXISTING BURIED ELECTRIC
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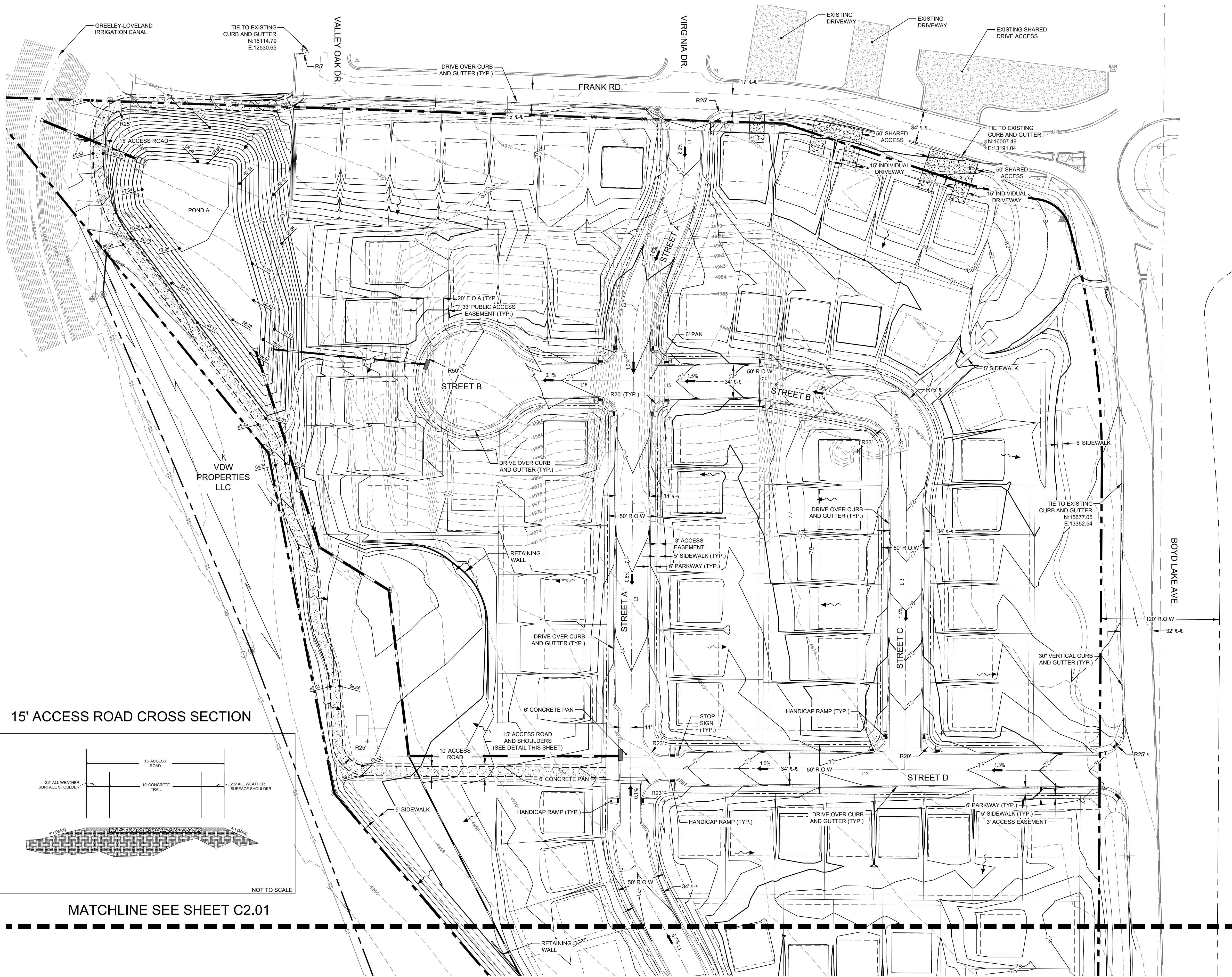


LAKEVIEW HOLDINGS, LLC.

BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
OVERALL UTILITY PLAN

ISSUE DATE: 5/16/18		PROJECT #: 215026
DATE	REVISION	COMMENTS
6/14/18	1st	Resubmittal
7/12/18	2nd	Resubmittal

SHEET NO.
8
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NORTH

LEGEND:

PROJECT BOUNDARY	
EXISTING CURB & GUTTER	
PROPOSED CURB & GUTTER	
FUTURE CURB & GUTTER	
PROPOSED EASEMENT LINE	
EXISTING R.O.W	
PROPOSED R.O.W	
PROPOSED LOT LINE	
PROPOSED STORM SEWER	
PROPOSED INLET	
EXISTING STORM SEWER	
PROPOSED GRADE	
PROPOSED SPOT ELEVATION	
EXISTING SPOT ELEVATION	
PROPOSED SLOPES	
TEMPORARY TURNAROUND	

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- THE TOP OF FOUNDATION ELEVATIONS SHOWN ARE THE MINIMUM ELEVATIONS REQUIRED FOR PROTECTION FROM THE 100-YEAR STORM.
- CROSSPANS ALONG PUBLIC STREETS AT THE INTERSECTION OF PRIVATE DRIVES SHALL HAVE CONCRETE EXTENDED TO THE RIGHT-OF-WAY LINE.
- ALL PROJECT DATA IS ON THE CITY OF LOVELAND VERTICAL DATUM, NAVD 88 (UNADJUSTED). SEE COVER SHEET FOR BENCHMARK REFERENCES.
- ALL LINE AND CURVE INFORMATION IS IN REGARD TO THE STREETS CENTERLINES.
- BOYD LAKE INFORMATION
NORMAL WATER SURFACE ELEVATION: 4959.59
CITY OF LOVELAND 100-YEAR WATER SURFACE ELEVATION: 4960.08
STATE ENGINEER'S OFFICE (SEO) MAXIMUM WATER SURFACE ELEVATION: 4962.00
- TEMPORARY TURNAROUND TO BE CONSTRUCTED WITH AN ALL-WEATHER SURFACE AND DESIGNED TO SUPPORT A 40 TON VEHICLE. THE PORTIONS OF THE TEMPORARY TURNAROUND OUTSIDE OF PUBLIC RIGHT-OF-WAY IS TO BE OWNED AND MAINTAINED BY THE HOA.

LINE TABLE		
LINE	LENGTH	BEARING
L1	43.28'	S01°55'20.56"W
L2	65.94'	S22°50'20.08"W
L3	485.35'	S00°24'02.78"W
L4	70.82'	S28°40'12.34"E
L5	299.92'	S16°01'44.52"E
L6	107.12'	S42°51'10.44"E
L7	20.24'	S00°00'00.00"E
L8	119.95'	N89°35'57.22"W
L9	16.01'	S47°08'49.56"W
L10	102.58'	N90°00'00.00"W
L11	52.11'	S73°58'15.48"W
L12	473.54'	N89°35'57.22"W
L13	321.38'	N00°24'02.78"E
L14	90.56'	N78°39'41.55"W
L15	115.93'	N89°35'57.22"W
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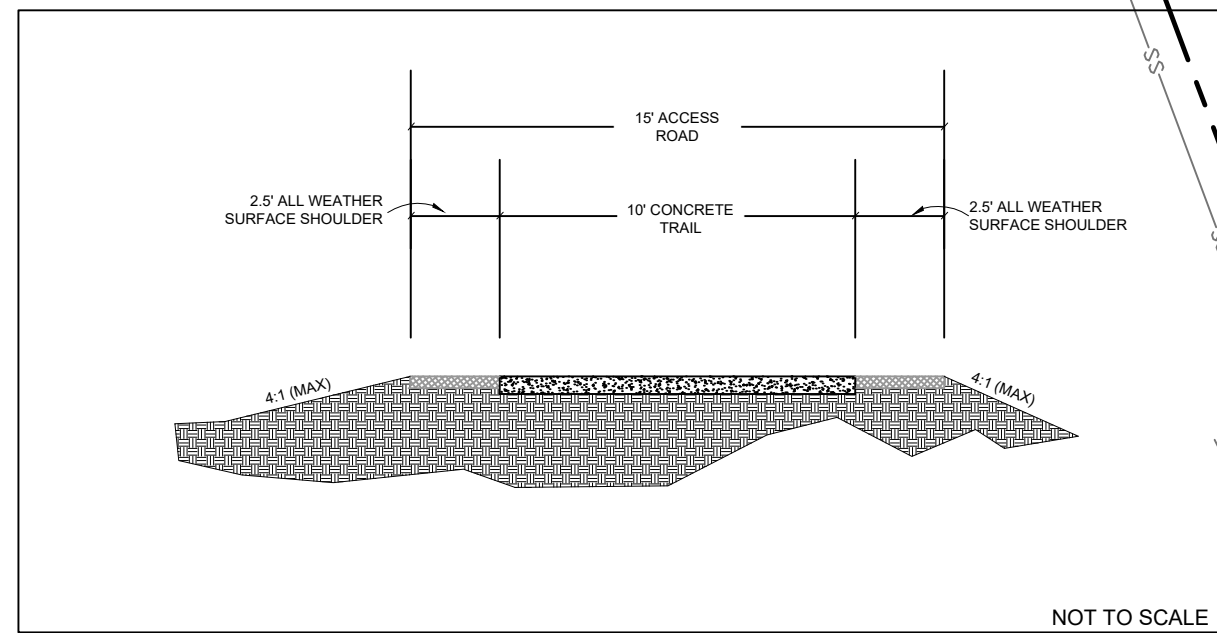
CURVE TABLE				
CURVE	DELTA	RADIUS	LENGTH	BEARING
C1	20°55'00"	165.00'	60.24'	S12°22'50"W
C2	22°26'17"	165.00'	64.62'	S11°37'11"W
C3	29°04'15"	165.00'	83.72'	S14°08'05"E
C4	12°38'28"	165.00'	36.40'	S22°20'58"E
C5	26°49'26"	165.00'	77.25'	S29°26'27"E
C6	42°51'10"	165.00'	123.41'	S21°25'35"E
C7	43°15'13"	165.00'	124.56'	S68°46'26"W
C8	16°01'45"	165.00'	46.16'	S81°59'08"W
C9	79°03'44"	50.00'	68.99'	N39°07'49"W
C10	10°56'16"	165.00'	31.50'	N84°07'49"W

CALL UTILITY NOTIFICATION CENTER OF COLORADO



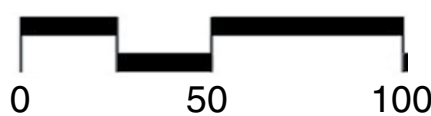
CALL 2 BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

15' ACCESS ROAD CROSS SECTION



MATCHLINE SEE SHEET C2.01

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FORT COLLINS: 301 North Howes Street, Suite 100, 80521
GREELEY: 820 8th Street, 80531

NORTHERN
ENGINEERING

970.221.4158
northernengineering.com

LAKEVIEW HOLDINGS, LLC.

BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
GRADING PLAN

ISSUE DATE: 5/16/18		PROJECT #: 215026
DATE	REVISION COMMENTS	
6/14/18	1st Resubmittal	
7/12/18	2nd Resubmittal	

SHEET NO.

9

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MATCHLINE SEE SHEET C2.00



LEGEND:

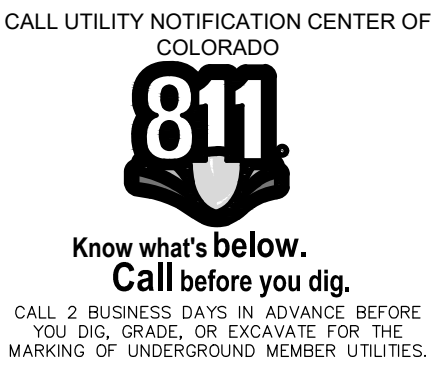
PROJECT BOUNDARY	
EXISTING CURB & GUTTER	
PROPOSED CURB & GUTTER	
FUTURE CURB & GUTTER	
PROPOSED EASEMENT LINE	
EXISTING R.O.W	
PROPOSED R.O.W	
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PROPOSED SPOT ELEVATION	
EXISTING SPOT ELEVATION	
PROPOSED SLOPES	
TEMPORARY TURNAROUND	

GENERAL NOTES:

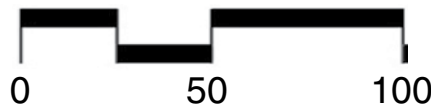
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CURVE TABLE					
CURVE	DELTA	RADIUS	LENGTH	BEARING	CHORD
C1	20°55'00"	165.00'	60.24'	S12°22'50"W	59.90'
C2	22°26'17"	165.00'	64.62'	S11°37'11"W	64.21'
C3	29°04'15"	165.00'	83.72'	S14°08'05"E	82.82'
C4	12°38'28"	165.00'	36.40'	S22°20'58"E	36.33'
C5	26°49'26"	165.00'	77.25'	S29°26'27"E	76.54'
C6	42°51'10"	165.00'	123.41'	S21°25'35"E	120.55'
C7	43°15'13"	165.00'	124.56'	S68°46'28"W	121.82'
C8	16°01'45"	165.00'	46.16'	S81°59'08"W	46.01'
C9	79°03'44"	50.00'	68.99'	N39°07'49"W	63.65'
C10	10°56'16"	165.00'	31.50'	N84°07'49"W	31.45'



These drawings are instruments of service provided by Northern Engineering Services, Inc. and are not to be used for any type of construction unless signed and sealed by a Professional Engineer in the employ of Northern Engineering Services, Inc.



FORT COLLINS: 301 North Howes Street, Suite 100, 80521
GREELEY: 820 8th Street, 80631

NORTHERN
ENGINEERING

970.221.4158
northernengineering.com

LAKEVIEW HOLDINGS, LLC.

BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
GRADING PLAN

ISSUE DATE: 5/16/18		PROJECT #: 215026
DATE	REVISION COMMENTS	
6/14/18	1st Resubmittal	
7/12/18	2nd Resubmittal	

SHEET NO.
10
10 OF 15



LEGEND:

PROJECT BOUNDARY
EXISTING CURB & GUTTER
PROPOSED CURB & GUTTER
PROPOSED STORM SEWER
PROPOSED INLET
EXISTING STORM SEWER
DESIGN POINT
OVERLAND FLOW
DIRECT FLOW
DRAINAGE BASIN LABEL
DRAINAGE BASIN BOUNDARY

- NOTES:**
- 1. REFER TO THE "PRELIMINARY DRAINAGE LETTER FOR BOYD LAKE PLACE" FOR ADDITIONAL INFORMATION.
 - 2. ADDITIONAL DRAINAGE INFORMATION WILL BE PROVIDED WITH FUTURE SUBMITTAL.
 - 3. BOYD LAKE INFORMATION
NORMAL WATER SURFACE ELEVATION: 4959.59
CITY OF LOVELAND 100-YEAR WATER SURFACE ELEVATION: 4960.08
STATE ENGINEER'S OFFICE (SEC) MAXIMUM WATER SURFACE ELEVATION: 4962.00

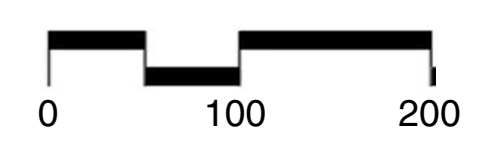
FOR DRAINAGE REVIEW ONLY
NOT FOR CONSTRUCTION

CALL UTILITY NOTIFICATION CENTER OF COLORADO

Know what's below.
Call before you dig.

CALL 2 BUSINESS DAYS IN ADVANCE, BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

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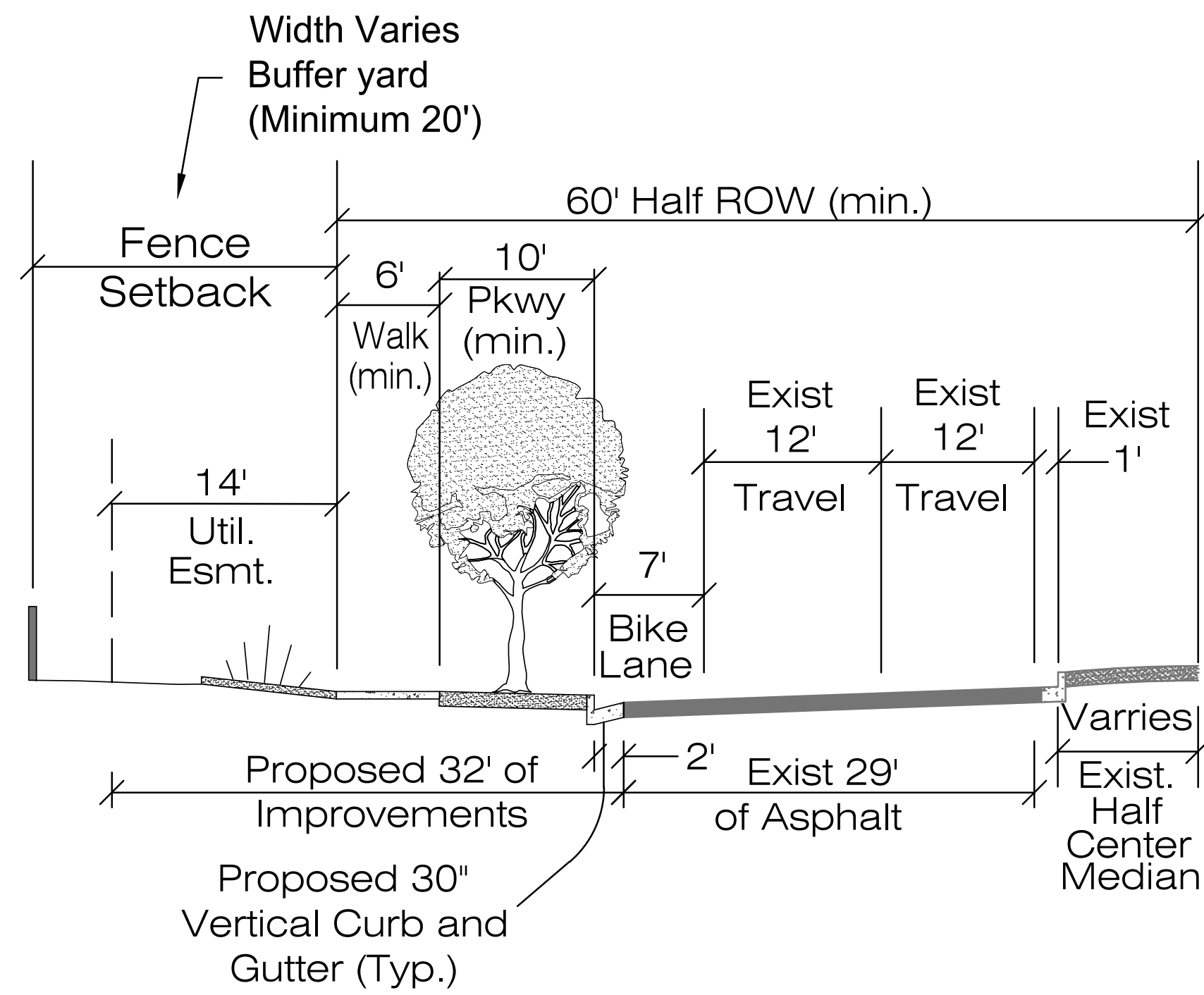
FORT COLLINS: 301 North Howes Street, Suite 100, 80521 970.221.4158 northernengineering.com
GREELEY: 820 8th Street, 80531

LAKEVIEW HOLDINGS, LLC.

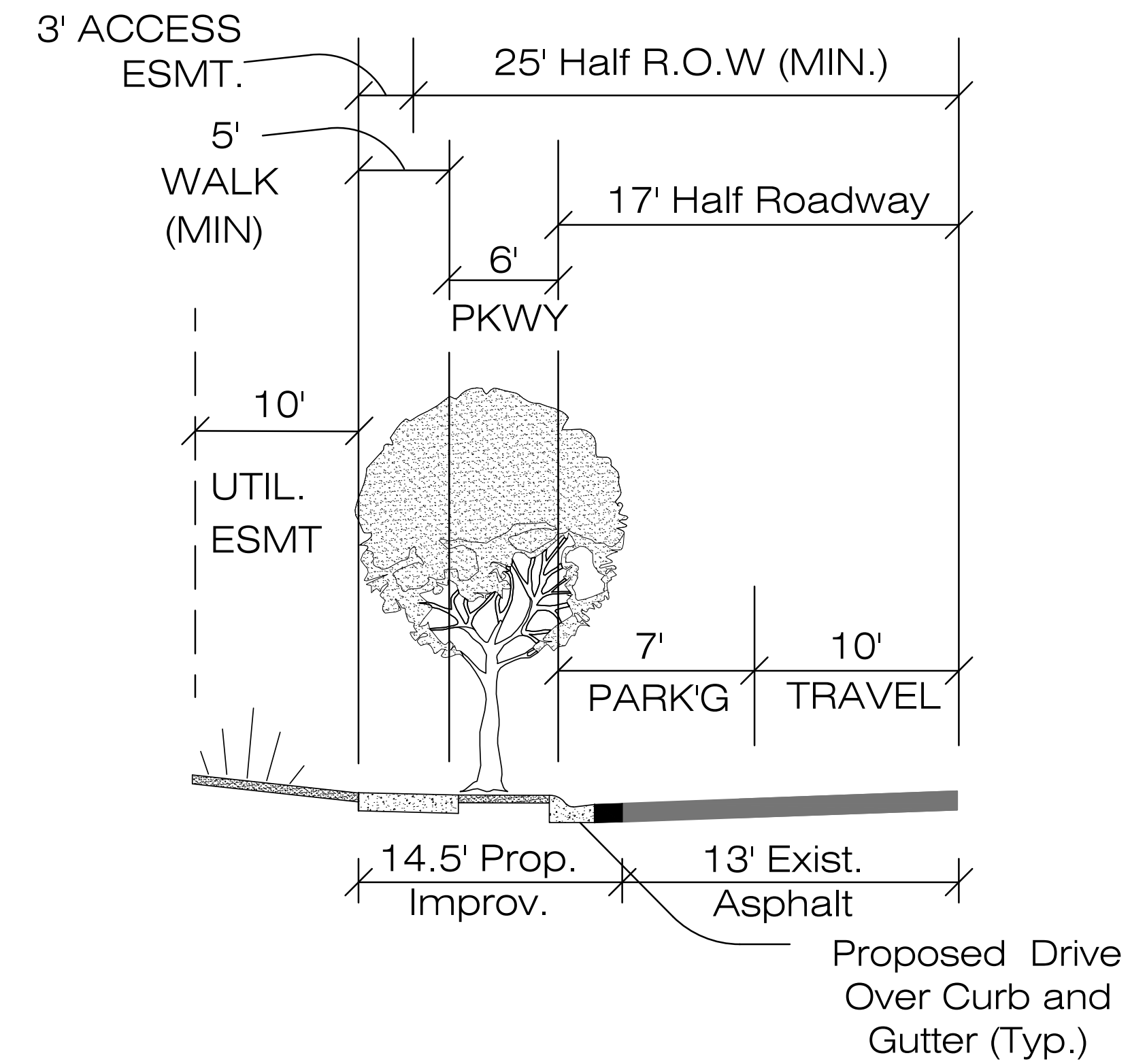
BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
DRAINAGE EXHIBIT

ISSUE DATE: 5/16/18		PROJECT #: 215026
DATE	REVISION COMMENTS	
6/14/18	1st Resubmittal	
7/12/18	2nd Resubmittal	

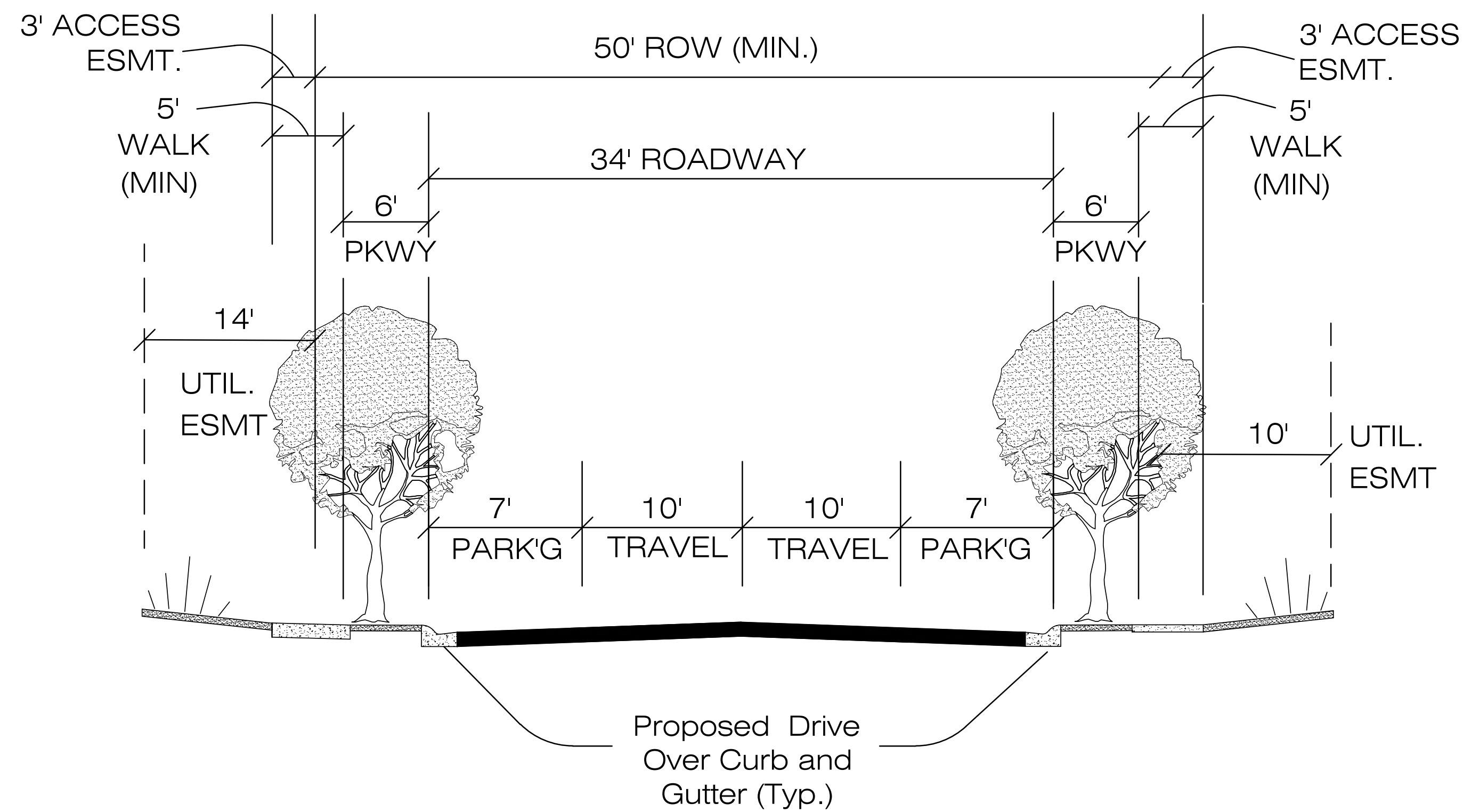
SHEET NO.
11
11 OF 15



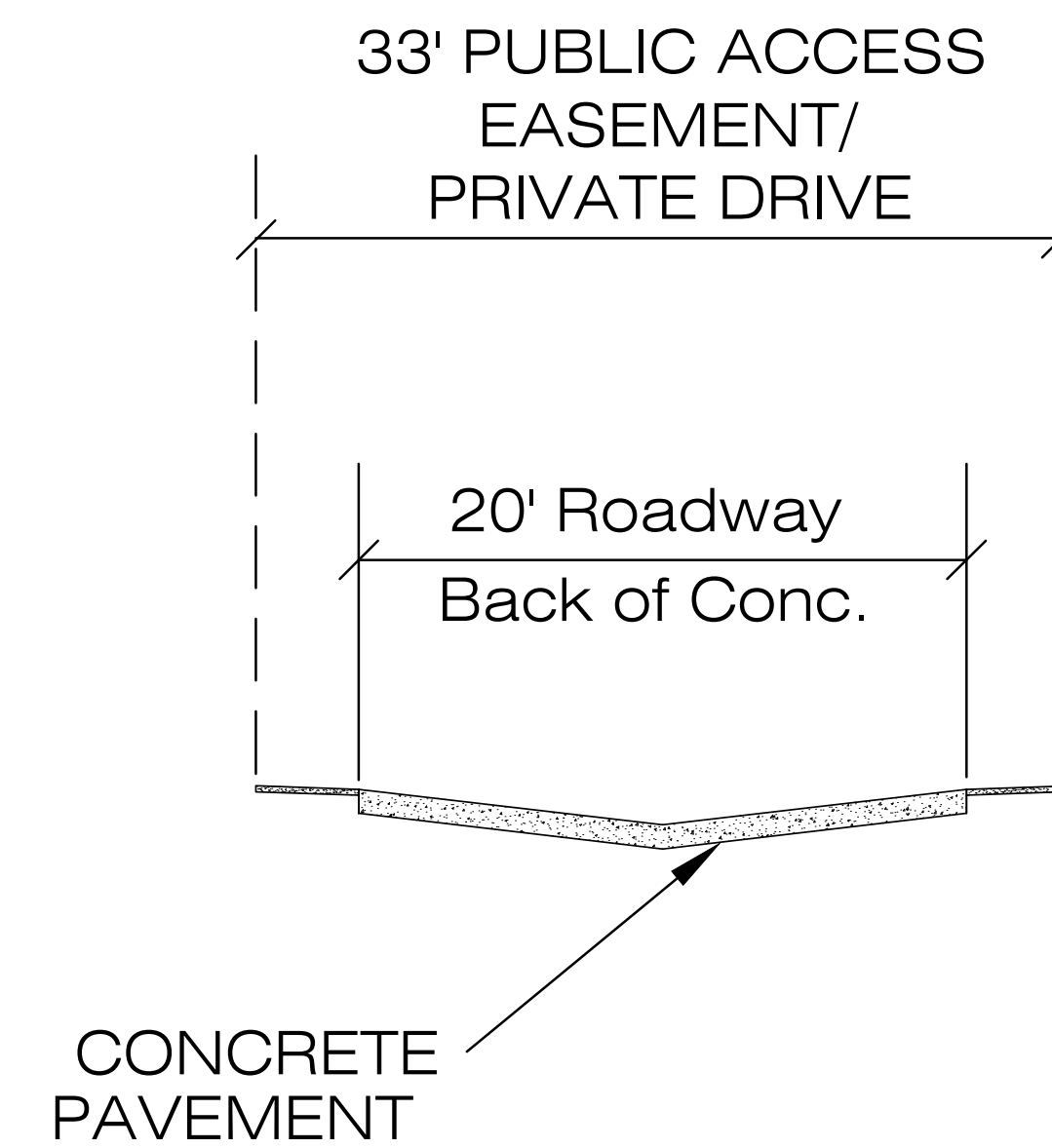
BOYD LAKE AVE.



FRANK RD.



LOCAL RESIDENTIAL



Note: City trash will not use private drives for trash collection

PRIVATE DRIVEWAY
AT THE END OF THE CUL-DE-SACS
(STREET B & STREET E)

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Not To Scale



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GREELEY: 820 8th Street, 80531

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LAKEVIEW HOLDINGS, LLC.

BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
TYPICAL CROSS SECTIONS

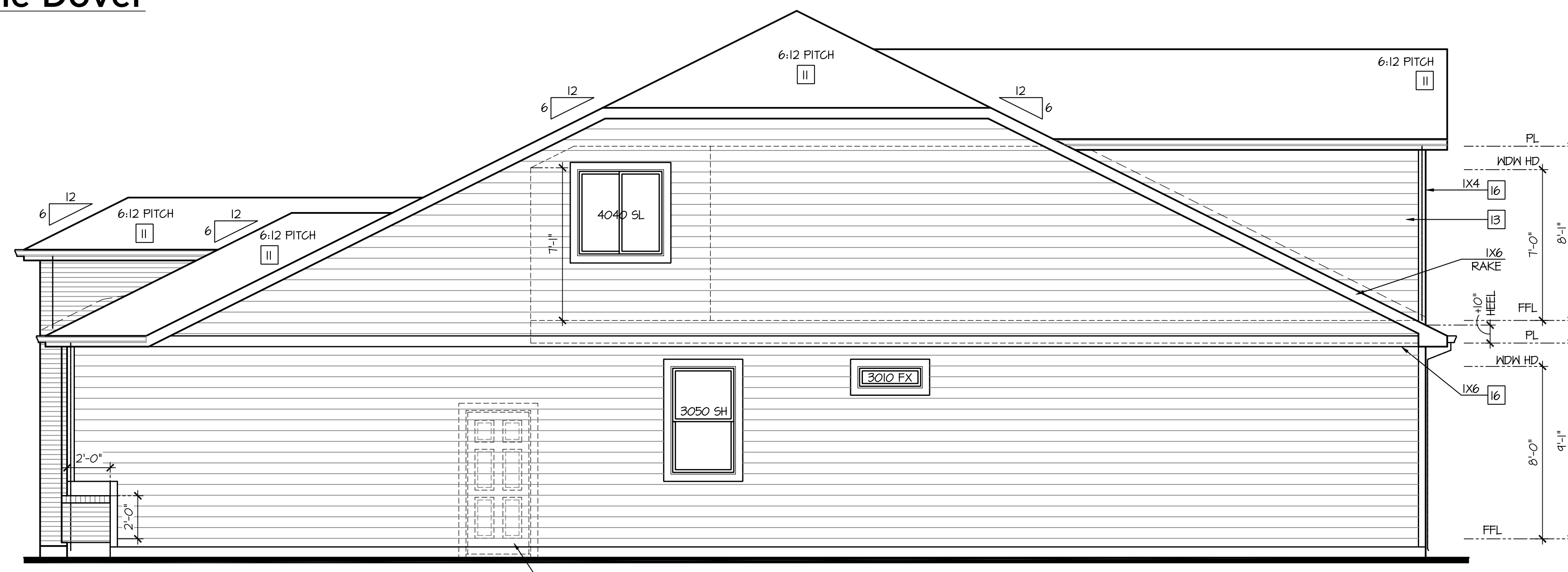
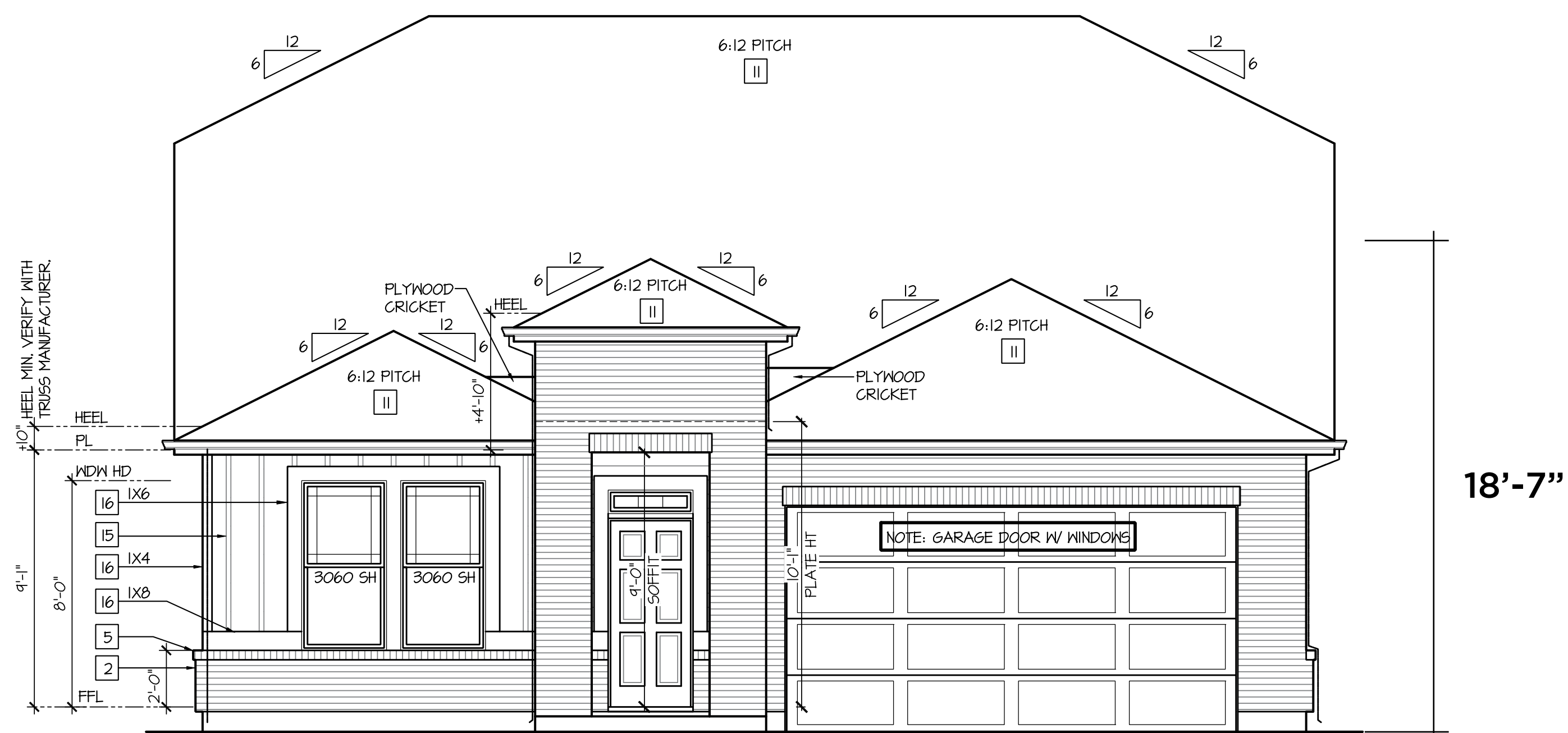
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6/14/18	1st Resubmittal	215026
7/12/18	2nd Resubmittal	

SHEET NO.

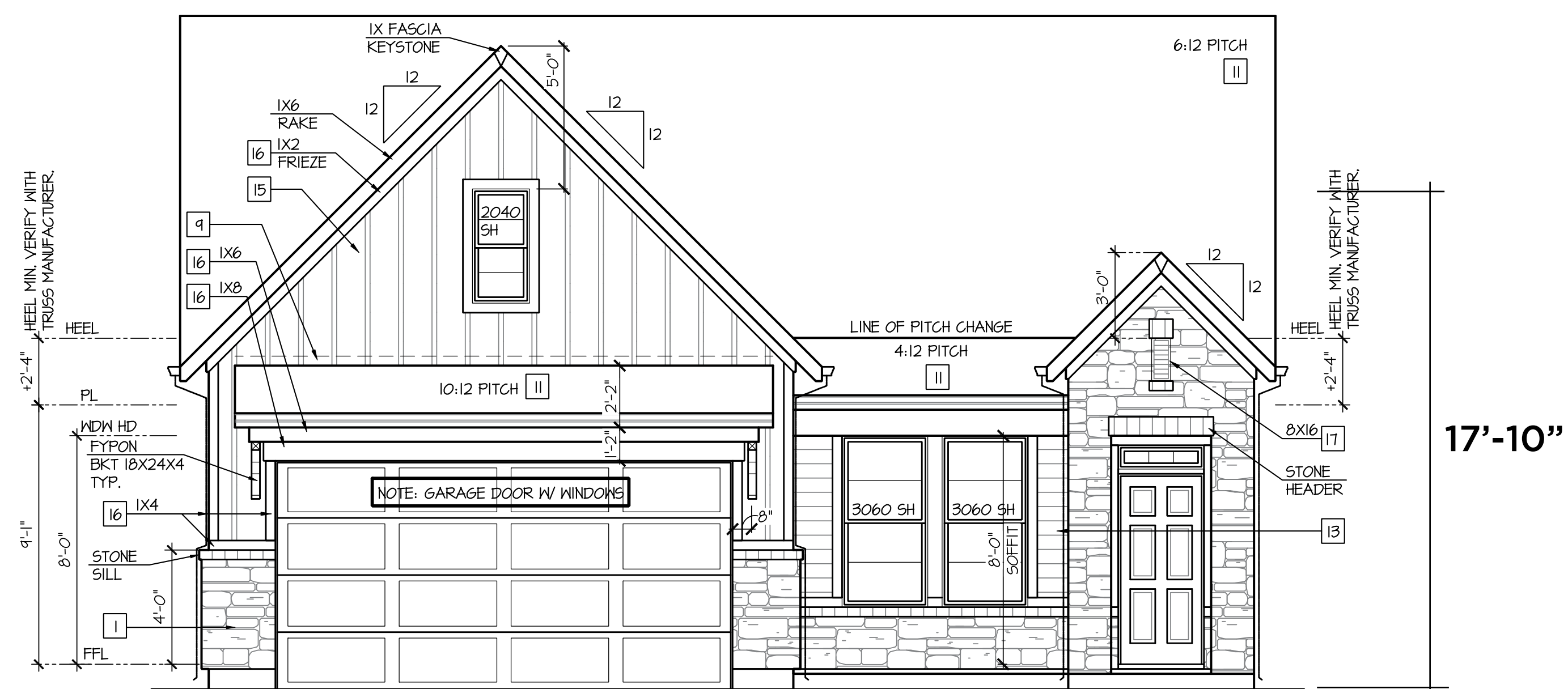
12

12 OF 15

The Dover



The Aster



Scale: 1/4" = 1'-0"



LAKEVIEW HOLDINGS, LLC.

BOYD LAKE PLACE PRELIMINARY DEVELOPMENT PLAN PROPOSED ARCHITECTURE STYLES

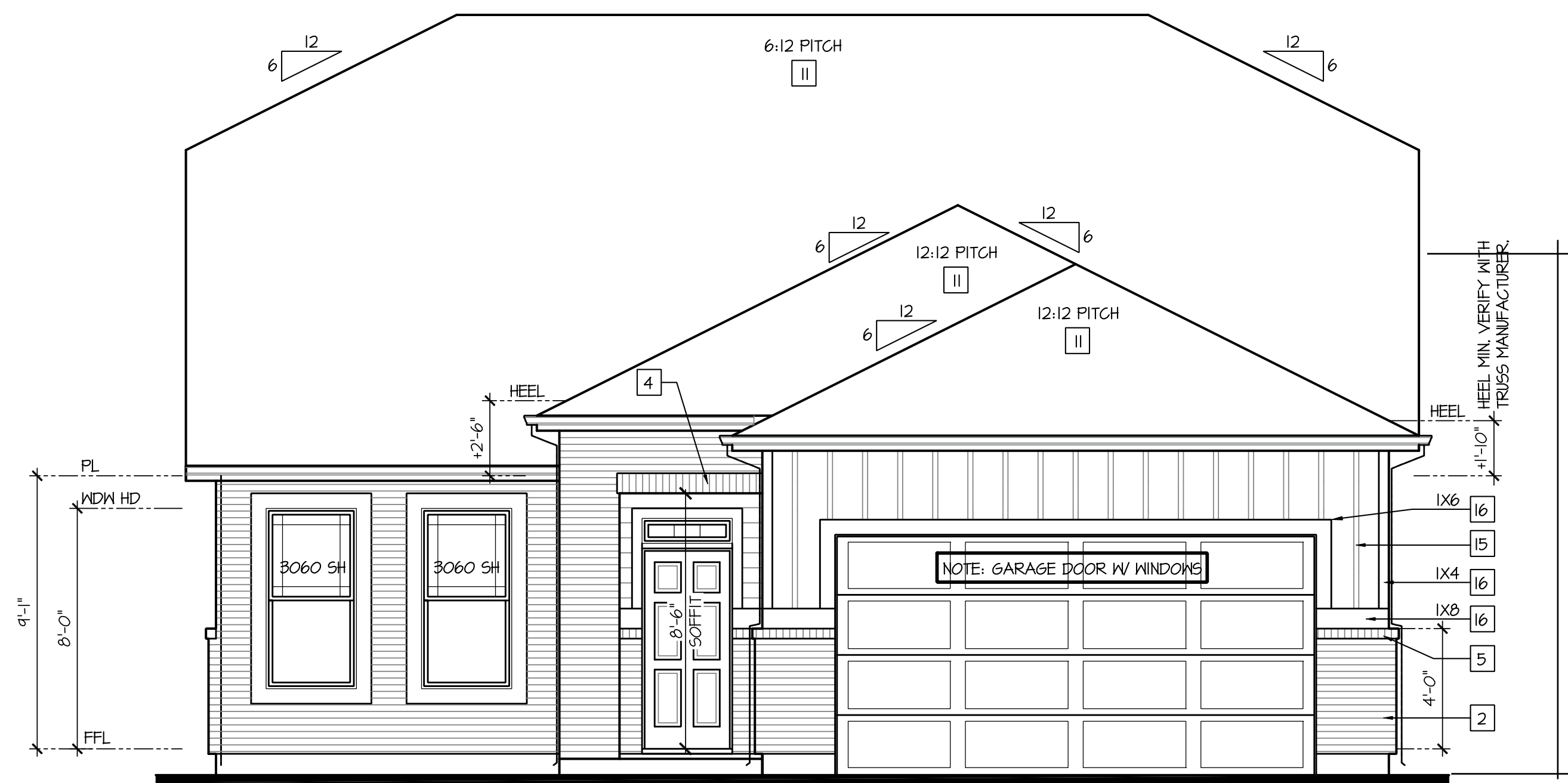
ISSUE DATE: 5/16/18		PROJECT #: 215026
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SHEET NO.

13

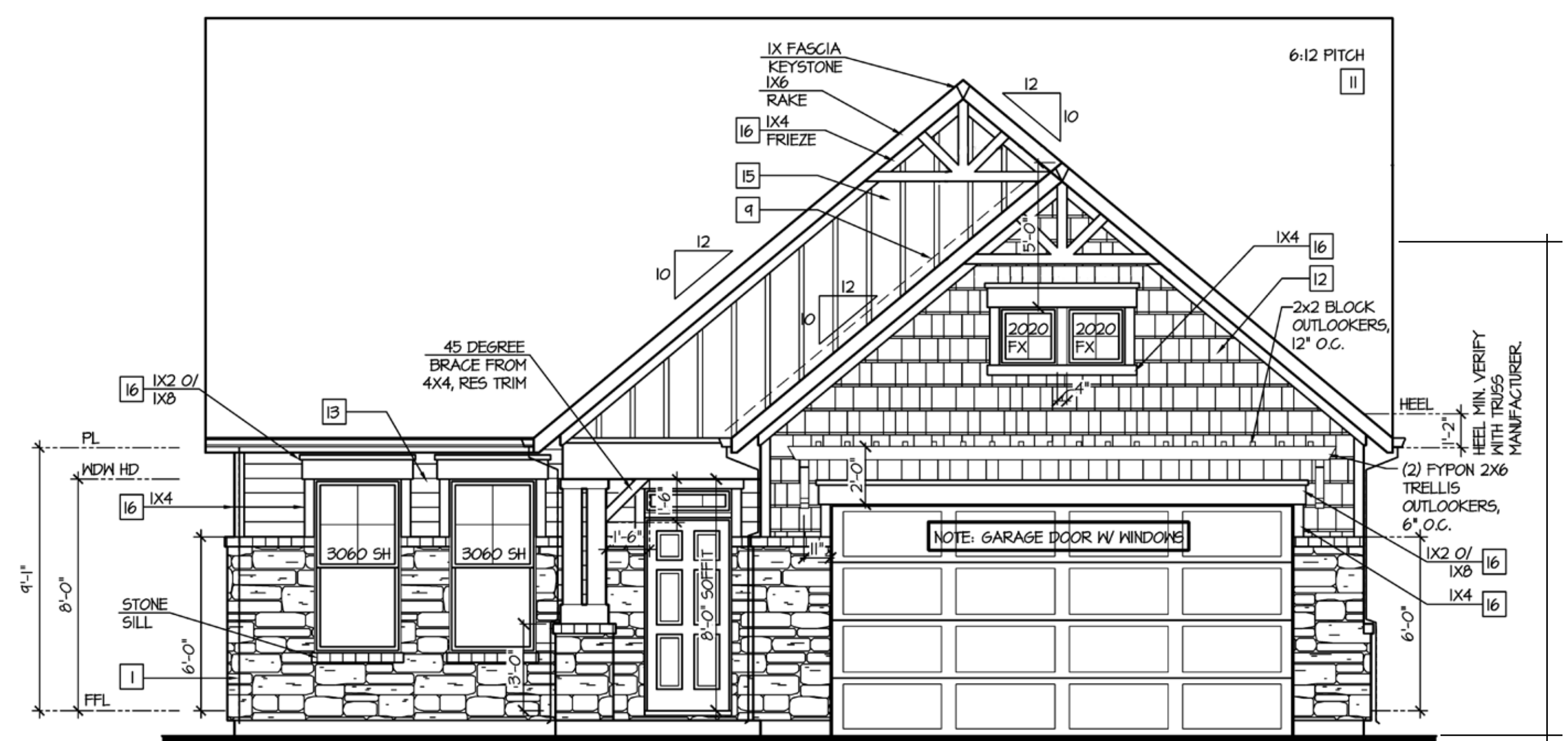
13 OF 15

The Bristol



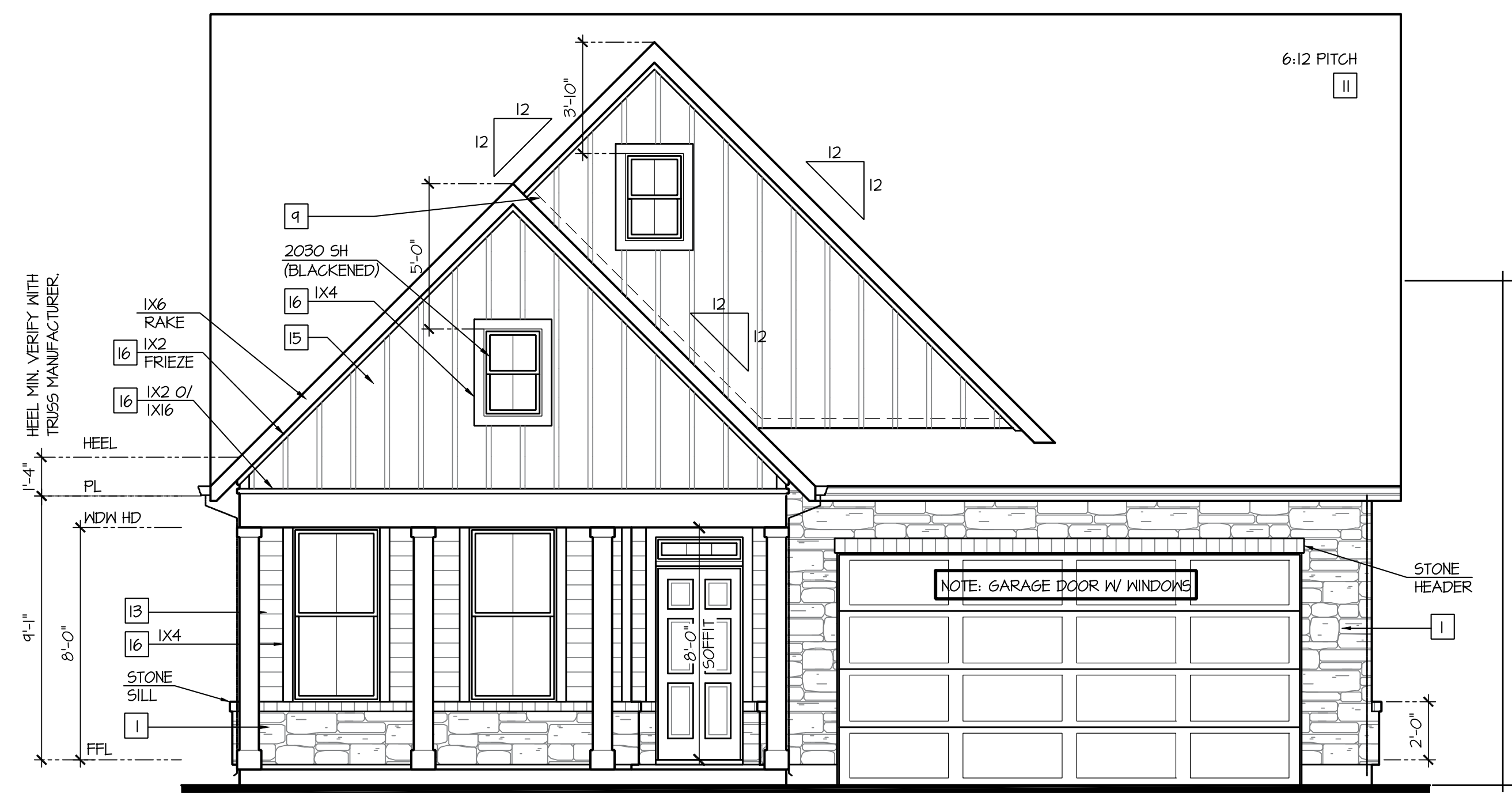
18'-9"

The Arlington II

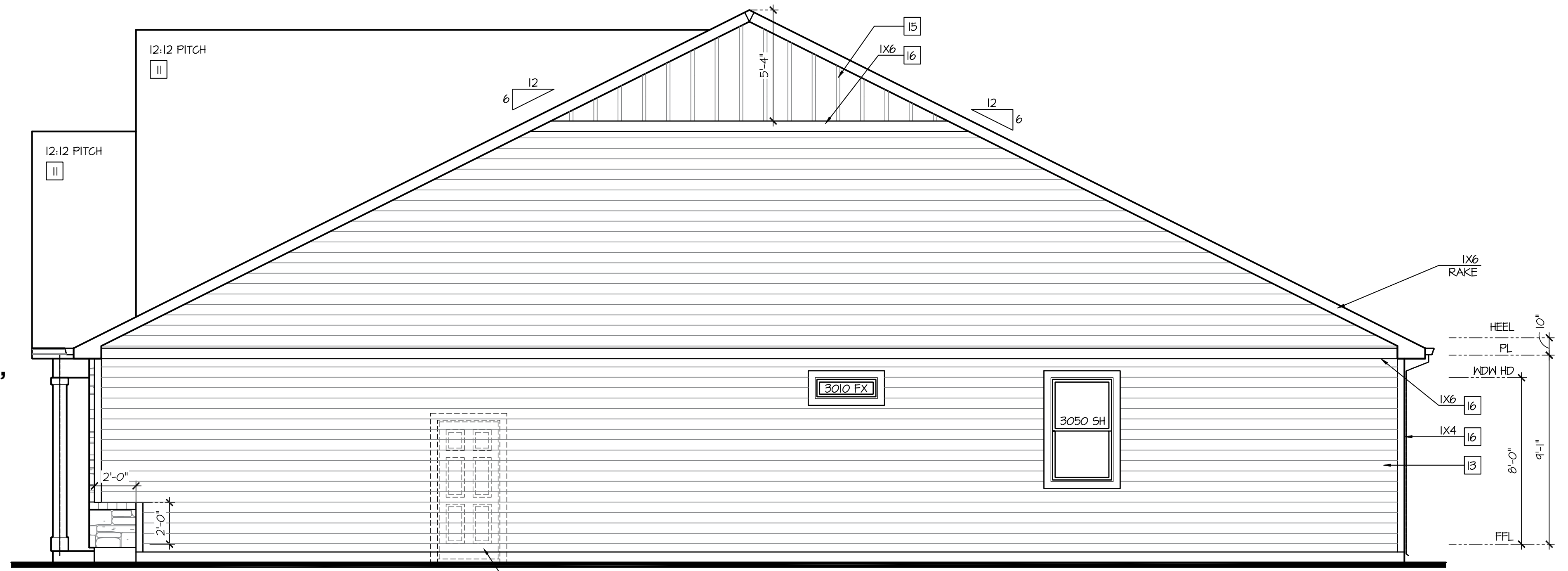


18'-5"

The Clifton



19'-4"



Scale: 1/4" = 1'-0"



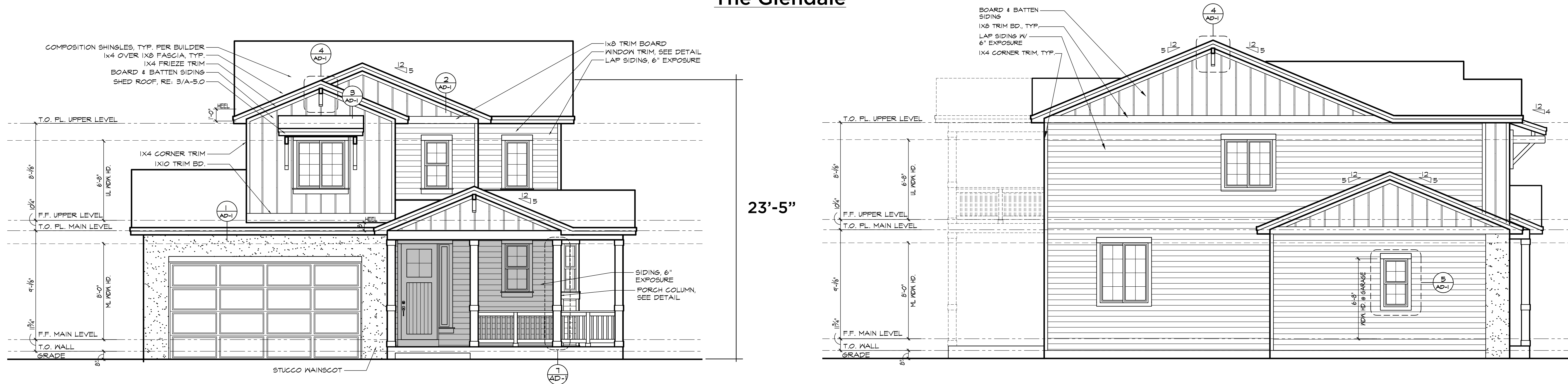
LAKEVIEW HOLDINGS, LLC.

BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
PROPOSED ARCHITECTURE STYLES

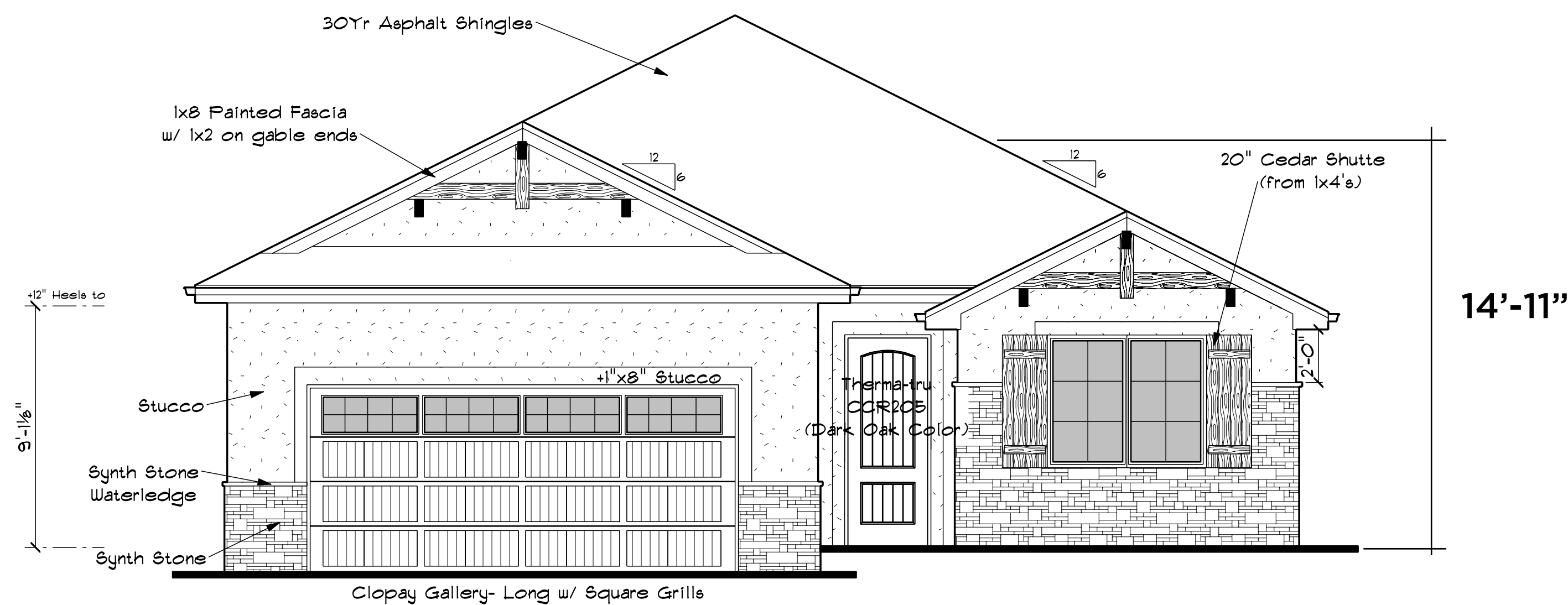
DATE	REVISION COMMENTS	PROJECT #
6/14/18	1st Resubmittal	215026
7/12/18	2nd Resubmittal	

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14
14 OF 15

The Glendale



The Crabapple



Scale: 1/4" = 1'-0"



LAKEVIEW HOLDINGS, LLC.

BOYD LAKE PLACE
PRELIMINARY DEVELOPMENT PLAN
PROPOSED ARCHITECTURE STYLES

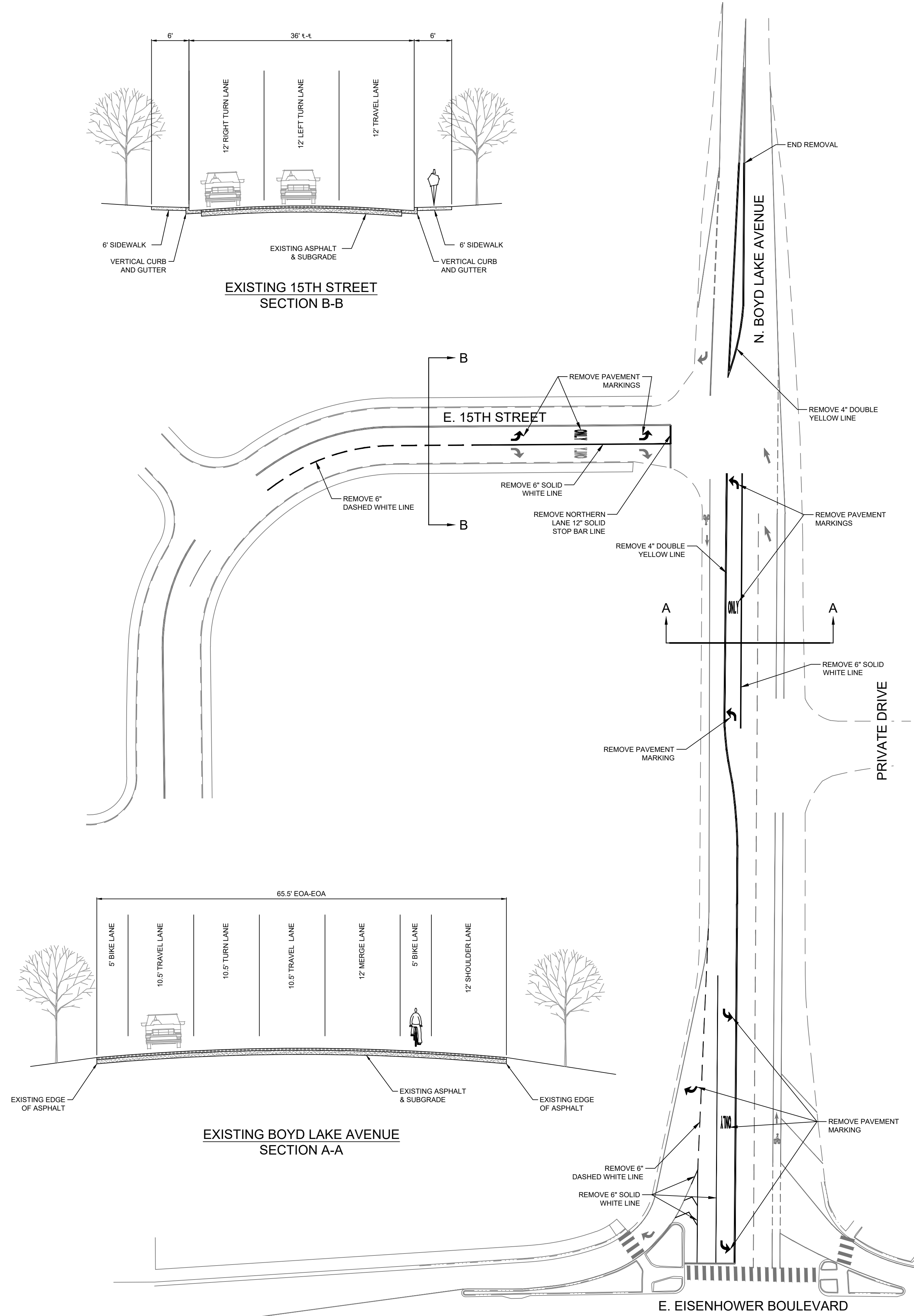
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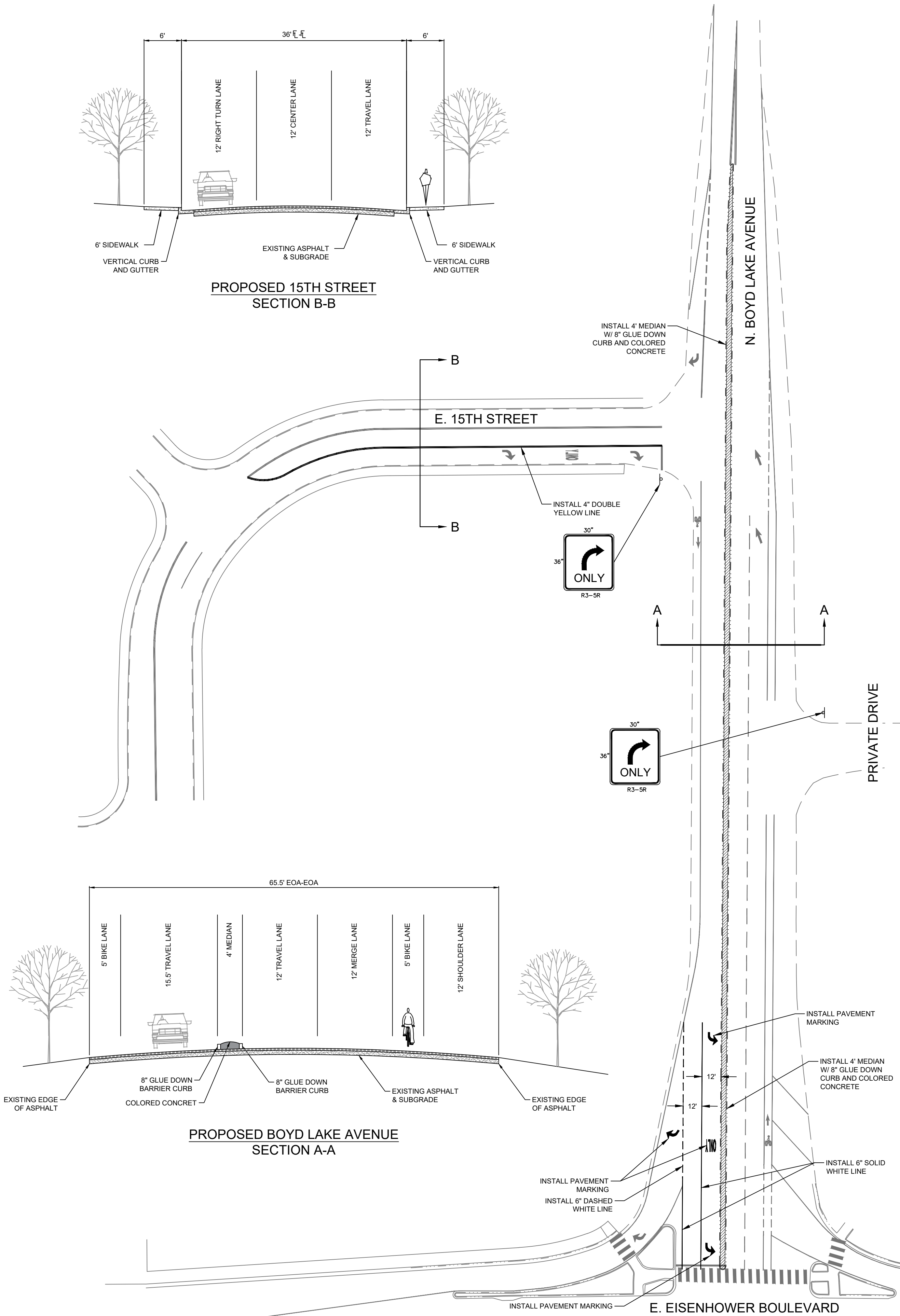
15

15 OF 15

EXISTING BOYD LAKE AVENUE / 15 STREET INTERSECTION



PROPOSED BOYD LAKE AVENUE / 15 STREET INTERSECTION



Revisions:
No. _____
Date: _____
REVIEW SET
NOT FOR CONSTRUCTION
JULY 10, 2018

These drawings are provided by Northern Engineering Services, Inc. and are not to be used for any other project without the written consent of a Professional Engineer in the State of California. Northern Engineering Services, Inc.

N **E** **NORTHERN ENGINEERING**
9341 COLLEGE AVE., SUITE 100, BOYD LAKE, CA 94921
970.221.1100
nortnengr.com

PROJECT:	884-007	DATE:	JULY 10, 2018
DESIGNED BY:	C. Snowden	SCALE:	1" = 30'
DRAWN BY:	C. Snowden	REVIEWED BY:	C. Snowden

BOYD LAKE PLACE
BOYD LAKE AVENUE
IMPROVEMENTS

Sheet
C4.00
10 of 10



Current Planning Division

410 E. 5th Street • Loveland, CO 80537
(970) 962-2523 • eplan-planning@cityofloveland.org
www.cityofloveland.org/DC

Planning Commission Staff Report

July 23, 2018

Agenda #: Regular Agenda - 1
Title: Boyd Lake Place Addition & PUD
Applicant: Lakeview Holding, LLC,
represented by Chad Rodriguez
Request: **Annexation and PUD Preliminary
Development Plan**
Location: Southwest corner of Boyd Lake
Avenue and Frank Road, west of
High Plains School.
Existing Zoning: County FA -Farming
Staff Planner: Kerri Burchett

Staff Recommendation

APPROVAL

Recommended Motions:

Move to make the findings listed in Section VII of the Planning Commission staff report dated July 23, 2018 and, based on those findings recommend that City Council approve the following:

- 1. The Boyd Lake Place Addition, subject to the conditions listed in Section VIII, as amended on the record and zone the addition Boyd Lake Place PUD; and*
- 2. The Boyd Lake Place PUD Preliminary Development Plan.*

Summary of Analysis

The public hearing is to consider the following items:

- Annexation of 27.3 acres
- Zoning to Planned Unit Development (PUD) with a Preliminary Development Plan

Boyd Lake Place Addition and PUD proposes a low density, single-family development marketed towards active adults. The property is located on the west side of Boyd Lake Avenue, across from High Plains School, and south of Boyd Lake Shores, a single-family subdivision developed in the County. The proposal requests 98 single-family lots at a density of 3.6 units per acre. This density aligns with the City's Comprehensive Master Plan designation of low density residential.

The development plans for the property originally proposed a 360-unit apartment complex. Based on significant neighborhood opposition, the applicant modified the zoning proposal to single-family residential. The 5,500 square foot lot sizes, ranch-style housing plans and amenities such as a putting green, horseshoe pit, bocce lawn, trails, and covered seating areas, target an active adult demographic. The neighborhood has expressed appreciation of the land use change to single-family, however concerns have still been voiced regarding traffic and school capacity. The traffic study, traffic route analysis, and school district referral are included as Attachments 2, 3, and 4 to this report.

Staff is recommending approval of the annexation and PUD zoning as the property lies within the City's growth management area, is in compliance with statutory annexation requirements and the Intergovernmental Agreement with Larimer County, and is consistent with the City's Comprehensive Master Plan. Findings supporting the recommendation are included in Section VII of this report.

I. SUMMARY

The proposal is to annex approximately 27 acres located at the southwest corner of Frank Road and Boyd Lake Avenue and zone the property Planned Unit Development (PUD). The property is located directly west of High Plains School and townhomes developed in the Lakes at Centerra. The property is south of Boyd Lake Shores, which is a single family residential subdivision developed under Larimer County jurisdiction (see vicinity map below). The requested PUD zoning would allow the development of 98 single-family residential lots at a density of 3.6 units per acre. The density aligns with the City's Comprehensive Master Plan that designates the property as low density residential with a targeted density range of 2-4 units per acre.

Annexation and zoning is the first step in developing a residential subdivision in the City. Annexation requires findings of compliance with State Statutes regarding contiguity with municipal boundaries, an intent to develop at an urban level, and an indication that the property can be served with infrastructure. Additionally, annexations are subject to compliance with the Intergovernmental Agreement with Larimer County, which requires the annexation of properties located within the City's Growth Management Area that are eligible for annexation. In determining appropriate zoning, the City's Comprehensive Master Plan and associated philosophies set forth the City's vision for development.

The zoning for the property is proposed as a PUD Preliminary Development Plan (PDP). A PDP is a specific plan that establishes allowed uses, a preliminary site layout, landscaping, and development standards for the property, including minimum lot size, setbacks, open space, and architecture.

The PUD zone was selected by the applicant to allow flexibility in providing smaller lot sizes, as the development will be marketed towards older adults desiring less yard maintenance. The average lot size proposed is 5,500 square feet. PUD zoning was also selected to provide area neighbors with more certainty in terms of uses, density, and site layout. The neighborhood feedback has been instrumental in helping the applicant define and modify the development proposal from the original 360-unit apartment complex to the 98 single-family homes currently proposed.



The PDP site plan shows the proposed layout of the development. Vehicular access to the site would be from a full movement intersection on Frank Road that aligns with Virginia Drive to the north, and two right-in/right-out accesses from Boyd Lake Avenue. Frank Road is currently under County jurisdiction and will be annexed to the City with this proposal. A future connection to the Lost Creek roundabout, south of the property, will be made when the property to the south is annexed into the City and developed. The neighborhood has expressed concerns regarding the timing of the future connection and associated traffic impacts on Frank Road and the Boyd Lake Shores Subdivision. Please refer to Section VI.B of this report for information regarding neighborhood concerns.

The proposed development includes detached sidewalks with a tree lawn along all streets and an off-street pedestrian trail that loops around the perimeter of the site. Approximately 6 acres of open space would be developed and enhanced including an amenity area with a putting green, horseshoe pit, bocce lawn, and covered seating with a viewing scope and grill.

PDP Site Plan



As the development will be marketed towards active adults, houses within the development would primarily be ranch style with building heights ranging from 18 to 24 feet, measured as the average between the peak and the eave. Development standards for streetscape, garages, building materials, and diversity in home elevations have been incorporated in the PDP. These standards include a minimum 25% masonry on all street facing elevations, anti-monotony provisions that restrict homes with similar front elevations from being constructed next to each other, limitation of garage doors to 47% of the building façade, and 75% of homes having garages recessed behind the house or a covered porch.

Typical Building Elevations



II. ATTACHMENTS

- 1. PDP Narrative provided by the Applicant
- 2. Thompson School District Impact Report
- 3. Traffic Impact Study
- 4. Traffic Memo and Route Analysis
- 5. Environmental Sensitive Areas Report Excerpt
- 6. Correspondence to City from the Surrounding Property Owners
- 7. Correspondence to Applicant from the Surrounding Property Owners
- 8. Annexation Map
- 9. PUD Preliminary Development Plan
- 10. East 15th Street-Boyd Lake Avenue Intersection ACF Mitigation

III. SITE DATA

ACREAGE OF SITE GROSS	27.3 AC
MASTER PLAN DESIGNATION	LOW DENSITY RESIDENTIAL
EXISTING ZONING	LARIMER COUNTY FA FARMING
PROPOSED ZONING	PLANNED UNIT DEVELOPMENT
EXISTING USE	VACANT
EXIST ADJ ZONING & USE - NORTH	COUNTY RESIDENTIAL - SINGLE-RESIDENTIAL
EXIST ADJ ZONING & USE - SOUTH.....	COUNTY FA FARMING - VACANT
EXIST ADJ ZONING & USE - WEST.....	COUNTY FA FARMING: VACANT AND BOYD LAKE
EXIST ADJ ZONING & USE - EAST.....	MILLENNIUM PUD – RESIDENTIAL TOWNHOMES & HIGH PLAINS SCHOOL
UTILITY SERVICE – WATER, SEWER	CITY OF LOVELAND
UTILITY SERVICE – ELECTRIC	CITY OF LOVELAND

IV. KEY ISSUES

City staff believes that all technical issues have been addressed regarding the annexation and PUD zoning. At the neighborhood meeting, concerns were voiced regarding the development of the property, primarily due to traffic impacts. Please refer to Section VI.B for a description of neighborhood comments. Additionally, information received from the neighborhood, including letters of support and opposition, are included as **Attachments 6 and 7**.

V. BACKGROUND

- A. **Physical Property:** The 27.3 acre property is vacant, zoned FA Farming in Larimer County, and has been historically used for farming operations of row crops and hay production. There are no mature vegetation stands on the site and the topography is nearly level. A small portion of the Greeley-Loveland Irrigation Company Canal is located in the northwest corner of the property.
- B. **Initial 1998 Annexation and Zoning Proposal:** Annexation of the property was originally proposed in 1998 and was called Lakeview Estates Addition. The proposal was for PUD zoning for a 289 unit residential development containing single-family and multifamily uses. The annexation and PUD were subsequently withdrawn.
- C. **Initial 2017 Annexation and Zoning Proposal:** The Boyd Lake Place Addition annexation proposal was submitted to the City in October of 2017. A zoning of R3, High Density Residential was requested to develop a 360-unit apartment complex. A comprehensive plan amendment was concurrently proposed to modify the property designation from Low Density to High Density Residential. A neighborhood meeting was held in December of 2017 and approximately 190 residents attended. The opposition to the project was significant and based on the neighborhood's concerns, the land use was modified to single-family residential.

VI. STAFF, APPLICANT, AND NEIGHBORHOOD INTERACTION

- A. **Notification:** An affidavit was received from Chad Rodriguez, the representative for Lakeview Holding, LLC, certifying that written notice was mailed to all property owners within 1,200 feet of the property, along with those property owners who attended the neighborhood meetings, on July 3, 2018. Notices were posted in prominent locations on the perimeter of the site also on July 3, 2018. There are no severed mineral owners associated with the property. A notice of the public hearing was published in the Reporter Herald on July 7, 2018.
- B. **Neighborhood Response:** A neighborhood meeting for the single-family proposal was held at 5:30 p.m. on May 25, 2018 at Embassy Suites. The meeting was attended by 85 neighbors and interested parties along with City staff and consultants. At the meeting, the neighborhood expressed great appreciation for the land use change to single-family residential, however they continued to voice concerns regarding traffic impacts, school impacts, and orientation of homes to Frank Road.
 - 1. **Traffic Impacts:** Traffic impacts from the proposed development have been the primary concern voiced by the neighborhood. Specific concerns regard traffic routes and a fear of cut through traffic on Valley Oak Drive through the Boyd Lake Shores Subdivision, lack of access to the Lost Creek roundabout south of the project, and the current congestion on Boyd Lake Avenue at the bridge by 15th Street and Eisenhower Boulevard.

- a. Cut-Through Traffic: A route analysis comparison, examining the likelihood of drivers using roads within Boyd Lake Shores to go north as opposed to using Boyd Lake Avenue, was conducted by the applicant's traffic engineer and is included as **Attachment 4**. The analysis indicates that in determining traffic distribution and trip distribution potential, traffic engineering principles are focused on the delay that a driver will experience when traveling on a particular street during a specific time interval. Drivers tend to take the quickest route. Based on the design, geometry, and characteristics of Boyd Lake Avenue, the analysis concludes that the majority of the project's generated traffic will utilize Boyd Lake Avenue as it is a quicker path instead of traveling through the roads in the Boyd Lake Shores Subdivision that will take longer to navigate.

- b. Access to Lost Creek Roundabout: Boyd Lake Avenue has a center median installed along the length of the project's frontage, preventing left turns out of the development directly onto Boyd Lake Avenue. There are 2 roundabouts within Boyd Lake Avenue north and south of the site. The northern roundabout is located at the intersection of Frank Road and the southern roundabout is at Lost Creek Dr. The development will have access to the northern roundabout via Frank Road. In regards to the Lost Creek roundabout, there is private property in the County situated between Boyd Lake Place PUD and the extension of Lost Creek Drive that prevents connection (see diagram to the right). While a future connection to this roundabout is planned once the private property to the south is annexed and developed, at this time the connection is not required to meet City standards based on the trips generated from the 98 homes. The neighborhood has voiced concerns that the lack of connection to the roundabout when the project develops will result in greater traffic on Frank Road.



- c. Congestion on Boyd Lake Avenue: In terms of overall congestion and pedestrian facilities on Boyd Lake Avenue, the City is currently constructing road improvements and sidewalks at the Eisenhower intersection, including a roundabout south of Eisenhower Boulevard at Mountain Lion Drive. The improvements will help alleviate peak hour delays and promote safety for pedestrians. Another future roundabout is anticipated at the intersection of 15th Street and Boyd Lake Avenue. The City has reviewed preliminary designs for the roundabout, however timing and funding for construction has not been established. Alternative options including medians that would limit turning movements on Boyd Lake Avenue and 15th Street are currently being reviewed by City staff. A raised median and access restrictions will bring this intersection into compliance with City standards as shown in **Attachments 4 and 10**. Please refer to the Transportation analysis in Section VII of this report for additional information.

2. School Impacts: The neighborhood expressed concerns regarding the capacity of High Plains school to accommodate children from the development. While the development will be marketed towards active adults, the development will not be age restricted and can therefore house families with children who would attend into High Plains. The Thompson School District has provided referral comments (**Attachment 2**) that indicate that when viewed from a feeder-system

perspective, the District anticipates that the overall impact from the project will be minimal. The District evaluated the development as a standard single-family development, not based on the target market of active adults. At build-out, the District projects that the development will produce a growth of approximately 19 students over the six elementary grades, based on current yield ratios. At the current student population levels, the increase will bring High Plains closer to capacity. The District further indicates that both the middle and high school impacts will be minimal.

3. Orientation of Homes to Frank Road: Frank Road is a local public road currently within County jurisdiction. The roads in the subdivision have been maintained by the Boyd Lake Shores Home Owners Association. With the annexation of Boyd Lake Place Addition, Frank Road will be annexed into the City as a local residential street and maintenance responsibility will be transferred to the City. Local residential roads permit driveway accesses and currently there are 6 homes in Boyd Lake Shores that have driveway accesses on Frank Road. The PDP proposes 12 single-family homes with driveway accesses on the south side of Frank Road. Four of the homes nearest the Boyd Lake Avenue intersection will have shared driveway accesses, as shown on the site plan on page 3 of this report. While the accesses meet the City's street standards, some members of the neighborhood have expressed concerns regarding congestion and safety of pedestrians with new driveways on Frank Road. The Boyd Lake Shores Subdivision does not have sidewalks and there is no sidewalk along the north side of Frank Road. With the development of Boyd Lake Place PUD, Frank Road will be widened on the south side and have curb, gutter, and a detached sidewalk installed. The sidewalk will improve pedestrian safety along Frank Road.

Frank Road Aerial View



VII. FINDINGS AND ANALYSIS

The chapters and sections cited below are from the Loveland Comprehensive Plan and Loveland Municipal Code. Applicable findings contained in the Comprehensive Plan and Municipal Code are specified in italic print followed by the staff analysis as to whether the findings are met by the submitted application.

Annexation and Zoning

A. Annexation Policies and Eligibility

1. Loveland Comprehensive Master Plan, Section 4.2

- a. Annexation ANX2.A:** *Whether the annexation encourages a compact pattern of urban development.*
- b. Annexation ANX2.B:** *Whether the annexation would result in the creation of an enclave*
- c. Annexation ANX5.B:** *Whether the applicant has demonstrated that reasonable efforts have been made to assemble adjoining land parcels to allow for the preparation of a master plan for a larger area, rather than submit separate individual proposals.*
- d. Annexation ANX1.C and 6:** *Whether the annexation encourages infill development and ensures that land is immediately contiguous to other land in the City that is already receiving City services, discouraging leapfrog and scattered site development.*
- e. Growth Management GM7:** *Whether the land proposed for annexation is within the City of Loveland Growth Management Area.*

2. Loveland Municipal Code, Section 17.04.020: *The annexation complies with the laws of the State of Colorado regarding annexation and the property proposed for annexation is otherwise eligible to be annexed because there is at least one-sixth contiguity between the City and the area seeking annexation and there is no evidence that two or more of the following conditions have been met:*

- a.** *Less than 50% of the adult residents of the area proposed to be annexed use some of the recreation, civic, social, religious, industrial or commercial facilities of the municipality and less than 25% of its adult residents are employed in the annexing municipality.*
- b.** *One-half or more of the land proposed to be annexed is agricultural, and the landowners of such agricultural land have expressed an intention under oath to devote the land to agricultural use for at least five years.*
- c.** *It is not physically practical to extend urban service which the municipality provides normally.*

Planning: Staff believes that this finding can be met, based on the following facts:

- The annexation complies with the Colorado State Statutes regarding annexation of lands and is within the City's Growth Management Area (GMA).
- No enclaves will be created by this annexation and there is no evidence that two or more of the conditions listed in Section 17.04.020 of the Municipal Code, cited above, have been met.
- The development of the property will encourage a compact pattern of urban development and will not be leapfrog or scattered site development. The land is immediately contiguous to the Lakes at Centerra residential development and the High Plains School that are receiving City services.
- The annexation complies with the Intergovernmental Agreement with Larimer County to annex property within the City's GMA that are eligible for annexation.

B. City Utilities/Services and Transportation

1. Loveland Comprehensive Master Plan, Section 4.2

a. Annexation ANX1.A and B: *Whether the annexation of land minimizes the length of vehicle trips generated by development of the land and whether the annexation minimizes the short and long term costs of providing community facilities and services for the benefit of the annexed area.*

2. Loveland Municipal Code

a. Section 17.04.040:

(i) Whether certain public facilities and/or community services are necessary and may be required as a part of the development of any territory annexed to the City in order that the public needs may be served by such facilities and services. Such facilities include, but are not limited to, parks and recreation areas, schools, police and fire station sites, and electric, water, wastewater and storm drainage facilities. Such services include, but are not limited to, fire and police protection, provision of water, and wastewater services.

(ii) Whether the annexation and development pursuant to the uses permitted in the zone district will create any additional cost or burden on the existing residents of the City to provide such facilities and services in the area proposed for annexation.

(iii) The annexation complies with the water rights requirements set forth in Title 19 of the Loveland Municipal Code.

b. Section 17.04.040,: *Whether all existing and proposed streets in the newly annexed property are, or will be, constructed in compliance with City street standards, unless the City determines that the existing streets will provide proper access during all seasons of the year to all lots and that curbs, gutters, sidewalks, bike lanes, and other structures in compliance with City standards are not necessary to protect public health, safety, and welfare.*

c. Section 18.04.010: *The zoning, as proposed, would: lessen congestion in the streets; secure safety from fire, panic, and other dangers; and promote health and general welfare.*

Transportation: Staff believes that this finding can be met, based on the following facts:

- A Traffic Impact Study (TIS), prepared by Kellar Engineering, P.E., has been submitted with the Boyd Lake Place Annexation and Preliminary Development Plan (PDP) which demonstrates that the existing and proposed transportation system, can adequately serve the proposal (see **Attachment C, D and E**).
- Access to the development will be provided by a proposed full-movement access to Frank Road and connection to Boyd Lake Avenue via the existing roundabout intersection. Additional right-in/right-out accesses will be provided directly onto Boyd Lake Avenue and a stubbed connection to the south boundary is being provided which would allow for a future connection to the roundabout at Boyd Lake Avenue and Lost Creek Drive.
- The TIS has demonstrated that all of the proposed accesses and street connections comply with LCUASS standards and meet the City's Adequate Community Facilities (ACF) requirements.
- Additional improvements off-site at the intersection of E 15th Street and Boyd Lake Avenue are proposed to be constructed to comply with ACF requirements. The City Engineer may consider a cash-in-lieu payment to satisfy this obligation.

- The proposed development is estimated to generate approximately 942 daily trips, 75 weekday AM peak hour trips, and 99 weekday PM peak hour trips.
- Conditions of approval are included that requires special conditions of the Developer to ensure compliance with the City's street standards and adequate facilities ordinance.

In conclusion, the development of the subject property pursuant to any of the uses permitted by right under the zoning district will not adversely impact any existing City infrastructure. A positive determination of adequacy for transportation facilities for the proposed application has been made under the provisions of paragraph i, above.

Fire: Staff believes that this finding can be met, based on the following facts:

- The site will comply with the requirements in the ACF Ordinance for response distance requirements from the first due Engine Company.
- The annexation of the property for development of residences will not negatively impact fire protection for the subject development or surrounding properties, and will be required to meet requirements of the currently adopted Fire Code at time of development.

Water/Wastewater: Staff believes that this finding can be met, based on the following facts:

- The subject annexation is situated within the City's current service area for both water and wastewater. There is an existing wastewater main adjacent to the site to the west. There is an existing water main in Boyd Lake Avenue.
- The Department finds that the annexation and zoning is consistent with the Department's Water and Wastewater master plan by being consistent with the 2005 Comprehensive Master Plan.
- Public water and wastewater facilities are available to serve the development.
- Conditions of approval are included that requires special conditions of the Developer to serve any future development within the annexation.

Power: Staff believes that this finding can be met, based on the following facts:

- The annexation and zoning is consistent with the Department's Power master plan by being consistent with the 2016 Comprehensive Master Plan.
- Public facilities are available to serve the development.
- The proposed development currently lies in the certified service territory of Xcel Energy. Upon completion of successful annexation, future development of the proposed annexation will be served power by the City of Loveland.

Stormwater: Staff believes that this finding can be met, based on the following facts:

- With the annexation and future development, the Developer will engineer certain Stormwater facilities that will adequately collect, detain, and release Stormwater runoff in a manner that will eliminate off-site impacts.
- Development of the subject property pursuant to any of the uses in the PUD would result in impacts on City infrastructure and services that are consistent with current infrastructure and service master plans.

C. Land Use

1. Loveland Comprehensive Master Plan

- a. **Land Use Plan:** *Whether the zoning is consistent with the Loveland Comprehensive Master Plan Land Use Plan or a "major plan amendment" request is being processed concurrently with the annexation and GDP application.*

Planning: Staff believes that this finding can be met, based on the following facts:

- The Comprehensive Master Plan designates the site as Low Density Residential with a target density range of 2-4 units per acre. The requested PUD density of 3.6 units per acre aligns with the low density residential designation in the Master Plan.
- The development will include detached sidewalks, an off-street trail system, and comfortable streets for biking, which is designated as the highest transportation priority mode in the Comprehensive Plan.
- Development standards promoted by the Comprehensive Master Plan have been incorporated into the PUD that sets forth an attractive streetscape with anti-monotony housing standards and garage provisions to avoid streets visually dominated by garages.
- The PUD was selected to allow flexibility to achieve quality design and a smaller lot neighborhood to accommodate a housing need.

2. Loveland Municipal Code

a. **Section 18.04.010:**

- (i) *Whether the PUD zoning will provide adequate light and air; prevent overcrowding of land; avoid undue concentration of population; and facilitate the adequate provision of transportation, water, sewage, schools, parks, and other public requirements.*
- (ii) *The character of the district and the particular uses in the PUD will preserve the value of buildings and encourage the most appropriate use of land.*

Planning: Staff believes that this finding can be met, based on the following facts:

- Development of the property will provide adequate light and air and prevent overcrowding of the land. The average lot size in the PUD is 5,500 square feet and setbacks will provide for a minimum of 10 feet between homes. 5.93 acres of the site will be dedicated to open space via walking paths, amenity areas, bufferyards and enhanced stormwater detention areas.
- The character of the district will preserve the value of buildings and encourage the most appropriate use of the land. The land use requested of low density residential is consistent with the single-family residential to the north and medium density townhomes to the east.
- As the project is contiguous to existing developments receiving city services, an extension of infrastructure services is practical. The developer will be required to construct needed infrastructure to serve the development.

- b. **Section 18.41.050.E.2.b** *The preliminary development plan meets the intent and objectives of Chapter 18.41 and the factors set forth in Section 18.41.050(D)(4)(b) and (c).*

1) Section 18.41.050.D.4(b): *Whether the proposed development will have a detrimental impact on property that is in sufficient proximity to the proposed development to be affected by it. If such impacts exist, the planning division shall recommend either disapproval or reasonable conditions designed to mitigate the negative impacts.*

Planning: Staff believes that this finding can be met based on the following facts:

- Neighborhood feedback has been incorporated into the project, modifying the use from the original multifamily apartment proposal to the single-family development. Building heights for the homes has also been reduced from the 35 feet permitted in the Municipal Code to a maximum of 26 feet, with housing models generally identified between 18-23 feet in height, measured in accordance with the Code. The neighborhood had voiced concerns regarding building heights and view corridors with the apartment proposal. Building heights in the Municipal Code are measured as an average between the peak and eave of the structure.
- The PDP incorporates design standards, architecture and landscaping that will promote a high quality development. The streetscape on Boyd Lake Avenue has been designed to provide a meandering 6-foot detached sidewalk and varied bufferyard widths to create interest along the street and relate to the informal streetscape on the east side of the road.

2) Section 18.41.050.D.4(c): *Whether the proposed development will be complementary to and in harmony with existing development and future development plans for the area in which the proposed development is to take place by:*

(i) Incorporating natural physical features into the development design and providing sufficient open spaces considering the type and intensity of use.

(ii) Incorporating site planning techniques that will foster the implementation of the city's master plans, and encourage a land use pattern that will support a balanced transportation system, including auto, bike, and pedestrian traffic, public or mass transit, and the cost effective delivery of other municipal services consistent with adopted plans, policies and regulations of the City.

(iii) Incorporating physical design features that will provide a transition between the project and adjacent land uses through the provisions of an attractive entryway, edges along public streets, architectural design, and appropriate height and bulk restrictions on structures.

(iv) Incorporating identified environmentally sensitive areas, including but not limited to wetlands and wildlife corridors, into the project design.

(v) Incorporating an overall plan for the design of the streetscape within the project, including landscaping, auto parking, bicycle and pedestrian circulation, architecture, placement of buildings and street furniture.

Planning: Staff believes that these findings can be met based on the following facts:

- *(finding i)* Passive and active open space is provided through-out the development, including off-street walking trails, outdoor covered gathering areas, and programmed amenities such as a putting green, bocce ball lawn and horseshoe pit.
- *(finding ii)* Specific site planning techniques have been incorporated into the PUD to promote safety for pedestrians, including detached sidewalks throughout the development and a neck-down of the street intersection adjacent to the primary amenity area. These techniques promote the philosophies of the City's master plan.
- *(finding iii)* The PDP incorporates design elements that will create attractive edges, including detached sidewalks with a treelawn, bufferyards, and a perimeter fence with stone columns along Boyd Lake Avenue. All landscaping will meet or exceed City bufferyard standards.
- *(finding iv)* The site has been historically farmed and there are no mature stands of vegetation. A small portion of the Greeley-Loveland Irrigation Canal is located in the northwest corner of the site, near the proposed detention pond in the development, and will

be left undisturbed. Landscaping within the western open space areas will be native to encourage low water usage and increase habitat value.

- (*finding v*) Improvements to Boyd Lake Avenue and Frank Road are proposed with the PDP. The street edges will be designed and landscaped consistently with the Code. A meandering, informal bufferyard will be located along Boyd Lake Avenue and all roads within the PUD will have detached sidewalks.

3. **Section 18.41.050.E.2.c:** *The PDP complies with applicable land use and development regulations in effect as of the date that the GDP was approved and any land use and development regulations adopted by the City after that date if the Planning Division and Planning Commission expressly find that compliance with such regulations is necessary to protect public health, safety, and welfare.*

Planning: Staff believes that this finding can be met based on the following fact:

- The PDP complies with applicable development regulations for land use and infrastructure in the Municipal Code. As the property will be developed in 1 phase, a preliminary development plan was selected to establish the zoning, as opposed to a general development plan. The finding concerning compliance with a general development is not applicable to the project.

D. Environmental Impacts

1. Loveland Comprehensive Master Plan, Section 4.2

- a. **Annexation ANX3.A:** *Whether the annexation will comply with the recommendations contained in the adopted Open Lands Plan and preserves open space or natural areas.*

Annexation ANX3.B: *Annexation will be allowed for the purpose of preserving or acquiring open space or natural areas.*

Annexation ANX4.A and B: *If the planning staff and/or the City have determined that significant negative impacts on the environment may occur from development allowed under the proposed zoning, an Environmental Impact Report, including a Wetlands Reconnaissance Report, has been prepared by a qualified specialist.*

Annexation ANX4.B: *Whether the annexation application includes a Phase I Environmental Report, prepared by a qualified specialist, ensuring that the land to be annexed does not contain hazardous or toxic substances that may pose a danger to the City or that reasonable mitigation measures can be taken in the event that such contamination exists.*

Annexation ANX4.D: *All development agreements must deal satisfactorily with any environmental impacts upon the property.*

Parks and Recreation: Staff believes that this finding can be met, based on the following facts:

- This property does not directly front onto Boyd Lake, however a small portion of the Greeley Loveland Irrigation Canal is located in the northwest portion of the site. No development adjacent to the canal is proposed.
- An environmentally sensitive report is included as **Attachment F**. The recommendations from the report have been incorporated into the PUD development standards. The use of native tree, shrub, and grass species will be emphasized along the western open space areas to reduce water usage and improve habitat for wildlife.

E. Miscellaneous

- 1. Loveland Municipal Code, Section 17.04.040.F:** *Whether the annexation is in the best interest of the citizens of the City of Loveland.*

Planning: Staff believes that this finding can be met, based on the following facts:

- The annexation and zoning of the property into a low density residential development is compatible with development in the surrounding area. Future subdivision plats will need to demonstrate compliance with the standards established in the PUD that promote a high quality development, along with City standards for infrastructure.
- A 6-foot detached sidewalk on Boyd Lake Avenue will be constructed with the development, which will provide greater pedestrian safety along Boyd Lake Avenue, particularly in light of pedestrian paths to the High Plains School.

- F. Mineral Extraction Colorado Revised Statute:** *The proposed location and the use of the land, and the conditions under which it will be developed, will not interfere with the present or future extraction of a commercial mineral deposit underlying the surface of the land, as defined by CRS 34-1-3021 (1) as amended.*

Planning: Staff believes that this finding can be met, due to the following fact:

- A geotechnical subsurface exploration report was submitted by Earth Engineering Consultants, Inc. The report evaluated the potential for the site to contain commercial mineral deposits as defined by State Statutes. The report concluded that no economically recoverable mineral resources were found.
- There are no severed mineral owners on the property.

VIII. RECOMMENDED ANNEXATION CONDITIONS

The following conditions are recommended by City Staff.

Transportation Development Review

1. All public improvements shall comply with the Larimer County Urban Area Street Standards (LCUASS).
2. The developer agrees to acquire and dedicate, at no cost to the City, any rights-of-way necessary for the required street improvements associated with this development.
3. Prior to the issuance of any building permits within the Boyd Lake Place Subdivision, pursuant to the provisions in Section 16.40.010.B of the Loveland Municipal Code, the Developer shall design and construct the following improvements as shown in the City approved Public Improvement Construction Plans for Boyd Lake Place:
 - a. All adjacent, onsite and offsite public improvements on Frank Road, Boyd Lake Avenue and all internal public streets that serve the development within the subdivision as shown in the City approved Public Improvement Construction Plans.

b. Off-site improvements at the intersection of Boyd Lake Avenue and E 15th Street to mitigate any failure of the City's ACF standards as shown on the City approved Public Improvement Construction Plans. The City Engineer may consider cash-in-lieu of construction to meet this obligation.

c. The City will evaluate the operational safety of the existing pedestrian activated crossings located at the roundabout intersections at Boyd Lake Ave./Frank Road and at Boyd Lake Ave./Lost Creek Drive. If any enhancements to these crossings are warranted to make them safer, as determined by the City Engineer, the Boyd Lake Place Developer will be obligated to install any recommended enhancements at no additional cost to the City. The cost for the enhancements is not to exceed \$10,000.

Water/Wastewater

4. At the time of development, the Developer shall, if not constructed by others, design and construct a 16" water main through the property. The main will need to connect to the existing 24" water main on the east side of Boyd Lake Ave near the point where it is reduced to 16" approximately 300' north of the centerline of Lost Creek Drive and be extended to the southern boundary of the property.
5. At the time of Preliminary plat, the Developer shall submit a Water and Wastewater Impact Demand Analysis. Additional criteria for the development maybe required based on the outcome of the Analysis.

Fort Collins Loveland Airport

6. Because of the proximity to Northern Colorado Regional Airport, an avigation easement shall be dedicated to the Fort Collins Loveland Airport prior to the approval of any subdivision plat. An avigation easement will allow for the unobstructed use and passage of aircraft in the airspace above the subject parcel. As part of this easement, provisions for property disclosures that identify proximity to the Airport, potential noise impacts, and future operations that may include increased noise and other associated impacts will be required.