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## Planning Commission Staff Report

June 26, 2017

**Agenda #:** Regular Agenda - 1

**Title:** Parkside Village Addition

**Applicant:** Kenneth Mitchell, Mosaic Real Estate LLC

**Request:** **Annexation, Zoning (Planned Unit Development (PUD) – Preliminary Development Plan), and Preliminary Plat**

**Location:** Northeast of Boedecker Reservoir along the south side of W. First Street

**Create Loveland Comprehensive Plan Land Use Designation:**

LDR – Low Density Residential

**Existing Zoning:** T - Tourist (Larimer County)

**Proposed Zoning:** Parkside Village PUD

**Staff Planner:** Troy Bliss

### **Staff Recommendation**

**CONDITIONAL APPROVAL** of the Annexation, Preliminary Development Plan, and Preliminary Plat.

#### **Recommended Motions:**

1. *Move to make the findings listed in Section VII of the Planning Commission staff report dated June 26, 2017, and, based on those findings, recommend that City Council approve the Parkside Village Addition, subject to the conditions listed in Section VIII, as amended on the record, and zone the addition to Parkside Village Planned Unit Development;*
2. *Move to make the findings listed in Section VII of the Planning Commission staff report dated June 26, 2017, and, based on those findings, recommend that City Council approve the Parkside Village PUD Preliminary Development Plan, and;*
3. *Move to make the findings listed in Section VII of the Planning Commission staff report dated June 26, 2017, and, based on those findings, recommend that City Council approve the Parkside Village PUD Preliminary Plat*

### **Summary of Analysis**

The public hearing is to consider the following items:

- Annexation of 41 acres of property owned by Heusinkveld/Martinson families;
- Preliminary Development Plan detailing development/zoning requirements for the proposed PUD; and;
- Preliminary Plat for the creation of lots, outlots, and tracts.

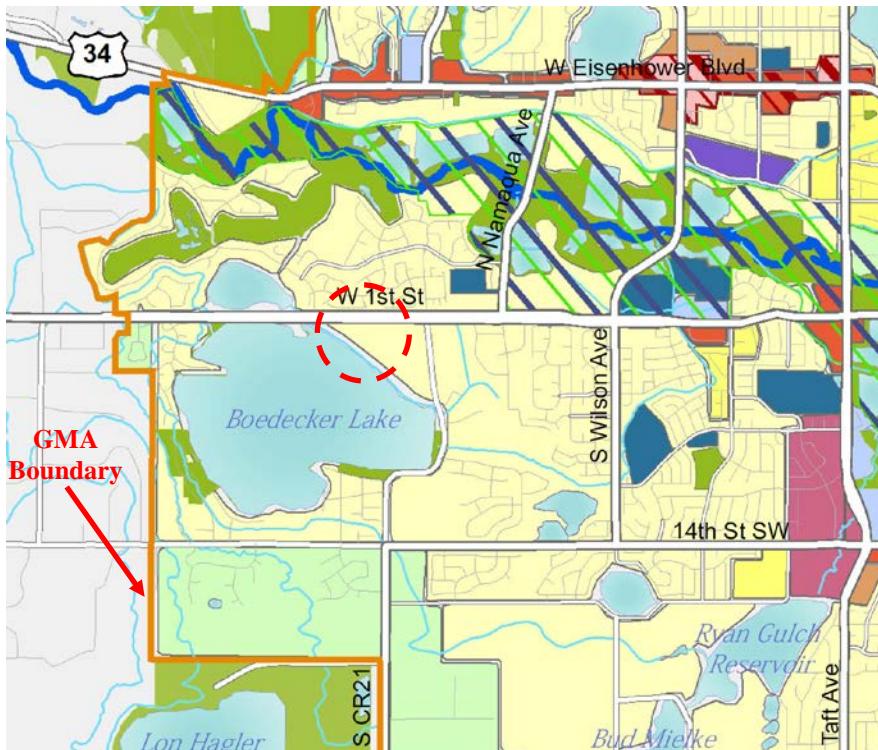
The proposal is to annex and incorporate the 41 acre property into the City for development of single-family detached homes and 14 attached two-family dwellings. Development is planned to be clustered on the eastern 24 acres of the site, leaving 17 acres as natural open space along the shoreline of Boedecker Reservoir with possible acquisition by the City's Open Lands Division.

Concerns regarding the development proposal expressed by the neighbors generally include increased traffic volumes, change in the rural character of the area, density, size and quality of lots/homes, and impacts to wildlife habitats.

## I. SUMMARY

This proposal is to annex 41 acres of land owned by the Heusinkveld/Martinson families and rezone from its current Larimer County designation of T – Tourist to PUD. A Preliminary Development Plan would establish the PUD zoning by detailing all facets of development design. The primary purpose in pursuing PUD zoning for this project is to achieve a level of flexibility for a clustered development on approximately 24 acres – protecting the shoreline of Boedecker Reservoir and its surrounding natural habitats.

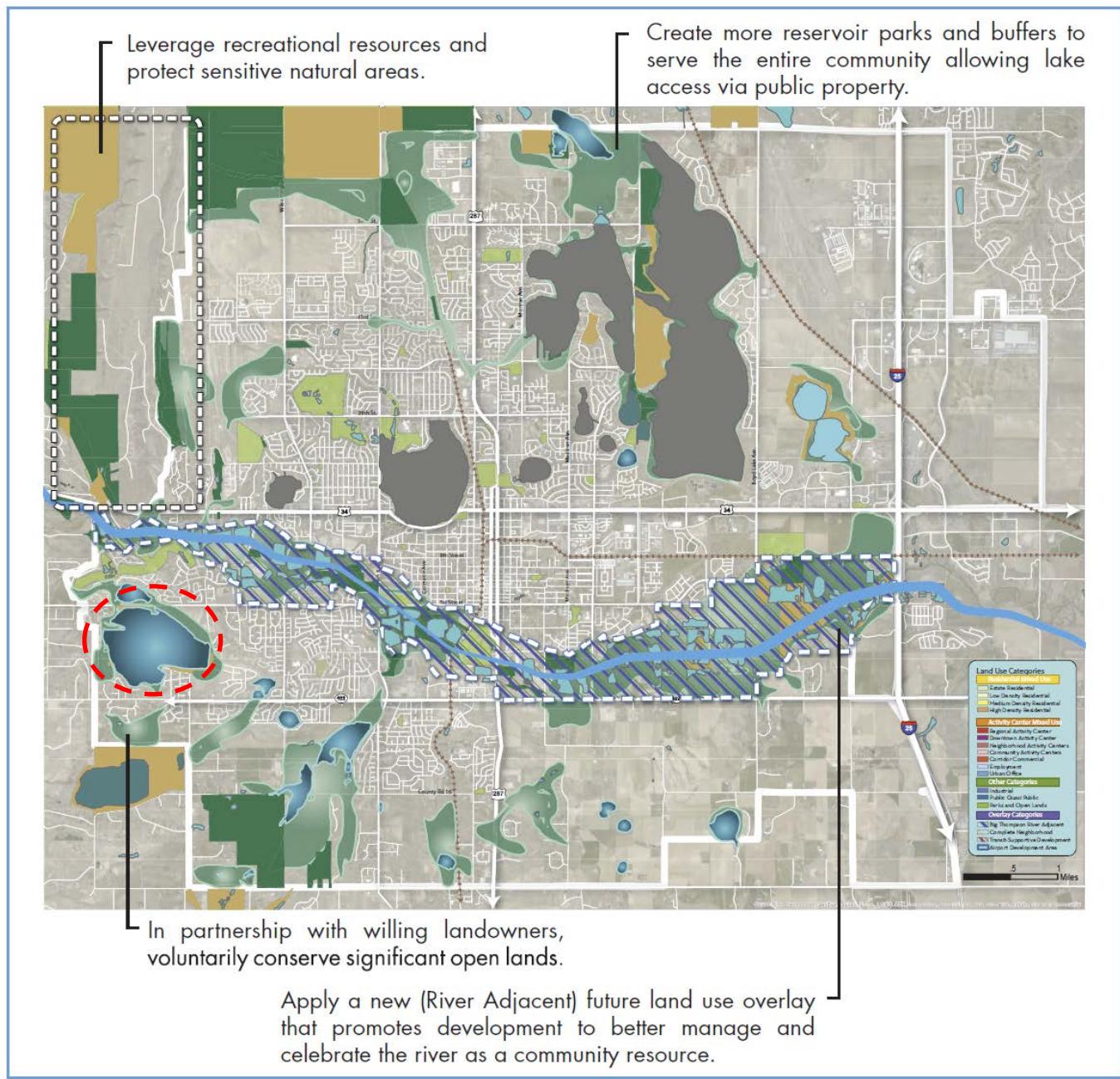
The zoning would allow for the development of 62 detached single-family dwellings and 14 paired two-family dwellings for a total of 76 units. Density of the development would fall at almost 2 units per acre – being at the low end of the Create Loveland Comprehensive Plan land use designation of Low Density Residential (LDR). A Preliminary Plat has also been prepared for the creation of all lots, outlots, and tracts and future dedications for all public rights-of-way and easements.



Location/Land Use The property is located northeast of Boedecker Reservoir along the south side of W. First Street. The entire 2,400+ linear feet of property along W. First Street is contiguous to the City's municipal boundary. In addition to the land use designation, Create Loveland Comprehensive Plan identifies the location of this property within the City's Growth Management Area (GMA). The GMA is an area where urban-level services are planned to be provided and that the City intends to annex such properties (if not already incorporated).

## Site Characteristics

Boedecker Reservoir is identified in the City's Natural Areas Inventory. This inventory identifies undeveloped properties containing potential natural values including habitats important to the City in terms of protection and preservation. This document was used in defining the portion of property that the City may seek to acquire for open lands as well as setting the distance from back of lots to the shoreline (i.e. 200 foot bufferyard). Regardless of whether or not the City acquires the 17 acres, no access from private lots to the natural area will be allowed. The area in which the development is focused, includes a prairie dog colony. The Applicant has represented a willingness to work with the Prairie Dog Coalition and individuals interested in relocating the animals. If suitable site(s) is/are available and relocation details arranged, the prairie dogs will move prior to construction activities beginning. Additional information regarding the natural characteristics of the property can be found in the attached Environmentally Sensitive Areas Report (see **Attachment C**) and in neighborhood correspondence (see **Attachment B**).



## PUD Design

The zoning request to establish a PUD follows the procedures outlined in Chapter 18.41 of the Loveland Municipal Code. For purposes of creating the PUD zoning, a non-phased development approach was selected, not requiring a General Development Plan (GDP), establishing the zoning through a Preliminary Development Plan (PDP). (The term “non-phased” refers to a site-specific development plan, identifying all proposed uses, site improvements, access/circulation, and design standards.) The following outlines key design elements related to the project:

- **Traffic:** The additional traffic generated by this development proposal has been analyzed in a Traffic Impact Study (TIS) (see **Attachment D**). General conclusions indicate that the proposed development can be accommodated by the existing street system with some widening along the frontage of W. First Street to install bike lanes, curb/gutter, and sidewalks. Additionally, off-site road improvements would be needed to accommodate this development that include an interim pedestrian/bike lane extension east to Crestone Drive (on the south side of W. First Street), restriping of lanes, and a roundabout design at the intersection of Crestone and W. First Street.
- **Fire Access and Circulation:** The Fire Protection Division has reviewed the submitted plans and has indicated that fire protection will be served with fire trucks accessing the site by way of Green Valley Drive (primary/only access into the site from W. First Street) or through an emergency access point at the west end of the development connecting into the Martinson Drive cul-de-sac.
- **Density:** The gross density of the proposed development is 1.85 units per acre (76 units applied over 41 acres). Another way to calculate density would be focusing just on the 24 acres in which development would be occurring – resulting in a gross density of 3.17 units per acre. Either scenario puts the density with the targeted density of 2 to 4 units per acre for LDR – Low Density Residential land uses identified in the Create Loveland Comprehensive Plan. In comparison, development that is currently under construction in the surrounding Mariana Butte subdivision includes projects such as the Mariana Butte 27<sup>th</sup> Subdivision (along both sides of Rossum Drive, south of W. Eisenhower Boulevard - all detached single-family homes) with a gross density of 3.07 units per acre. The Mariana Butte 25<sup>th</sup> Subdivision (north side of W. First Street, between Namaqua Avenue and Crestone Drive – combination of detached single-family and paired two-family homes) has a gross density of 4.20 units per acre.
- **Setbacks and Building Separation:** The PDP proposes the following setbacks:
  - Front – The minimum front yard setback from the street is 15 feet for the main structure of the house, which shall include any porch, casita, courtyard fencing or other dominant architectural feature. Garage door fronts are not included in the 15-foot setback.  
  
Garage doors will be set back a minimum of 20 feet from the front property line and a minimum of 3 feet behind any front elevation feature, including the main structure of the house, any porch, casita, courtyard fencing or other dominant architectural element.
  - Rear – The rear yard setback shall be a minimum of 15 feet from the rear property line.
  - Corner Side – 10 feet
  - Side – All side yards shall have a minimum setback of 5 feet from the side lot line.

- **Site Access and Parking:** The PDP proposes one intersection at W. First Street (Green Valley Drive). This is the only opportunity for access into the development given intersection spacing and property frontage along W. First Street. This intersection however would be full movement, allowing access and egress in both directions along 1st Street. Additionally, if the property directly east were to ever annex and develop in the City, an east/west local street connection is being planned with this proposed development. Parking along W. First Street would be prohibited. However, parking would be allowed along all local streets within the project. Each home would have at a minimum two-car garage parking with additional parking in the driveway for at least two vehicles by incorporating 20 foot setbacks to the garage.
- **Architecture:** Home elevations have not been provided in the Preliminary Development Plan because a specific builder has not been determined. In-lieu of this detail, the Applicant has created a design narrative within the development plan along with some additional supplementary architectural requirements (see **Attachment G**). One particular design element that comes with challenges, due to a majority of the lot widths (45 to 50 feet), is the percentage of street-facing garages (which will exceed 50% on homes). In response to this design constraint, the Applicant is proposing that:
  - All homes will have an exterior living space element on the front (i.e. porch, casita, or courtyard);
  - All garages will have raised panel doors with window treatments;
  - Perimeter framing of all garage doors will incorporate stone, and;
  - Another key component that is reflected on the development plans is all internal streets will include detached sidewalks with canopy tree lawns.

The photograph below is an example of what the Applicant anticipates a typical detached single-family home to look like within the proposed development.



- **Landscaping and Open Space:** A key feature to the overall project is the clustering of homes in order to preserve and protect the natural area around Boedecker Reservoir. Seventeen acres are proposed to be set-aside for this purpose. Negotiations with the City Open Lands Division to purchase and acquire the land are in process. Regardless of

whether the City acquires the land, it is being platted as an outlot, which comes with no development entitlements. Further, conditions are being recommended to prohibit direct access from private lots to this outlot. Within the area of development, most of the open space would serve to meet the stormwater needs of the project. Within these areas, there would be spaces for

active and passive recreation. The intent for the overall project is to take advantage of the natural open space and limit the amount of maintenance on individual lots.

- **Stormwater Design:** Surface run-off would be directed primarily to the northeast corner and central areas of the site, where detention facilities are to be located that will filter out pollutants, releasing storm water. The drainage provisions have been reviewed and approved at this preliminary stage by the City's stormwater engineering division.

## II. ATTACHMENTS

- A. Project Description provided by the Applicant
- B. Neighborhood Correspondence
- C. Environmental Sensitive Areas Report
- D. Traffic Impact Study
- E. Annexation Map
- F. Preliminary Development Plan
- G. Summary of Architectural Requirements
- H. Preliminary Plat

## III. SITE DATA

ACREAGE OF SITE GROSS .....	41 AC
MASTER PLAN DESIGNATION .....	LOW DENSITY RESIDENTIAL
EXISTING ZONING .....	LARIMER COUNTY T TOURIST
PROPOSED ZONING .....	PARKSIDE VILLAGE PLANNED UNIT DEVELOPMENT
EXISTING USE .....	VACANT/UNDEVELOPED
PROPOSED USE.....	SINGLE FAMILY & PAIRED RESIDENTIAL
EXIST ADJACENT ZONING & USE - NORTH.....	MARIANA BUTTE, SINGLE FAMILY RESIDENTIAL
EXIST ADJACENT ZONING & USE - SOUTH .....	LARIMER COUNTY BOEDECKER RESERVOIR
EXIST ADJACENT ZONING & USE - WEST.....	LARIMER COUNTY RURAL RESIDENTIAL AND CONSOLIDATED HOME SUPPLY DITCH
EXIST ADJACENT ZONING & USE - EAST.....	LARIMER COUNTY RURAL RESIDENTIAL
UTILITY SERVICE – WATER, SEWER .....	CITY OF LOVELAND
UTILITY SERVICE – ELECTRIC .....	CITY OF LOVELAND

## IV. KEY ISSUES

From a staff perspective, key issues continue to include negotiations between the developer and the City Open Lands Division concerning the proposed City acquisition of approximately 17 acres for open space. Currently, the City's contract with the property owners indicates that the 17 acres would not be purchased until such time that the property has been annexed and platted, creating the separate outlot (currently labeled as Tract A – requested to be changed to an outlot) (see **Attachment H**).

Pedestrian connectivity has also been challenging in terms of the relationship to unincorporated parcels directly to the east and west – creating the inability to extend sidewalks along W. First Street. Soft trails

within the natural open spaces are conceptual based upon topography and options for public use in-lieu of extending sidewalks along W. First Street. These design details will need to be resolved with the Final Development Plan and Final Plat.

Off-site street improvements along W. First Street will continue to include refinement of the design. Currently, an interim roundabout is planned at the intersection of W. First Street and Crestone Drive that would fit within the existing street right-of-way. Additionally, an interim sidewalk would extend from the subject property east to Crestone Drive along the south side of W. First Street within the existing right-of-way providing a pedestrian crossing at the roundabout to get to the north side of W. First Street.

As outlined below in Section VI, surrounding property owners have expressed additional concerns related to the project.

## **V. BACKGROUND**

Application for annexation was initially submitted to the City in July of 2016. City staff and the Applicant have spent a considerable amount of time working to develop appropriate zoning that would provide a level of flexibility for clustering development while balancing the desire to preserve/protect important natural features of the site. Input from interested parties throughout review has helped the City establish requirements/conditions that speak to some of the concerns relating to traffic, site design, architecture, and sensitivity to natural area/habitat. Almost a year later, after a lot of preliminary design and review meetings with the Applicant's team, a plan has been developed that is in-line with the intent of Create Loveland Comprehensive Plan and is designed to be adequately served by all City infrastructure/service needs. Looking ahead beyond the Planning Commission hearing, this project will proceed onto City Council for final consideration. It is anticipated that the public hearing with City Council will occur on August 15, 2017, with notice being provided in advance.

## **VI. STAFF, APPLICANT, AND NEIGHBORHOOD INTERACTION**

**A. Notification:** An affidavit was received from Kenneth Mitchell, Mosaic Real Estate LLC, certifying that written notice was mailed to all property owners within 1,200 feet of the property on June 7, 2017 and notices were posted in prominent locations on the perimeter of the site at least 15 days prior to the date of the Planning Commission hearing. There were no mineral owners associated with the property. In addition, a notice was published in the Reporter Herald on June 10, 2017.

**B. Neighborhood Response:** A neighborhood meeting was held at 5:30 p.m. on December 8, 2016 at the City of Loveland Development Center (410 E. 5<sup>th</sup> Street). The meeting was attended by approximately 40-50 neighbors and interested parties along with City staff and the Applicant. The following is an outline of the neighborhood meeting provided by City staff:

- Representatives from the Prairie Dog Coalition and prairie dog advocates expressed a strong desire to work with the Applicant in finding suitable locations for relocating the animals instead of having them exterminated.
- The concept of clustering development created the perspective of higher density compared to surrounding subdivisions and larger county parcels. As a result of the density, more traffic volume would be created, adding more congestion to an already very busy W. First Street.

- In addition to increased traffic volume, concerns about the safety and speeds along W. First Street were expressed. How will this development improve the situation?
- This is a very rural area. Why does the City want to see urban level development? Create Loveland Comprehensive Plan is not consistent with the vision of some individual's perspectives in how the City should grow.
- The property and the area in general around Boedecker Reservoir is a very picturesque setting, full of wildlife that everyone enjoys. The idea of even developing a portion of the site is alarming. Why does the City not consider purchasing the entire site for natural open space like Rivers Edge?
- What are the price point of the homes and the impacts to surrounding property values?
- The subdivision design with smaller/narrow lots is not compatible to the surrounding area. The type of homes are going to create a tract housing development that will loom over adjacent properties and block views of the reservoir and mountains.

Additional information regarding neighbor and interested party input is included as an attachment to this report (see **Attachment B**).

## VII. FINDINGS AND ANALYSIS

### **Annexation and Zoning**

The chapters and sections cited below are from Create Loveland Comprehensive Plan and Chapter 17 of the Loveland Municipal Code.

#### **Annexation Policies and Eligibility**

1. **Create Loveland: Development Review and Consistency, Annexation:** *The annexation complies with the laws of the State of Colorado regarding annexation.*
2. **Loveland Municipal Code, Section 17.04.020:** *The annexation complies with the laws of the State of Colorado regarding annexation and the property proposed for annexation is otherwise eligible to be annexed because there is at least one-sixth contiguity between the City and the area seeking annexation and there is no evidence that two or more of the following conditions have been met:*
  - a. *Less than 50% of the adult residents of the area proposed to be annexed use some of the recreation, civic, social, religious, industrial or commercial facilities of the municipality and less than 25% of its adult residents are employed in the annexing municipality.*
  - b. *One-half or more of the land proposed to be annexed is agricultural, and the landowners of such agricultural land have expressed an intention under oath to devote the land to agricultural use for at least five years.*
  - c. *It is not physically practical to extend urban service which the municipality provides normally.*

Planning: Staff believes that this finding can be met, based on the following facts:

- The annexation complies with the Colorado State Statutes regarding annexation of lands and is within the City's Growth Management Area (GMA).
- There is no evidence that two or more of the conditions listed in Section 17.04.020 of the Municipal Code, cited above, have been met.

- The development of the property will encourage a compact pattern of urban development. The land is immediately contiguous to City limits along the northern boundary. Properties to the north are already receiving City services.
- The annexation complies with the Intergovernmental Agreement with Larimer County to annex property within the City's GMA that are eligible for annexation.

## B. City Utilities/Services and Transportation

### 1. Loveland Municipal Code

#### a. Section 17.04.040:

- (i) *Whether certain public facilities and/or community services are necessary and may be required as a part of the development of any territory annexed to the City in order that the public needs may be served by such facilities and services. Such facilities include, but are not limited to, parks and recreation areas, schools, police and fire station sites, and electric, water, wastewater and storm drainage facilities. Such services include, but are not limited to, fire and police protection, provision of water, and wastewater services.*
- (ii) *Whether the annexation and development pursuant to the uses permitted in the zone district will create any additional cost or burden on the existing residents of the City to provide such facilities and services in the area proposed for annexation.*
- (iii) *The annexation complies with the water rights requirements set forth in Title 19 of the Loveland Municipal Code.*

#### b. Section 17.04.040,: *Whether all existing and proposed streets in the newly annexed property are, or will be, constructed in compliance with City street standards, unless the City determines that the existing streets will provide proper access during all seasons of the year to all lots and that curbs, gutters, sidewalks, bike lanes, and other structures in compliance with City standards are not necessary to protect public health, safety, and welfare.*

#### c. Section 18.04.010: *The zoning, as proposed, would: lessen congestion in the streets; secure safety from fire, panic, and other dangers; and promote health and general welfare.*

Transportation: Staff believes that this finding can be met, based on the following facts:

- Annexing and zoning property does not warrant compliance with the City's Adequate Community Facilities (ACF) ordinance. A condition is recommended to clearly ensure that all future development or land application within this proposed property shall be in compliance with the City of Loveland Street Plan, the Larimer County Urban Area Street Standards and any updates to either in effect at the time of development application.
- As identified in the City Municipal Code Title 16, a Traffic Impact Study will be required with all future development or other land use applications. The annexation will also be required to dedicate, free and clear, all applicable right-of-way to the City, at no cost to the City, at the time of development.
- Pending future proposed development within this property, of which review and approval by the City is required, the Transportation Engineering staff does not object to the proposed annexation and zoning.

Fire: Staff believes that this finding can be met, based on the following facts:

- The site will comply with the requirements in the ACF Ordinance for response distance requirements from the first due Engine Company.

- The proposed annexation/zoning will not negatively impact fire protection for the subject development or surrounding properties.
- Pending future proposed development within this property, of which review and approval by the Fire Authority is required, staff does not object to the proposed annexation and zoning.

Water/Wastewater: Staff believes that this finding can be met, based on the following facts:

The subject annexation is situated within the City's current service area for both water and wastewater. Regarding water there are two adjacent 12" water stubs one in Rossum Drive and one in Green Valley Drive that will be extended and looped through the development.

Regarding wastewater there is an existing wastewater stub in Green Valley Drive that can be extended into the site to serve as many homes as possible.

The conditions herein noted require the Developer of the subject annexation to analyze the feasibility of gravity wastewater service to serve any development within the annexation.

The Department finds that:

The annexation and zoning is consistent with the Department's Water and Wastewater master plan by being consistent with the 2005 Comprehensive Master Plan.

Public facilities are available to serve the development.

Power: Staff believes that this finding can be met, based on the following facts:

- The Department finds that the annexation and zoning is consistent with the Department's Power master plan.
- The property is currently being served by the City of Loveland for power services.
- Public facilities are available to serve the development.

Stormwater: Staff believes that this finding can be met, based on the following facts:

- With the annexation and future development, the Developer will engineer certain Stormwater facilities that will adequately collect, detain, and release Stormwater runoff in a manner that will eliminate off-site impacts.
- Development of the subject property pursuant to any of the uses permitted by right under the zoning district would result in impacts on City infrastructure and services that are consistent with current infrastructure and service master plans.

## C. Land Use

### 1. Create Loveland Comprehensive Master Plan: Development Review and Consistency

- a. *The proposal is consistent with the Policies and Supporting Strategies in Chapter 2: Our Future*

Planning: Staff believes that this finding can be met, based on the following facts:

- Create Loveland identifies the area proposed for annexation as having valuable natural areas and wildlife habitat. Specifically, the plan calls out areas around Boedecker Reservoir in which the City should work with willing landowners to conserve significant open lands.
- The development supports policies contained in Create Loveland including:

- Maintain natural areas according to management type;
- Protect and maintain environmental resources and quality; and
- Maintain and expand City recreational facilities as a valuable asset to the community.

b. *The proposal is consistent with the Land Use Plan and Land Use Designations contained in Chapter 3: Our Places*

Planning: Staff believes that this finding can be met, based on the following facts:

- The land use plan designates the site as low density residential. This category can allow for a variety of housing types but is predominately detached single-family at a low density. The targeted density range is 2-4 units per acre with building heights between 1-3 stories. With the annexation property, the Parkside Village development would have a density of 1.85 units per acre, which is consistent with the plan.
- The highest priority mode of transportation in the low density residential designation emphasizes pedestrian movement with detached sidewalks, off-street trail systems and connections to neighborhoods and commercial centers. The PDP requires detached walks along all streets and emphasizes pedestrian movement through internal walks looping through the development.

## 2. Loveland Municipal Code

### a. Section 18.04.010:

- (i) *Whether the zoning will provide adequate light and air; prevent overcrowding of land; avoid undue concentration of population; and facilitate the adequate provision of transportation, water, sewage, schools, parks, and other public requirements.*
- (ii) *The character of the district and the particular uses permitted by right in the district will preserve the value of buildings and encourage the most appropriate use of land.*

Planning: Staff believes that this finding can be met, based on the following facts:

- Development of the property will provide adequate light and air and prevent overcrowding of the land. The density of the development aligns with the city's vision established in the land use plan. While the homes will be up to 2 stories, street side bufferyards and interior landscaping will be provided.
- The zoning for the property proposed for annexation will match the character of the surrounding Mariana Butte development and encourages the most appropriate use of the land, based on the city's vision in Create Loveland.
- As the project is contiguous to existing developments receiving city services, an extension of infrastructure services is practical.

## D. Miscellaneous

### 1. Loveland Municipal Code, Section 17.04.040.F: *Whether the annexation is in the best interest of the citizens of the City of Loveland.*

Planning: Staff believes that this finding can be met, based on the following facts:

- The development supports policies in the comprehensive plan.
- The proposal aligns with the city's vision for redevelopment.
- The property is within the city's growth management area and complies with the city's Intergovernmental Agreement with Larimer County.

2. **Loveland Municipal Code, Section 18.41.050.D.4.c:** *Whether the GDP incorporates environmentally sensitive areas into the project design. Environmentally sensitive areas include, but are not limited to, wetlands, wildlife habitat and corridors, slopes in excess of 20%, flood plain, soils classified as having high water table, stream corridors, and mature stands of vegetation.*

Planning: Staff believes that this finding can be met, based on the following fact:

- An environmentally sensitive areas report was submitted with the annexation and PDP and was prepared by Blue Mountain Environmental Consulting (see **Attachment C**). The report indicates unique habitat within the project area, particularly within the cottonwood gallery along Boedecker Lake. The environmental report indicates that, based upon the proposed development plan, no significant ecological resources will be adversely impacted on this site. It does suggest, however, that ethical steps be taken for removal of the prairie dogs.

**F. Mineral Extraction Colorado Revised Statute:** *The proposed location and the use of the land, and the conditions under which it will be developed, will not interfere with the present or future extraction of a commercial mineral deposit underlying the surface of the land, as defined by CRS 34-1-3021 (1) as amended.*

Planning: Staff believes that this finding can be met, due to the following facts:

- There are no severed mineral leasehold owners on the property.
- A geologic hazards and mineral extraction evaluation report was submitted with the annexation and was prepared by Earth Engineering Consultants, LLC. The report indicated that no apparent significant geologic hazards exist on the property. Additionally the report indicates that due to the existing surrounding developments, small parcel size, depth of overburden clay and relatively thin sand and gravel lens, the deposit would not classify as “a commercial resource” under Colorado House Bill –HB 1529.

**Preliminary Development Plan and Preliminary Plat**

The chapters and sections cited below are from the Loveland Municipal Code pertaining to Preliminary Plats and Preliminary Development Plans. Applicable findings contained in the Municipal Code are specified in italic print followed by the staff analysis as to whether the findings are met by the submitted application. Adequate Community Facilities (ACF) analyses are included below. The Planning Commission decision to approve or deny the applications is to be based on a majority vote on whether or not the findings can be met.

**A. City Utilities and Services**

**1. Section 16.20.030:**

- a. The proposed public facilities and services are adequate, consistent with the City's utility planning, and capable of being provided in a timely and efficient manner*
- b. The subdivision complies with the water rights requirements in Title 19.*
- c. The subdivision has been reviewed in accordance with the Loveland Comprehensive Master Plan, including the Parks and Recreation Functional Master Plan, and other pertinent plans approved and adopted by the City, to insure that the subdivision is designed in accordance with good engineering practices and provides for safe and convenient movement.*

2. **Section 16.24.012:** *Electric and water distribution system improvements, sewer collection improvements, storm drainage control facilities, and other improvements as required to be constructed with the subdivision have been designed in accordance with the City of Loveland "Storm Drainage Criteria Manual," 1986 Edition, as amended and the latest edition of the "Development Standards and Specifications Governing the Construction of Public Improvements."*
3. **Section 16.24.090:**
  - a. *All new and replacement sanitary sewer and water supply systems have been designed to minimize or eliminate infiltration of floodwaters in the system.*
  - b. *The subdivision proposal has adequate drainage provided to reduce exposure to flood damage.*
  - c. *The subdivision proposal has public utilities and facilities such as sewer, gas, electrical, and water systems located and constructed to minimize flood damage.*
  - d. *The development proposal conforms to all federal, state, and local floodplain regulations*
  - e. *When deemed necessary by the Director of Development Services or the Planning Commission for the health, safety, or welfare of the present or future population of the area or necessary to the conservation of water, drainage, and sanitary facilities, the subdivision of land within the flood fringe and floodway, or any stream, river, or drainage course has been prohibited.*
4. **Section 16.24.140:** *All proposed utility facilities, including, but not limited to, gas, electric power, telephone, and CATV cables, are located underground. Where practical, existing utility facilities located above ground, except when located in a public right-of-way, are to be removed and placed underground.*
5. **Chapter 16.41:** *A positive determination of adequacy, or a positive determination of adequacy with conditions, has been made in accordance with Section 16.41.100 for fire protection and emergency rescue services, Section 16.41.120 for water facilities and services, Section 16.41.130 for wastewater facilities and services, Section 16.41.140 for storm drainage facilities, and Section 16.41.150 for power.*
6. **Section 18.41.050:**
  - a. *Development permitted by the PDP will not have negative impacts on City utilities. If such impacts exist, Section 18.41.050.D.4(b) of the Loveland Municipal Code requires City staff to recommend either disapproval of the PDP or reasonable conditions designed to mitigate the negative impacts.*
  - b. *Whether development permitted by the PDP will be complementary to and in harmony with existing development and future development plans for the area in which the PDP is located by incorporating public facilities or infrastructure, or cash-in-lieu, that are reasonably related to the proposed development so that the proposed development will not negatively impact the levels of service of the City's services and facilities.*

**Fire:** Staff believes that this finding can be met, due to the following:

- The development site will comply with the requirements in the ACF Ordinance for response distance requirements from the first due Engine Company.
- The proposed development of townhome clusters will not negatively impact fire protection for the subject development or surrounding properties.

**Water/Wastewater:** The subject annexation is situated within the City's current service area for both water and wastewater. Regarding water there are two adjacent 12" water stubs one in Rossum Drive and one in Green Valley Drive that will be extended and looped through the development.

Regarding wastewater there is an existing wastewater stub in Green Valley Drive that can be extended into the site to serve as many homes as possible. There is currently no existing water or wastewater service to the development area. The Department finds that the Development will be compliant to ACF for the following reasons:

- The proposed development will not negatively impact City water and wastewater facilities.
- The proposed development is in harmony with existing and future development and incorporates public infrastructure designed so that the proposed development will not negatively impact the levels of service of the City utilities adjacent to the development.
- The proposed facilities shown on the Preliminary PICPs have been designed pursuant to the City's Development Standards.
- The proposed facilities have been design to minimize flood damage and infiltration.

**PW-Stormwater:** Staff believes that this finding can be met, due to the following:

- When final designed and built, the development will not negatively impact City storm drainage utilities and will comply with the Adequate Community Services ordinance outlined in the Loveland Municipal Code, Section 16.41.140.
- No irrigation ditches traverse the site.
- No natural drainage courses/open channels traverse the site.

**Power:** This development is situated within the City's growth management area but not in the current service area for power. Upon successful annexation, City of Loveland will serve power to the proposed development. The Department finds that the Development will be compliant to ACF for the following reasons:

- The PDP is consistent with the Department's power master plan by being consistent with the 2016 Comprehensive Master Plan.
- The proposed development will not negatively impact City power facilities.
- The proposed development is in harmony with existing and future development and incorporates public infrastructure designed so that the proposed development will not negatively impact the levels of service of the City utilities adjacent to the development.

## B. Transportation

1. **Section 16.20.030:** *The subdivision has been reviewed in accordance with the Loveland Transportation Master Plan to insure that the subdivision is designed in accordance with good engineering practices and provides for safe and convenient movement.*
2. **Section 16.24.015:** *Streets, street signs, highways, curb and gutter, traffic control devices, and other improvements as required to be constructed with the subdivision have been designed in accordance with the Larimer County Urban Area Street Standards, as amended.*
3. **Section 16.24.040:** *Streets have been designed to have a logical relationship to topography and to the location of existing or platted streets in adjacent properties. Certain proposed streets, as determined by the City engineer, have been extended to the boundary of the subdivision to provide for traffic circulation within the vicinity*
4. **Section 16.41.110:** *A positive determination of adequacy, or a positive determination of adequacy with conditions, has been made for transportation facilities in accordance with Chapter 16.41 of the Loveland Municipal Code.*
5. **Section 18.41.050:**
  - a. *Development permitted by the PDP will not have negative impacts on traffic in the area. If such impacts exist, Section 18.41.050.D.4(b) of the Loveland Municipal Code requires City*

*staff to recommend either disapproval of the PDP or reasonable conditions designed to mitigate the negative impacts.*

**b.** *Whether development permitted by the PDP will be complementary to and in harmony with existing development and future development plans for the area in which the PDP is located by incorporating public facilities or infrastructure, or cash-in-lieu, that are reasonably related to the proposed development so that the proposed development will not negatively impact the levels of service of the City's services and facilities.*

**PW-Transportation:** Staff believes that this finding can be met, due to the following:

- A Traffic Impact Study (TIS) has been submitted with the Parkside First Subdivision Preliminary Development Plan (PDP) and Preliminary Plat applications which demonstrates that the transportation system, incorporating the required roadway improvements, can adequately serve the land uses proposed.
- Access to the site will be from W 1st Street aligning with Golden Valley Drive to the North.
- The applicant's traffic engineer, Matt Delich, P.E., has submitted a Traffic Impact Study (TIS) that indicates that the traffic associated with the proposed development will meet the City's standards. The proposed Parkside First Subdivision is estimated to generate approximately 816 daily trips, 63 weekday AM peak hour trips, and 82 weekday PM peak hour trips.
- Off-site intersection improvements are proposed at W 1st Street and Crestone which are required to meet ACF standards.  
In conclusion, the development of the subject property pursuant to any of the uses permitted by right under the zoning district will not adversely impact any existing City infrastructure when typical roadway improvements are constructed. A positive determination of adequacy for transportation facilities for the proposed PDP has been made under the provisions referenced above.

## C. Land Use

### 1. Section 18.41.050.E.2:

- a. The preliminary development plan conforms to the general development plan on file with the city where the property is being developed in phases.*
- b. The PDP conforms to the intent and objectives of Title 18 with regard to Planned Unit Developments and any applicable area plan.*
- c. The PDP complies with applicable land use and development regulations in effect as of the date that the GDP was approved and any land use and development regulations adopted by the City after that date if the Planning Division and Planning Commission expressly find that compliance with such regulations is necessary to protect public health, safety, and welfare.*
- d. Development permitted by the PDP will not have detrimental impacts on property that is in sufficient proximity to the PDP to be affected by it.*

**Current Planning:** Staff believes that these findings can be met, due to the following:

- The proposed development would not be subject to a GDP given the proposed zoning structure. The PDP for Parkside Village was prepared in-lieu of a GDP as a non-phased PUD.
- Key objectives that this PDP achieves as specified in Section 18.41.020 include a clustered development that conserves open space, making for more efficient use of land to support the cost effective delivery of public services and facilities. Encouraging innovative

residential development for a growing population. And preserve the site natural characteristics.

- Detached single-family and paired two-family dwellings are consistent with the type and density of surrounding urban development as seen in the Mariana Butte Subdivision. Site improvements such as the inclusion of detached sidewalks along all public streets provide a higher focus towards walkability for pedestrians. All common open space areas with the development will be landscaped according to City standards. And all homes will be designed to integrate into the overall character of surrounding development in terms of materials, scale, and height.

## **D. Subdivision and Design Standards**

### **1. Section 16.20.030:**

- a. The subdivision does not create, or mitigates to the extent possible, negative impacts on the surrounding property.*
- b. The lots and tracts are laid out to allow efficient use of the property to be platted.*
- c. The subdivision provides desirable settings for buildings, protects views, and affords privacy, protect from noise and traffic, and uses resources such as energy and water in keeping with responsible resource stewardship.*
- d. The subdivision has been reviewed in accordance with the pertinent portions of the adopted Comprehensive Master Plan.*

### **2. Section 16.24.050: All lots comply with the standards set forth in the GDP and, to the extent practical, lot lines are at right angles to the street line or at right angles to the tangent of the curve of the street line.**

### **3. Section 16.24.120:**

- a. Landscaping complies with the requirements set forth in the GDP and bufferyards required pursuant to the GDP are within separate tracts of land, separate from individual residential lots.*

- b. Street trees are located in compliance with the City's Site Development Performance Standards and guidelines, unless waived by the Director.*

### **4. Section 18.41.050.E.2: Development permitted by the PDP will be complementary to and in harmony with existing development and future development plans for the area in which the PDP is located by:**

- a. Incorporating natural physical features into the PDP design and providing sufficient open spaces considering the type and intensity of proposed land uses.*
- b. The PDP incorporates environmentally sensitive areas, including but not limited to wetlands and wildlife corridors, into the project design.*
- c. Incorporating site planning techniques that will foster the implementation of the Create Loveland Comprehensive Plan.*
- d. Incorporating physical design features that will provide a transition between the project and adjacent land uses through the provisions of an attractive entryway, edges along public streets, architectural design, and appropriate height and bulk restrictions on structures.*
- e. Incorporating an overall plan for the design of the streetscape within the project, including landscaping, auto parking, bicycle and pedestrian circulation, architecture, placement of buildings and street furniture.*

**Current Planning:** Staff believes that these findings can be met, due to the following:

- The streetscape design is a key design element of the overall project that includes detached sidewalks along all public streets associated with this development. Tree lawns will be

created between the sidewalks and curb/gutter as a result which goes above normal requirements for street trees.

- Approximately 17 acres of the 41 total acres is anticipated to be acquired by the City Open Lands Division for preservation/protection of valued natural area and wildlife habitat. In addition, greater buffering from the backs of lots to the shoreline of Boedecker Reservoir is being created through the establishment of an outlot to further preservation/protection.
- The natural open space areas particularly to the south and west will help create a more rural transition to existing county residences as well as the Boedecker Reservoir. The treatment along W. First Street will mimic landscape improvements directly north as seen in Mariana Butte. Access into the site will align with Green Valley Drive. The PDP also establishes architectural standards that are unique to this development but provide similar relationships to height and bulk with existing surrounding homes.

### **III. RECOMMENDED CONDITIONS**

Conditions are being recommended by the Development Review Team (DRT) for consideration of the Annexation, Zoning (Preliminary Development Plan) and Preliminary Plat. If recommended and/or modified by the Planning Commission, the conditions for annexation will be included in an annexation agreement presented to City Council for consideration. If recommended and/or modified by the Planning Commission, the conditions listed under the Preliminary Development Plan and Preliminary Plat will be included on the associated plan sets and eventually captured in a development agreement with any subsequent Final Development Plan and Final Plat approvals.

#### Annexation

##### Current Planning

1. Annexation of the Parkside Village Addition shall not occur without approval of the Parkside Village PUD Preliminary Development Plan.
2. Annexation of the Parkside Village Addition will create an enclave of unincorporated property currently owned by McCrimmon (parcel #952-1-00-005). As a result, the City may require annexation of this property in the future which may have an impact on development or existing development associated with the Parkside Village Addition.

#### PW-Transportation:

1. All public improvements with any future development shall comply with the Larimer County Urban Area Street Standards (LCUASS).
2. The developer/owner agrees to acquire and dedicate, at no cost to the City, any rights-of-way necessary for the required street improvements associated with any future development of the land.
3. Prior to the issuance of any building permits for future development, pursuant to the provisions in Section 16.40.010.B of the Loveland Municipal Code, the Developer shall design and construct the following public improvements unless already designed and constructed by others:
  - a) The ultimate adjacent street improvements on west First Street to current 2-lane arterial standards including pavement widening, bike lanes, curb & gutter, a landscaped parkway and sidewalk.

b) All interior public streets to serve the development as shown on the City approved Public Improvement Construction Plans.

4. Any other off-site improvements required will be determined by the findings of a Traffic Impact Study (TIS) at the time a development application is submitted for review.

#### Water/Wastewater

1. At the time of Final Plat the developer shall provide a final Water and Wastewater System Impact Analysis.

2. The Developer shall extend the wastewater main through the development to provide future service to the west, 4420 W 1st Street, and to the east.

3. The Developer shall provide a 12" water main looped through the site with two points of connection to the existing 12" water mains. The Developer shall extend the water main to provide future water service to the west and to the east.

#### Preliminary Development Plan

##### Current Planning

1. All lots abutting Outlot D (as currently depicted in the Preliminary Plat) shall be prohibited from having direct access to Outlot D and the natural area (currently depicted as Tract A in the Preliminary Plat). All lots abutting Outlot D shall be required to install split rail fencing with no gates or other openings prior to issuance of a certificate of occupancy. In addition, the Home Owners Association (HOA) shall include this condition within their respective covenants.

2. Homes shall be limited to a maximum building height of 35 feet as measured in Section 18.04.040 of the Loveland Municipal Code.

3. Developer shall be responsible for installation of all common area (outlot) landscaping/irrigation including all tree lawns along public streets. Sequencing for these landscape improvements will be established with the Final Development Plan.

4. The Home Owners Association (HOA) shall own and maintain all outlots within the Preliminary Development Plan.

5. A restoration and maintenance plan shall be provided with a Final Development Plan identifying proper restoration of all outlots disturbed during construction along with a maintenance plan for the Home Owners Association (HOA) to follow.

6. The Final Development Plan shall provide building elevations for all home designs.

#### PW-Transportation

1. All public improvements shall comply with the Larimer County Urban Area Street Standards (LCUASS).

2. The developer agrees to acquire and dedicate, at no cost to the City, any rights-of-way necessary for the required street improvements associated with this development.

3. Prior to the issuance of any building permits within the Parkside Subdivision, pursuant to the provisions in Section 16.40.010.B of the Loveland Municipal Code, the Developer shall design and construct the following public improvements unless already designed and constructed by others:

- a) All ultimate street improvements on W 1st Street to the 2-lane arterial standard including left turn lanes, pavement widening, bike lane, curb & gutter and detached sidewalk as shown on the City approved Public Improvement Construction Plans titled Parkside Subdivision prepared by Northern Engineering.
- b) All public street improvements (with construction phasing) within the Parkside subdivision as shown on the City approved Public Improvement Construction Plans titled Parkside Subdivision prepared by Northern Engineering.
- b) Intersection and pedestrian crossing improvements at W 1st Street and Crestone Ave./CR21 as shown on the City approved Public Improvement Construction Plans titled Parkside Subdivision prepared by Northern Engineering.
- c) The off-site pedestrian path on the South side of W 1st Street connecting Parkside Village to the intersection of Crestone Ave/CR21 as shown on the City approved Public Improvement Construction Plans titled Parkside Subdivision prepared by Northern Engineering.

4. City signed Site Development Plans (including any associated Public Improvement Construction Plans), or the issuance of building permits, does not allow any construction within public street or alley rights-of-way or pedestrian easements. A separate City Development Construction Permit or Street right-of-way (ROW) Work Permit must be obtained by the Developer and/or his Contractor at the City Project Engineering office (and approved by Project Engineering) prior to any repair or construction of sidewalk, curb and gutter, driveway accesses, or any other construction in City street or alley rights-of-way or pedestrian easements, (this includes all items proposed in rights-of-way such as utility street cuts, sidewalk ramps, construction staging proposed in street, landscaping, traffic control, etc.). (Call 970-962-2510 to discuss details to obtain a ROW Work Permit).

5. Prior to the commencement of any construction activity that will involve any existing or proposed street signs or traffic control devices for or within public street rights-of-way (ROW), the Developer and/or his Contractor shall contact the City Traffic Division at (970) 962-2535 to coordinate the removal, relocation, installation, and/or proper storing of the sign(s) or traffic control device(s) and obtain a ROW work permit from the City Public Works Engineering Division to do such work. However, if the Developer and/or his Contractor removes or relocates any existing street sign(s) or traffic control device(s) for or within the public ROW without first obtaining a ROW work permit from the City Public Works Division, then the contractor will be charged for the labor, materials, and equipment to reinstall the sign(s) or traffic control device(s) as deemed necessary by the City. The Developer and/or his Contractor will also be charged to replace any existing street signs or traffic control devices that were damaged or blemished during any construction activity as deemed necessary by the City. The Developer and/or his Contractor may also be subject to additional fines as per the Loveland Municipal Code.

6. All trees, shrubs, and other plant materials located within clear sight triangles shall be trimmed in accordance with the requirements of Section 7 of the Larimer County Urban Area Street Standards (LCUASS). Under current LCUASS requirements, trees shall be limbed to a height of not less than eight (8) feet and shrubs and other plant materials shall be maintained at a height of not more than thirty (30)

inches, and said maintenance shall be conducted in perpetuity. Trees are also required to be kept limbed up a minimum of 8' above all street sidewalks.

#### Parks and Recreation

1. Tree removal and construction activities shall comply with the federal Migratory Bird Treaty Act. No activities shall occur near an occupied nest and this area shall be monitored between February 15th and July 15th for potential raptor nesting. Buffer zones and seasonal restrictions shall be followed per the Colorado Parks & Wildlife guidelines.
2. Noxious weeds shall be managed on the property to maintain the ecological integrity of areas not disturbed by construction. Management of noxious weeds within portions of the property not impacted by construction should enhance ecological values of residual lands. Disturbed areas shall be revegetated with a native seed mix as soon as possible and best management practices employed throughout construction to limit soil erosion and sedimentation.
3. Any wetland, drainage, or ditch crossings shall encourage wildlife movement beneath the crossing structure(s). If bridge abutments are proposed, they shall be located outside any wetland / drainage areas. If culverts are proposed, they shall be partially buried or have an open bottom (such as an arched water crossing) to encourage wildlife movement beneath the structure.
4. A 30-foot wide trail easement from the southeast corner to the northwest corner of the property shall be required at preliminary plat. The trail easement shall be dedicated for public access.
5. A 6 foot wide soft trail shall be constructed with the first phase of development within the new 30 foot trail easement. The City of Loveland and the developer shall agree to locate the trail and the developer shall maintain the trail and easement until such time as the City of Loveland agrees to the maintenance of the easement and/or trail.
6. Construct a 6-foot wide pedestrian connection from sidewalk at the Parkside Village development to the soft surface trail within the 30' easement. Maintain this connection up to the 30' easement. Construction shall occur during the first phase of development.

#### Preliminary Plat

##### Current Planning

1. Tract A shall be changed to an Outlot (Outlot E) prior to submittal of a Final Plat.

## LAND USE CONSISTENCY

This application for Annexation and PUD Zoning recognizes the City of Loveland's vision of **a vibrant community surrounded by natural beauty, where you belong.** The unique location of the subject site on the northeastern shore of Boedecker Reservoir provides the opportunity to achieve stated City goals **to protect sensitive natural areas and wildlife habitats from development impacts and to maintain and expand parks and recreation facilities as a valuable asset to the community.**

The City of Loveland Parks Department desires lake access for the entire community as well as a walking path along the shoreline of Boedecker Reservoir. The Proposed Parks Parcel in this request is 17.735 acres of the 41.229 net acres requested for Annexation. This is in concert with the City of Loveland's desire **to create more reservoir parks and buffers to serve the entire community, allowing lake access via public property.** The logical path to satisfy this goal is **to coordinate land development and land conservation efforts between city departments, Larimer County, non-profit partners and landowners.** In the case of this request, that effort has been ongoing with the Loveland Parks Department in recognition of the unique nature of the subject property.

The proposed PUD zoning with the associated Preliminary Development Plan is consistent with the site's future land use designation of Low Density Residential. This zoning classification provides for low density single family homes with limited duplex and multi-family homes. **This zoning district can consist of a variety of housing types with an average density of 2-4 units per acre and block lengths of 250-650 feet.**

After the acquisition of the Park Parcel by the City of Loveland, the remainder 23.494 acre site is proposed for development of 69 lots and 76 residences. This is a density of 1.8 units per gross acre and 3.2 units per net acre, consistent with Low Density Residential standards. The PUD contains 7 duplex lots and 62 detached single family homes. The detached single family homes are further delineated with 28 premium view lots with panoramic views of Boedecker Lake and the Front Range, 26 lots surrounding a community open space and 8 perimeter lots abutting an undeveloped parcel to the east. The several types of lots will support a range of product types and pricing:

- Luxury custom home sites with a compact footprint that takes advantage of superior views of the Front Range and Boedecker Reservoir. These lots are designed for a low maintenance lifestyle typical of empty nesters, with upscale construction but minimal yard area to allow for a “lock and leave” lifestyle. Emphasis is on native landscaping adjoining a buffer that provides protection for adjacent riparian resources.
- A smaller lot product that is more affordable and semi-custom in nature. These lots do not have compelling views, but offer solid value and affordability compared to the high end product on the western perimeter overlooking Boedecker Reservoir.
- A duplex product that offers the greatest level of affordability within the community and affords a reasonable market entry point for this unique infill community.

Equally important, the proposed community mirrors the City goal ***to encourage a pattern of compact and contiguous development.***

The site offers ready access to existing roads and utilities in place on the perimeter and annexation would require only minimal additional utility and road infrastructure. There are water and sanitary sewer mains, electricity and gas service, telecommunication facilities and storm drainage already in place, as well as immediate access to West 1<sup>st</sup> Street, which is also known as Larimer County Road Number 20.

The proposed zoning of PUD with Preliminary Development Plan is compatible with the adjacent Marianna Butte PUD, which is a mixture of duplexes, small lot and larger lot product. Immediately northwest of the site are luxury duplexes with unique lakeside views. Due north are low density single family homes offering a traditional single family lifestyle. East of the subject property are two tracts of land that are unzoned at present and lie in unincorporated Larimer County. The south and southwest border of the subject property adjoin Boedecker Reservoir.

The proposed annexation and PUD zoning offers a unique opportunity to achieve one of the major policy goals of the City of Loveland, that being the following:

***“inside the Loveland GMA, lead in protecting open lands using a variety of protection techniques in partnership with willing landowners.”***

## Troy Bliss

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**From:** Troy Bliss  
**Sent:** Monday, June 19, 2017 3:46 PM  
**To:** 'Gregory Dellinger'  
**Subject:** RE: parkside village

Greg, I apologize for getting back to you so late. I missed your email from last week. As it relates to the Parkside Village project, there would be an interim roundabout at the intersection of W. First Street and Crestone Drive (which aligns with County Road 21 to the south of W. First Street). The roundabout would be designed to fit within existing right-of-way and be interim until such time as other unincorporated properties annex/develop or the City completes the ultimate improvements. Either way, the ultimate improvements are many years out.

I can certainly appreciate your perspective in questioning why more and more building continues. This area is part of the City's Growth Management Area (GMA) and intended to be urbanized at some point. With respect to the Parkside Village project, the idea of clustering development to protect/preserve 17+ acres around Bodecker Reservoir was seen as a positive in maintaining a high quality of life. It was also important that the proposed development incorporate good design standards. For example, all the streets within the development include detached sidewalks with tree lawns between sidewalk and curb/gutter and homes would have to adhere to a higher level of architectural design.

Hope this information is helpful. Again, sorry for getting back to you so late. Thank you.

### Troy Bliss

Senior Planner  
Current Planning  
Development Services  
410 East Fifth Street  
City of Loveland, Colorado 80537  
(970) 962-2579  
[Troy.Bliss@cityofloveland.org](mailto:Troy.Bliss@cityofloveland.org)



**From:** Gregory Dellinger [mailto:[gdell07@gmail.com](mailto:gdell07@gmail.com)]  
**Sent:** Wednesday, June 14, 2017 8:11 AM  
**To:** Troy Bliss <[Troy.Bliss@cityofloveland.org](mailto:Troy.Bliss@cityofloveland.org)>  
**Subject:** parkside village

What are your plans for the intersection of cr21 and 1st street? It is already very dangerous and hard to get out, plus many new homes are being built across the street already. Why must we keep building and building and destroy the quality of life in this area????? greg

## Troy Bliss

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**From:** Robert Paulsen  
**Sent:** Friday, June 16, 2017 9:17 AM  
**To:** 'Holly Trumble'  
**Cc:** Troy Bliss; Jenell Cheever  
**Subject:** RE: Sub division W 1st / Loveland Planning Commission hearing on June 26th

Holly,

Sorry for the error in my previous email. **The public hearing is on Monday, June 26<sup>th</sup> beginning at 6:30 pm in the City Council Chambers**--which are located in the Loveland Civic Center at the corner of 3<sup>rd</sup> Street and Washington Avenue. The Planning Commission encourages public testimony at the hearing, but understand that your oral comments may be limited to approximately 3-5 minutes particularly if there are a lot of citizens interested in speaking on this matter.

***Bob Paulsen, AICP***

Current Planning Manager

City of Loveland

970-962-2670

[Robert.Paulsen@cityofloveland.org](mailto:Robert.Paulsen@cityofloveland.org)



**PLEASE NOTE THAT CURRENT PLANNING HAS MOVED TO THE NEW DEVELOPMENT CENTER AT 410 EAST 5<sup>TH</sup> STREET IN DOWNTOWN LOVELAND.**

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**From:** Holly Trumble [mailto:[hotstampinholly@gmail.com](mailto:hotstampinholly@gmail.com)]

**Sent:** Thursday, June 15, 2017 8:45 PM

**To:** Robert Paulsen <[Robert.Paulsen@cityofloveland.org](mailto:Robert.Paulsen@cityofloveland.org)>

**Subject:** Re: Sub division W 1st

Thank you Bob, the meeting is Mon the 26th correct, not Sat the 24th?  
Holly

Sent from my iPhone

On Jun 15, 2017, at 5:39 PM, Robert Paulsen <[Robert.Paulsen@cityofloveland.org](mailto:Robert.Paulsen@cityofloveland.org)> wrote:

Mark and Holly Trumble,

I am in receipt of your email. Thank for taking time to express your concerns. The email (letter) will be distributed to the Planning Commission this coming week and considered as part of the official record at the Planning Commission public hearing on June 24<sup>th</sup>.

Should you have questions about the Planning Commission hearing or further comments, please feel free to contact me.

**Bob Paulsen, AICP**

Current Planning Manager

City of Loveland

970-962-2670

[Robert.Paulsen@cityofloveland.org](mailto:Robert.Paulsen@cityofloveland.org)

<image001.jpg>

**PLEASE NOTE THAT CURRENT PLANNING HAS MOVED TO THE NEW  
DEVELOPMENT CENTER AT 410 EAST 5<sup>TH</sup> STREET IN DOWNTOWN LOVELAND.**

**From:** Holly Trumble [<mailto:hotstampinholly@gmail.com>]

**Sent:** Thursday, June 15, 2017 5:25 PM

**To:** Robert Paulsen <[Robert.Paulsen@cityofloveland.org](mailto:Robert.Paulsen@cityofloveland.org)>

**Subject:** Sub division W 1st

Mark & Holly Trumble

708 Cove Ct  
Loveland CO 80537  
970-290-1368

Bob Paulsen, Liaison  
500 E 3rd St  
Loveland CO 80538

Proposed Housing Development, W 1st St just north of CR 21

Dear Mr Paulsen;

We live in Mariana Cove on the west side of the Boedecker Reservoir State Wildlife Area. Our home sits on the lake and looks directly at the bluff under consideration for development. We are very concerned that the City would even entertain the amount of dwellings on this piece of property, with this being a State Wildlife area! We have numerous Cooper hawks and Great horned owls, three pair of breeding Osprey eagles with young, a pair of breeding Bald Eagles with two young and the island of Great Blue Herons that sits in the south end of the reservoir, with approximately 20 nests all with young. This is the only shoreline that is without homes so the coyotes and many raptors have unlimited food resources on this property due to the prairie dogs, if this housing development is brought to fruition all of these animals will lose this important food source. After all Boedecker is a State Wildlife Area, not just another water source, as stated on the website.

The homes that surround the lake are all upscale homes on large lots, these proposed lots are so small it will look like a trailer trash. Seriously this will bring harm to our beautiful wildlife area that we love so much, nothing to say of bringing our home values down significantly. We look directly at this coast line, I can't even imagine seeing it dotted with homes. The waterfowl hunters would also lose this area due to these homes being too close to the water's edge.

The Herons are such a site to see each spring, if we don't protect these animals who will? All through the year we watch as the Bald Eagles and Osprey raise their young right here at Boedecker State Wildlife park!! The Elk and Deer wander these shorelines and play in the water

with their young, isn't it wonderful to have our kids be able to watch in a natural environment instead of having to go to the zoo to see them?

I would ask you to come and visit Boedecker either by boat, kayak or walk through our neighborhood to experience what we see and feel every day.

We would like to see this area be put into a Conservation Trust so that it will always remain a Wildlife area. There is so little beautiful land left in Loveland, we ask the Planning Commission to vote against this proposed development, or at least consider a lesser amount of rooftops. Each one of us are responsible for what we make of our town, please allow this area to remain beautiful and unspoiled by ugly cracker box houses.

Sincerely,

Mark & Holly Trumble

( I have sent this letter to all members of the Planning Commission at the above street address)

--

**Holly Trumble**

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*GCH CH AKC/UKC Pili Lani's Kilohani Kei of Aslan CGCA RE JWD THDX*

*CH Pili Lani's HanoHano CGC*

[www.pililaniportuguesewaterdogs.blogspot.com](http://www.pililaniportuguesewaterdogs.blogspot.com)

## Troy Bliss

---

**From:** 2bugnme@gmail.com  
**Sent:** Friday, June 16, 2017 11:45 AM  
**To:** Ken Mitchell  
**Cc:** kristin@tbgroup.us; Troy Bliss  
**Subject:** Re: Parkside Village

Ken, thank you for the response. I had been wondering if my email fell on deaf ears since I had not heard anything back since February.

Do you know if they are planning to do the same with the Namaqua Road/1st street intersection? If not I fear that 4-way stop will ultimately become a bottle neck going east out of the newly planned round about.

Again thank you for your response!

Regards,  
Deidra Fahrenbruch

Sent from my iPhone

On Jun 9, 2017, at 1:14 PM, Ken Mitchell <[kmitch2000@gmail.com](mailto:kmitch2000@gmail.com)> wrote:

Deidra,

You wrote an email to Troy Bliss on February 7, 2017 suggesting the use of a roundabout as a possible mitigation measure to traffic issues in the Marianna Butte area.

The City of Loveland listened. Attached is a roundabout design for the intersection of 1<sup>st</sup> Street and CR 21. We have agreed to build a temporary roundabout until such time as the City of Loveland can build a more permanent feature. Our traffic engineer and the City of Loveland have agreed that this will greatly mitigate neighborhood traffic issues.

I'd like your support of our proposal. Along with the roundabout, we're working with the City of Loveland to help them acquire 17+ acres of this Parkside Village land for a park and soft surface hiking trail. This would give Marianna Butte residents almost a mile of lakefront trail, preserve wildlife habitat along the shoreline and offer access to the lake for fishing and no-wake watercraft.

Letters of support should be addressed to Troy Bliss, who is copied on this email.

Kenneth R. Mitchell  
Mosaic Real Estate LLC  
1021 Nightingale Drive

Fort Collins, CO. 80525  
970-685-1575  
[kmitch2000@gmail.com](mailto:kmitch2000@gmail.com)

<Roundabout Design.pdf>

## Petition Against proposed North Boedecker Lake Development

We, the undersigned, oppose the proposed development at Boedecker Hill (Parcel# 9520100001) on the north side of Boedecker Lake.

We petition the Planning Commission and City Council to deny the application for development.

The proposed “Parkside Development” negatively impacts neighborhood resident’s quality of life, severely impacts wildlife at Boedecker Lake, poses traffic safety concerns, and goes against the requests of the surrounding community.

## Troy Bliss

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**From:** Roger and Sherry Svendsen <rsvend@comcast.net>  
**Sent:** Thursday, March 23, 2017 10:03 AM  
**To:** Troy Bliss  
**Subject:** Parkside Village

Hello Troy,

I believe you are the fellow that hosted a community meeting on the subject proposed development on the north side of Boedecker Lake. Thanks for that .... it was very informative.

Concerning that activity, as well as the new development on the north side of 1st directly north of Parkside, I am concerned about speed limits on 1st. I don't know whether you are familiar with traffic on 1st in this area, but we live in The Overlook just west of this recent activity. Folks typically travel at speeds above the 45 MPH posted for 1st. In the summer especially, motorcycles really get going on the stretch between Boedecker and Buckingham lakes. I would estimate that they get to 70 MPH along that stretch sometimes. With more traffic entering 1st Street at the top of the rise where these two developments are taking place, I fear there is danger of some serious accidents. When one enters 1st traveling north on 23rd, it is pretty blind looking west.

So, I suggest changing the speed limit from 45 to 35 from the Cove to Wilson. Of course, enforcement will be necessary, but I think this might be the only prevention possible short of a new stop sign. Would this fall under your overall task on these projects? What do you think?

Thanks in advance for your consideration.

Roger Svendsen

252 Two Moons Dr.

970-593-3012

## Troy Bliss

---

**From:** Sharon McCrimmon <mccrimmongrannymac@gmail.com>  
**Sent:** Sunday, March 5, 2017 3:51 PM  
**To:** Troy Bliss  
**Subject:** Parkside Subdivision

Dear Troy,

I am sending this in regards to my concerns on this subdivision that I understand has not been approved as yet. I live next door to it on the West side and notice that they have started to dig and have the Prairie Dogs confessed and roaming on to the land to the East of the property. I understood at the meeting that time would be given to catch and release these animals. Apparently this new developer from Florida does not respect the laws of Colorado, or keep his word.

I will voice how I think that 75 plus housetops on this parcel of land is too many for this area. It also does not conform with the rest of the homes. To my knowledge a traffic count has not as of this time has not been done on first street going East or West from Rossum drive. living here I can share with you that at peak times it is quite high. With one way into this subdivision and the same way out. This could be an accident waiting to happen. @ 2 cars or more per family this is over 150 vehicles per day.

Ron and I bought this house knowing that this land was for sale. We expected to see a developer put in 1 Mil. \$\$\$ plus homes the same as on Rossum Dr and across the Street from us. I am so disappointed to have seen the map of Parkside, which reminds me of a RV park spaces.

I certainly hope all of the council and yourself have walked on this parcel and looked at the views of the lake and the mountains. This is so much prettier than being in town and living on the East side of lake Loveland. It is a 1 mil. \$\$\$ view and it has wild birds to enjoy such as Eagles Osprey, hawks, Great Blue Herons, and so many more, plus Deer, Elk, Coyotes, and more wild life.

City folk and the Rural folk live in the City or Rural areas because that is what they are seeking. I live in Rural and never expected the City to engulf me like the bind weed that we get in our gardens almost right down to my back yard. I hope you rethink this and tell him he has to do one acre or 1/2 acre lots per home and make it to conform to what is already in this area. since he is from Florida where the homes he previous had built, had a 10 foot or 5 foot space between lots I am sure he does not know the difference or he is a truly greedy man.

I recommend that the city purchase this whole portion and use it as a future park. Ron and I are Senior citizens and would be willing to sell our home for fair market value and the city could use this land and have a nice area for visitors to park and a nice place for offices. It is 2000 sq ft. up and 2000 sq ft down. They could purchase it when they could afford it. Do I really want to sell? NO, but in order to make it a nicer place for the future of our city, when you include it as a part of Loveland We would do this.

I have spent hours composing this and have had many interruptions while doing so. I will be at the meeting on Tuesday and will join in on the conversation if that is O.K.

thank you for taking the time to read this and .....

Sincerely, Sharon McCrimmon

4420 W. 1st Street  
Loveland, co. 80537  
. 970-669-4621

## Troy Bliss

---

**From:** Holly Trumble <hotstampinholly@gmail.com>  
**Sent:** Friday, March 3, 2017 3:08 PM  
**To:** Troy Bliss  
**Subject:** Parkside development

Hi Troy

I live on Boedecker Reservoir, I am seeing activity on the Boedecker Hill and wondered what is happening. I have been watching City Council meetings that might approach the subject again and have seen nothing on this project.

I am very concerned with my property value since I live directly on the lake and look straight at that hill.

I wrote to you at the beginning of this project begging to reconsider this huge housing development or at the least reduce the number of homes to protect some of the habit of wildlife.

I would love to know if there is a possibility of still having a say in this matter as many of my neighbors are very concerned. It looks to me like the city has made their decision without any more input from the folks that live right there.

If you could get back to me as soon as possible that would be great.

Thank you for your time

Holly Trumble

--

**Holly Trumble**

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*CH Pili Lani's HanoHano CGC*

[www.pililaniportuguesewaterdogs.blogspot.com](http://www.pililaniportuguesewaterdogs.blogspot.com)

## Troy Bliss

---

**From:** Deidra Fahrenbruch <2bugnme@gmail.com>  
**Sent:** Tuesday, February 7, 2017 10:19 AM  
**To:** Troy Bliss  
**Subject:** Mosaic Real Estate LLC proposed Subdivision - Parkside Village

Hello Troy, I attended the meeting about this subdivision back in December and I just wanted to comment on the traffic issues that were brought up. There was a comment made about the speed limit. I do not believe that the speed limits need to be changed. That would not fix any of the issues it would only make them worse and even more difficult to maintain the efficient flow of traffic. I propose that the city add two roundabouts. One at the intersection of 1st Street and Namaqua Road where there is currently a 4-way stop. Then another roundabout at the intersection of 1st Street and County Road 21/Crestone Drive.

These roundabouts would automatically create better flow at both intersections without any effect on the speed limit. This approach worked in the exact same fashion when roundabouts were added at the intersections of Cnty Rd 11C and Cnty Rd 30 as well as Cnty Rd 30 and Timberline Rd.

I hope that you will take these roundabouts into considerations as a more efficient way to improve the flow of traffic vs stop signs or lights.

Thank you,  
Deidra Fahrenbruch  
970-250-2904

## Troy Bliss

---

**From:** Steve Goppert <sgoppert@me.com>  
**Sent:** Thursday, January 26, 2017 12:04 PM  
**To:** Troy Bliss  
**Subject:** Comments on Parkside Village  
**Attachments:** Parkside Village Comments - EA.docx; ATT00001.htm

Gary

I hope its not too late to comment. I used the EA as a framework for the comment attached. Please contact me if something is unclear.

I have general observation for the city planning. In 1960 my Dad was transferred from the Air Force Academy to El Segundo, CA. Back then there were two diaries with in 5 miles and open fields, orange groves in Orange County. I watch it all disappear. The place is crowed an over developed. In 1982 I was on a team to incorporate the Black Forest. The idea was get control of our zoning and avoid the SoCal over development there. The “old timers” could not conceive of the development that has taken place since. I cringe every time I come over Palmer Divide at the shopping malls and high density residences that covet the fields I once drove my on my way home.

When I moved to Loveland 20 years ago the drive from I25 to town was mostly through mostly fields on 34. Now we have McWhinney-ville and homes. I have found that city governments are very susceptible to the overtures of developers, who have the time “lobby” for projects, and tantalize with a few more dollars from sales taxes or whatever. Today I see tract homes spreading over empty fields on Wilson, massive development east of I25 etc. Just like SoCal and the Black Forest with attendant heavy traffic. I this continues Loveland, Fort Collins, Windsor, and Johnstown city boundaries will be like SoCal: a sign on the street with no discernible boundaries.

I urge the city, through your good offices, to preserve the quality of life at least to the West of town by not annexing high-density project like Parkside or any others. Thought out Loveland I see reflections of a desire to maintain life quality with the trail system, parks, etc. So the intent is there. Now is the time to for balance. Money, money, money should yield to quality of life.

Specific concerns about the Parkside project are in the attached comments.

Thanks for your time reading this. I you can, please pass my concerns along to the appropriate folks in the city.

January 16, 2017

Mr. Troy Bliss, Senior Planner  
Ms. Leah Johnson and Ms. Joan Schafer, Loveland City Councilwomen  
500 East Third Street Suite 300  
Loveland, CO 80537

I am extremely concerned about the development proposal known as Parkside Village on West First Street at Boedecker Lake. It is my understanding this development will consist of 70 mixed residential units/townhomes with one main entry/exit onto First Street.

As a homeowner living in Mariana Butte subdivision for the past 9 years (adjacent to First Street), this development will have a severe impact on the quality of life of existing residents such as myself, as well as negatively impact the City's infrastructure. My main concerns are:

1. This development will significantly increase traffic on First Street
  - a. 70 residences = an additional ~140 vehicles. How will this volume of traffic be managed??
  - b. Increased vehicles = increased noise, safety concerns for cyclists/pedestrians/wildlife, pollution, and wear and tear on roads
  - c. Having only one entry/exit to this development further intensifies the impact to traffic on First Street.
2. This development will strain the City's water usage
3. This development will disturb and potentially harm ample wildlife in the area as well as risk pollutants into Boedecker Lake (run-off of pesticides and other chemicals).
4. High density housing/low value townhomes = noise, street lights, higher crime, reduced property values

While I STRONGLY oppose the CURRENT proposal, I do not oppose the *possible* annexation of this property entirely. If it MUST happen, I would like to see less dense development (large estate lots that blend into the natural surroundings better and fit with the existing developments/values in the area). I would also like to see another entry/exit point to reduce the traffic bottleneck on First at Green Valley. Ideally, I would love to see this developed as Open Space/Natural Area to preserve the rural beauty around the Lake.

Loveland, like most Northern Colorado cities, is growing TOO FAST!! There are already developments on First Street & Crestone and in Marianna Butte on Rossum. Please help to keep the charm, quiet, and natural beauty in tact!! I chose Loveland for those reasons. I don't want to have to move if the tranquility is ruined!

Thank you for your time and consideration.

Regards,



Debbie Vischer  
4510 Pika Drive  
Loveland, CO 80537  
970-231-3226

December 29, 2016

Mr. Troy Bliss, Senior Planner  
Ms. Leah Johnson and Ms. Joan Schafer, Loveland City Councilwomen  
500 East Third Street Suite 330  
Loveland, CO 80537

Dear Mr. Bliss, Ms. Johnson and Ms. Schafer:

As a home owner living in the Mariana Butte subdivision, I object to the current proposed annexation of the property for the Parkside Village on West First Street at Boedecker Lake. It is my understanding this development will consist of 70 mixed residential units/townhomes.

My concern is this development will significantly increase traffic on First Street, impact the City's water usage and disturb the ample wildlife in the area. Having only one entry/exit to this development further intensifies the impact to traffic on First Street.

I do not oppose the possible annexation of this property entirely, but would like to see less dense development. I would also like to see at least one more entry/exit point to reduce traffic congestion.

Please take these concerns into consideration as this issue is brought before the Council.

Sincerely,



Marilyn Davis  
4530 Pika drive  
Loveland, CO 80537  
970-203-1205

## Troy Bliss

---

**From:** Rebekka Juszczak <rjuszczak@yahoo.com>  
**Sent:** Monday, December 26, 2016 8:55 PM  
**To:** Troy Bliss  
**Subject:** Parkside addition off First Street

Hi Troy,

I believe you spoke with my relator, Tammy Walker, recently regarding the proposed development on the South side of First street.

We live in Marianna Butte and our home backs up to First street with current views of the open prairie dog field and a very pleasant view of the mountain range to the Southwest. We have already addressed our concerns with the Mosaic developer but we wanted to reach out to you too.

I'm sure the response to this development has not brought in much positive feedback from area residents who are concerned about the impact on the area on many platforms including home values, the environmental impact, traffic implications and our large concern, which is losing the views that we have.

We live in Montmorency court and enjoy the open space that is becoming so hard to find out West. When looking at the current proposal it appears that 2 duplexes will be constructed right behind our home directly in the line of sight of the Southwest range. Specifically lot 1 and 2 of Block 1. Needless to say we are not thrilled at this aspect of the proposal and want to continue to express our concerns and dislike for this specific element of the proposal. A big part of our rationale for purchasing our house was the views and open space located behind the home so we would hate to lose that, especially since we have only been in this home for 9 months and were planning on being here for many years.

One thought I had was to see if there was any interest from local residents or groups in banding together to purchase the land under an LLC to ensure it remains a dedicated open space. I am aware that property taxes would need to be paid annually with this idea.

Please email me back or call me at 630-338-5332 regarding this proposal and our concerns.

Thank you,  
Rebekka

Art and Jan Grotenhuis  
4480 Pika Drive  
Loveland, CO 80537

December 15, 2016

Troy Bliss  
Senior Planner  
City of Loveland  
500 East Third St.  
Loveland, CO 80537

Dear Mr. Bliss,

We are writing to express our concerns relative to the proposed annexation of property for the Parkside Village addition on West First Street at Boedecker Lake.

We understand that this development will consist of seventy mixed residential units.

Our concerns are not with the development of this property but with the number of units being proposed with only one entry/exit to the development across from Green Valley Drive.

This will add a lot more traffic on First Street, increase water usage for the city, disturb the ample wildlife in the area along with the need to relocate the prairie dog village that currently exists there.

Again, we do not oppose the possible annexation but would like to see fewer units there and at least one more entry/exit to the development.

Please take our concerns under consideration as this issue is brought before you.

Sincerely,  
Art and Jan Grotenhuis

Mark & Holly Trumble  
708 Cove Ct  
Loveland CO 80537  
970-290-1368

Troy Bliss  
500 E 3rd St Ste 330  
Loveland CO 80537

Proposed Housing Development W 1st St just north of CR 21

Dear Mr Bliss;

We live in Mariana Cove at the southernmost end on the west side of the Boedecker Reservoir State Wildlife Area. Unfortunately we were unable to attend to the City meeting regarding this site but have been in contact with those that did attend. We are very concerned that the City would even entertain the amount of dwellings on this piece of property, with this being a State Wildlife area! We have numerous hawks and owls, two pair of breeding Osprey eagles, a pair of breeding Bald Eagles and the island of Great Blue Herons. This is the only shoreline that is without homes so the coyotes and many raptors have unlimited food resources on this property due to the prairie dogs, if this housing development is brought to fruition all of these animals will be driven somewhere else. After all Boedecker is a State Wildlife Area as stated on the website.

The homes that surround the lake are all upscale homes on large lots, these proposed lots are so small it will look like a trailer trash. Seriously this will bring harm to our beautiful wildlife area that we love so much, nothing to say of bringing our home values down significantly. We look directly at this coast line, I can't even imagine seeing it dotted with homes. The waterfowl hunters would also lose this area due to these homes being too close to the water's edge.

The Herons are such a site to see each spring, if we don't protect these animals who will? All through the year we watch as the Bald Eagles and Osprey raise their young right here at Boedecker State Wildlife park!! The Elk and Deer wander these shorelines and play in the water with their young, isn't it wonderful to have our kids be able to watch in a natural environment instead of having to go to the zoo to see them?

We would like to see this area be put into a Conservation Trust so that it will always remain a Wildlife area. There is so little beautiful land left in Loveland, we ask the Council to vote against this proposed development.

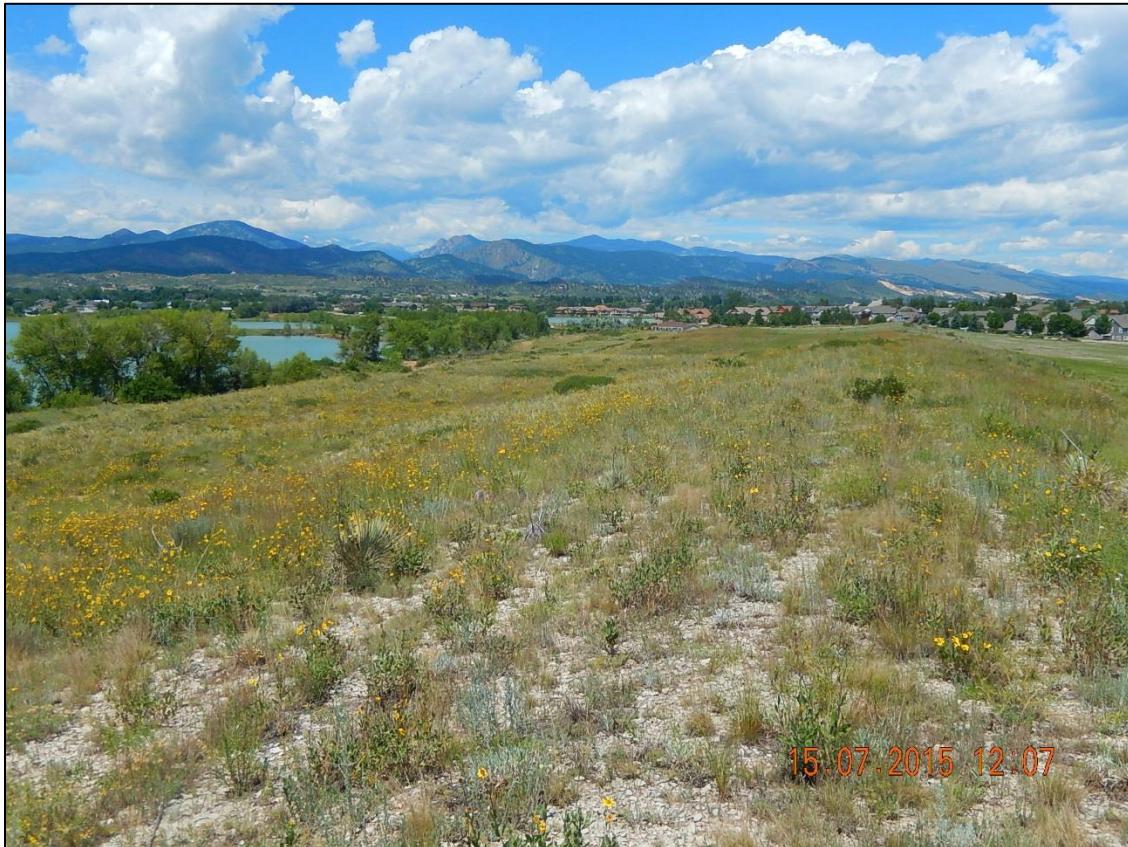
Sincerely,

  
Mark & Holly Trumble

PARKSIDE VILLAGE ADDITION - NEIGHBORHOOD MEETING  
 DECEMBER 8, 2016

<u>NAME</u>	<u>ADDRESS</u>	<u>PHONE OR EMAIL</u>
Doug & Rose Moore	4416 Red Fox Ct.	dmoore@williams <sup>pd</sup>
Roger & Sherry Svensen	252 Two Moons	Rsvennd@comcast.net
Tim Faircl	5255 Deer Meadow Ct.	jbf2ab2@comcast.net
DENNIS KINDE	5218 DEER MEADOW CT	dwkinde@yahoo.com
ROGER JOHNSON	693 DEER MEADOW DR	rjrogerjohnson@adcom
Faulkner, STE	403 Rossen Dr.	j-faulkner@gmail.com
Joe Schwind	4305 Red Fox #1	jxschwind@yahoo.com
Ashley Waddell	3732 Mead St.	ashley.trailrunner@gmail.com
Nancy Baros	629 Deer Meadow Dr.	Nancybarose@gmail.com
Anne Lander	4659 Foothills Dr.	
Ray Cachrane	3942 West St	
Deborah & Richard McFadden	5500 Mystic Owl Ct	deborah@8.2.com
Mel Anderson	152 Two Moons Drive	970 443 4546
Dave & Deida Fahrenbruch	382 N. Core Dr.	970-256-2904
John & Julie Vink	383 N Core Dr	970 449 8323
MARY WILKINSON	480 Jacqueline Dr	Mkwiki@msn.com
TIM BUNGER	4405 PIKA DR	TPBUNGER@YAHOO
Maralyn Radice	4539 Foothills DR	Mgradice@Aol.com
Rob & Diane Lipp	5434 Nishy Gullet	
Ken & Wendy Cooper	4475 Pika Drive	KWcooper5@hotmaile...
STAN GOPPERT	5275 LIGHTHOUSE POINT CT	sgoppert@...com
Barbara Jelphel-Warbs	227 Western Sand Pl.	970 278 9501
SHAROLYN LEERER	4440 MONTMORENCY PL	HEKERS33@1CLOUD.COM
Carlyn Peden & Matt Putney	5400 Nantucket Ct	mputney@gmail.com
Sharon McCrimmon	4420 W. 1st St	Ron@...mail.com
Ron McCrimmon	4420 W. 1st St.	

# PARKSIDE VILLAGE AT BOEDECKER HILL ENVIRONMENTALLY SENSITIVE AREAS REPORT<sup>©</sup>



July 2015  
Larimer County, Colorado

*Prepared by:*



**BLUE MOUNTAIN ENVIRONMENTAL CONSULTING**

Supporting Sustainable Management of Natural Resources

937 MALLARD DRIVE, FT. COLLINS, CO 80521 (970) 224-0851

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## 1.0 Study Area

This report documents ecological characteristics within the proposed Parkside Village community (the Property) on 42.9 acres (Parcel # 9520100001) in Loveland. The legal description for the site is a portion of the northeast quarter Section 20, Township 5 North, Range 69 West of the 6<sup>th</sup> Principal Meridian in Larimer County, Colorado. Numerous maps are provided in Appendix A including a Vicinity Map, Bubble Diagram, Site Concept Map and Aerial Map and Photo Point Map.

### 1.1 Project Background

The proposed Parkside Village community encompasses 42.9 acres (1,868,724 square feet) and is located along the north and eastern shoreline of Boedecker Lake in Loveland. West First Street borders the northern boundary of the Property for approximately 2,382 feet. The core development area consists of +/- 17.5 acres with greatest density occurring near the northeast corner of the parcel. Located on an existing bluff overlooking the lake, the community will likely consist of 100 lots containing townhomes, patio homes and single family residences. The lots in this area will be approximately 50 feet wide by 100 feet deep and will consist of homes ranging in size from 1,850 square feet to 3,200 square feet. East of the single-family detached homes, development will consist of duplexes or townhomes on lots that are approximately 24 feet wide by 90 feet deep and will consist of homes ranging from 1,250 square feet to 2,250 square feet. The type and number of residential structures is indicated in the below table.

Lots	Number
Townhomes (3-Plex)	27
Townhomes (4-Plex)	24
Patio Homes	20
Single Family Residences	29

Development adjacent to the Property includes the Mariana Butte community to the north, Mariana Springs and Mariana Cove communities to the west and private residences to the east. Boedecker Lake State Wildlife Area (SWA) forms the western boundary as well as the shoreline boundary of Boedecker Lake. Boedecker Lake SWA provides the public with recreational activities including hunting for dove and waterfowl, a warm-water fishery and wildlife viewing. The developer proposes to work with the City of Loveland Parks Department and other interested public stakeholders in their acquisition of approximately 24 of the 42.9 acres. This acquisition will provide community-wide benefit and maximize the value of Boedecker Lake and is well suited for the proposed open space, proposed park and linear trail to further enhance connectivity in the region.

## 2.0 Site Inventory

This report conforms to the Loveland Colorado 2005 Comprehensive Plan (Section 3.2), the 2014 Parks and Recreation Master Plan (Appendix E) and the Loveland Municipal Code (Chapters 18.41 and 16.20). The site was visited by Matt Tobler (Natural Resource Specialist) and Clint Hinebaugh (Wildlife Biologist) with Blue Mountain Environmental Consulting on July 15<sup>th</sup>, 2015. Additional information regarding the site and its characteristics was gathered from sources as cited in the following sections. Maps are provided in Appendix A.

### 2.1 Mature Stands of Vegetation

A diagonal berm separates the Property into northeast and southwest sections which support different ecological communities. A third community is found along the lakeshore. The below discussions detail species composition and ecological features of these areas. Community names correspond to the Colorado Vegetation Classification Project, a Vegetation Map is provided in Appendix A.

**Disturbed Rangelands:** The northwest section contains a prairie dog colony. This area is highly disturbed and dominated field bindweed (*Convolvulus arvensis*) a State listed noxious weed. Vegetation along roadsides was dominated by typical early seral species, including rabbitbrush (*Chrysothamnus* spp.), Russian thistle, Canada thistle (*Cirsium arvense*), yellow sweetclover (*Melilotus officinalis*), smooth brome and prickly lettuce (*Lactuca serriola*).

**Shrub / Grass / Forb Mix:** Native prairie dominates the area southwest of the berm to the irrigation ditch. Vegetation included yucca (*Yucca* spp.), skunkbrush sumac (*Rhus trilobata*), prickly pear (*Opuntia* spp.), hairy false goldenaster (*Heterotheca villosa*), fringed sage (*Artemisia frigida*), field sagewort (*Artemisia campestris*), yellow salsify (*Tragopogon dubius*), field bindweed, wooly loco (*Astragalus mollissimus*), prairie flax (*Linum lewisii*), beardtongue (*Pensetmon* spp.), Mexican hat (*Ratibida columnifera*), wavyleaf thistle (*Cirsium undulatum*), pepperweed (*Lepidium* spp.), Woods' rose (*Rosa woodsii*), scarlet globemallow (*Sphaeralcea coccinea*), white sagebrush (*Artemisia ludoviciana*), broom snakeweed (*Gutierrezia sarothrae*), mountain brome (*Bromus marginatus*), smooth brome (*Bromus inermis*), sideoats grama (*Bouteloua curtipendula*), blue grama (*Bouteloua gracilis*), threeawn (*Aristida* spp.), Indian ricegrass (*Achnatherum hymenoides*), Canada wildrye (*Elymus canadensis*), western wheatgrass (*Agropyron smithii*), needle and thread (*Hesperostipa comata*) and squirreltail (*Elymus elymoides*). Invasive species included cheatgrass (*Anisantha tectorum*) and musk thistle (*Carduus nutans*).

**Forested Riparian:** Vegetation between the irrigation ditch and the lake shore included plains cottonwood (*Populus deltoides*), green ash (*Fraxinus pennsylvanica*), peachleaf willow (*Salix amygdaloides*), poison ivy (*Toxicodendron rydbergii*), sedges, cattail, rushes and Russian thistle (*Salsola tragus*).

## 2.2 Jurisdictional and Non-Jurisdictional Wetlands

The National Wetlands Inventory (<http://www.fws.gov/wetlands/>) was assessed on July 22, 2015, and indicated that no wetlands exist on the Property. Buckingham Ditch extends for approximately 2,300 feet along the southern boundary and is a non-classified artificial wetland. In reality it is likely that wetlands exist between Buckingham Ditch and the high-water mark of the lake; however, no development is proposed for these areas, so wetlands were not delineated. A small depression in the northeast corner of the site was tested but did not support hydric soil indicators and as such does not qualify as wetland.

## 2.3 Wildlife Habitat and Corridors

An occupied 12.5-acre black-tailed prairie dog (*Cynomys ludovicianus*) colony exists on the northeast corner of the Property. Due to habitat fragmentation in the region, this colony has been isolated and habitat has been degraded as a result. Large infestations of field bindweed and bare ground are common throughout the colony and native grasses and forbs are almost non-existent.

South and west of the prairie dog colony, a transitional zone of native and non-native short- and mixed-grass prairie habitat exists. Seasonal and/or year-round use can be expected by a number of mammals such as mice, voles, fox squirrel (*Sciurus niger*) and eastern cottontail rabbit (*Sylvilagus florianus*). Given the Property's location adjacent to Boedecker Lake, species such as mule deer (*Odocoileus hemionus*), elk (*Cervus elaphus*), striped skunk (*Mephitis mephitis*) and red fox (*Vulpes fulva*) may be common.

With the exception of the margins along Boedecker Lake and the Buckingham Ditch, the Property is relatively treeless. Raptors would probably utilize the grasslands primarily for hunting rodents and small birds, especially during the warmer months. Some suitable nesting sites for hawks and owls were identified in the cottonwood stands to the south of the Property. Avifauna expected to utilize the Property include horned lark (*Eremophila alpestris*), burrowing owl (*Athene cunicularia*), American robin (*Turdus migratorius*), mourning dove (*Zenaidura macroura*), western kingbird (*Tyrannus verticalis*), magpies (*Pica pica*), killdeer (*Charadrius vociferus*), swallows, crows (*Corvus brachyrhynchos*) and ravens (*Corvus corax*).

There are no known occurrences or habitat for sensitive and specially valued species on the Property. The USFWS Information, Planning and Conservation System (IPAC) was accessed to determine species of concern in Larimer County, which include the Preble's meadow jumping mouse (*Zapus hudsonius preblei*), Ute lady's tresses orchid (*Spiranthes diluvialis*) and Colorado butterfly plant (*Gaura neomexicana* ssp. *coloradensis*), all of which are associated with riparian/wetland areas.

Typical habitat for the Preble's meadow jumping mouse is composed of well-developed riparian vegetation with adjacent, relatively undisturbed grassland communities and a nearby water source. These riparian areas should include a relatively dense combination of grasses, forbs and

shrubs. Based on these habitat requirements and the negative trapping data in the area, it is Blue Mountain Environmental Consulting's opinion that the Preble's meadow jumping mouse does not occur on the site and development of the Property will not adversely affect Preble's meadow jumping mouse.

Similarly, it is Blue Mountain Environmental Consulting's opinion that the Ute lady's tresses and Colorado butterfly plant do not occur on the site. The closest known occurrences of these plants are near the northwest boundary of Fort Collins (Ute lady's tresses) and within the City of Fort Collins-managed Soapstone Prairie Natural Area near the Wyoming border (butterfly plant). Suitable habitat is not present within the Property area.

## 2.4 Natural Areas Identified in the Sites Report

According to the City of Loveland Natural Areas Sites report (2008), Boedecker Lake (Site 55), is "a large open water body bordered by urban development on the south, open water on the north and grass/forb fields elsewhere. A few scattered large trees exist at-various locations. The lake lacks appreciable wetlands, probably because of fluctuating water levels. The value of this site lies in the large amount of open water that provides good waterfowl habitat, especially for migrating species. Areas of barren shoreline provide habitat for shorebirds. Waterfowl habitat would be improved with the creation of islands; if densely vegetated, these islands would provide duck nesting habitat but would discourage goose nesting. The Colorado Parks and Wildlife (CPW) notes that 25 pelicans use the lake and bald eagles are seen in the large trees in winter. The CPW also notes that the lake is important as a walleye and crappie fishery. The addition of shrubs and trees around the lakeshore would greatly benefit many species of wildlife, especially raptors. However, as urban development encroaches, raptor use of the area may decline. Similarly, if urban development eliminates habitat adjacent to the lake or interrupts movement corridors to and from the lake, wildlife values will decrease." The site is rated as having an overall habitat value of 6 (on a scale of 1 – 10), raptor habitat value of 6 and mammal habitat value of 4 with a 'medium' enhancement potential.

## 2.5 Physical Linkages

Residents of Loveland have grown accustomed to the resident and migratory elk population. A migration corridor for elk exists approximately 2 miles to the west, and the Property lies within a Winter Concentration Area. Buckingham Ditch and Boedecker Lake margins serve as a wildlife migration corridor on-site. In addition to avifauna, it is expected that raccoons, skunks and other urban-adapted species use these zones to move through the region. The current development plans do not call for development along the corridor; therefore, the proposed development should not affect the functioning of these areas and associated natural habitats as a migration corridor. According to the City of Loveland Natural Areas Sites report the shore on Boedecker Lake has connectivity to two other Natural Areas including the Small Lake North of Boedecker (Site 65) and the Ditch Northwest of Boedecker (Site 66).

## 2.6 Drainage Patterns and Floodways

At present, moisture drains to the northeast and southwest. The highest elevation on the Property is an artificial berm that is visible on the Aerial Map and in the contours of the Site Concept Map. According to the Loveland Floodplain Map accessed on July 22, 2015 (<http://maps.cityofloveland.org/Apps/?viewer=Floodplain>), the Property is outside of the floodplain.

## 2.7 Irrigation Canals, Ditches and Water Courses

Buckingham Ditch extends for approximately 2,300 feet along the southern boundary and is a non-classified artificial wetland.

## 2.8 Slopes Over 20 Percent

The entire Property is relatively flat, only small areas in excess of 20% slope may exist on the southern side of the artificial berm.

## 2.9 Soils

The Web Soil Survey (<http://websoilsurvey.sc.egov.usda.gov/App/HomePage.htm>) was accessed on July 29 to develop a custom soil report for the project area. There are six soil types in the project area. The depth to water table and erodibility rating is illustrated in the below table. In general, the erosion hazard is low and the depth to the water table is high.

SYMBOL	NAME	EROSION HAZARD	DEPTH TO WATER TABLE
48	Heldt clay loam 0-3% slope	Slight	>200 cm
53	Kim loam 0-3% slope	Slight	>200 cm
54	Kim loam 3-5% slope	Slight	>200 cm
59	Laporte-Rock outcrop complex3-30% slope	Slight	>200 cm
63	Longmont clay0-3% slope	Slight	69 cm
66	Minnequa silt loam 3-9% slope	Slight	>200 cm

**Erosion Hazard** The ratings in this interpretation indicate the hazard of soil loss from off-road and offtrail areas after disturbance activities that expose the soil surface. The ratings are based on slope and soil erosion factor K. The soil loss is caused by sheet or rill erosion in off-road or off-trail areas where 50 to 75 percent of the surface has been exposed by logging, grazing, mining, or other kinds of disturbance. The hazard is described as "slight," "moderate," "severe," or "very severe." A rating of "slight" indicates that erosion is unlikely under ordinary climatic conditions; "moderate" indicates that some erosion is likely and that erosion-control measures may be needed; "severe" indicates that erosion is very likely and that erosion-control measures, including revegetation of bare areas, are advised; and "very severe" indicates that significant erosion is expected, loss of soil productivity and off-site damage are likely, and erosion-control measures are costly and generally impractical.

**Depth to Water Table** "Water table" refers to a saturated zone in the soil. It occurs during specified months. Estimates of the upper limit are based mainly on observations of the water table at selected sites and on evidence of a saturated zone, namely grayish colors (redoximorphic features) in the soil.

## 2.10 Landfills and Hazardous Industrial Use

The site appears to be in its natural state and was likely used for agricultural and recreational purposes. Landfills, materials dumping and industrial use were not apparent; however, this report does not constitute a Phase 1 Environmental Assessment as defined by ASTM 1527.

## 2.11 Faults, Aquifers and Discharge Areas

The Geologic Hazards and Mineral Extraction Evaluation prepared by Earth Engineer Consultants, LLC (EEC, 2015) does not indicate any faults in the project area. According to the United States Geologic Service ([http://pubs.usgs.gov/ha/ha730/ch\\_c/C-text6.html](http://pubs.usgs.gov/ha/ha730/ch_c/C-text6.html)) accessed on July 31, 2015, Boedecker Lake may contribute to the Denver Basin Aquifer system.

## 2.12 Operating High Water Line

Although there is more than 3,000 feet of shoreline associated with Boedecker Lake on the Property, there are no wetlands or perennial water bodies on the site. Access to the shoreline is limited in summer due to water in the Buckingham Ditch. The high-water line is indicated on site maps.

# 3.0 Assessment of Potential Impacts of Proposed Development

Construction of Parkside Village will require the removal of the prairie dog colony. Black-tailed prairie dogs are not listed by the United States Fish and Wildlife Service (USFWS), recognized as small game by Colorado Parks and Wildlife and are considered a destructive rodent pest by the Colorado Department of Agriculture. Because the colony exists on private land, the

landowner must give consent prior to any activity or removal of prairie dogs. Removal options for this location include trapping, using poison baits and using fumigants (poison gas).

Trapping, transporting and relocating is generally the most expensive and least viable option and requires obtaining one of three possible permits from Colorado Parks and Wildlife as well as consent from the District Wildlife Manager. Options for this process include the following:

1. Wild to Black-Footed Ferret Program Permit
2. Wild to Raptor Rehabilitation Permit
3. Wild to Wild Relocation Permit

Several different types of pesticides can be used for prairie dog control/removal in Colorado, including grain baits and products that generate poison gases (fumigants). All of these products are federal restricted use pesticides, except for the USDA gas cartridge. Unless a private pesticide applicators license has been issued to the landowner by the Colorado Department of Agriculture, a commercial applicator must be hired that is licensed in the 'outdoor vertebrate' category. Whichever route is chosen, the developer should collaborate with the neighbor to the east, Mr. Raymond Cochrane, as the colony extends on to his property as well. Timing of prairie dog control should be considered prior to construction activities. For example, grain baits are typically most successful in the fall because prairie dogs eat grass seeds to build up fat reserves for the winter and any baits that contain zinc phosphide are restricted to July 1<sup>st</sup> – early winter. It is important to understand that removal of the prairie dogs might generate some disapproval by the general public.

A setback of 200 rather than 300 feet may be applicable in this case for several reasons. First, the most significant ecological values at the site result from cottonwood galleries and wetlands which are confined to a narrow band along the lakeshore. Second, nearly all site activities would be confined to an area dominated by a single noxious weed species resulting in minimal impacts to native vegetation. The proposed cluster development will further limit the extent of disturbance to ecological values while minimizing costs to public infrastructure. A significant public benefit will also be attained by construction of the proposed hard surface trail that will link other open spaces with the lakeshore on this property.

## 4.0 Recommendations for Protection, Mitigation and Enhancement

Potential raptor nesting habitat exists in the cottonwood gallery along Boedecker Lake. If construction or tree removal is proposed to begin during the potential bird nesting periods, a nest search is recommended within two weeks prior to the start of construction or tree removal to avoid Migratory Bird Treaty Act infractions. For this location, nesting activity should be monitored between February 15<sup>th</sup> to July 15<sup>th</sup> and recommended buffer zones and seasonal restrictions should be followed per CPW guidelines <https://cpw.state.co.us/Documents/WildlifeSpecies/LivingWithWildlife/RaptorBufferGuidelines2008.pdf>.

According to the provided diagram for Parkside Village, two zones of the +/- 17.5-acre development core will contain 5-8 Dwelling Units (DU)/acre along West First Street and 3-5 DU/acre on the bluff, respectively. This portion of the Property is already heavily disturbed from the prairie dog colony, and native prairie to the south will remain relatively un-disturbed. The nearly 2,300 feet of cottonwoods along the Lake margins will also remain undisturbed allowing avifauna, particularly raptors with nest sites, roost sites, and hunting perches for the open water habitat to the south. To reduce impacts to natural features on site, the developer has proposed to limit private yards within the community with emphasis placed on public open space.

Noxious weeds should be managed within all natural areas, disturbance corridors, paths, rights of way and developed areas to maintain the ecological integrity of areas not disturbed by construction. Management of noxious species within portions of the property not impacted by construction would enhance ecological values of these residual lands. Disturbed areas should be revegetated as soon as possible with an appropriate native seed mix. Best management practices should be employed throughout construction to limit soil erosion and sedimentation. Additionally, appropriately designed, constructed, and maintained storm water control systems should be employed to protect lake water quality

The team at Blue Mountain Environmental Consulting believes that no significant ecological resources will be adversely impacted on this site. The primary concern on-site is the ethical removal of black-tailed prairie dogs, timing of construction to avoid disturbing nesting birds, increased runoff from hardened surfaces and the introduction of undesirable plants. All these concerns can be mitigated by the employment of recommendations made herein.

## References

City of Loveland Floodplain Maps and Documents.

<http://maps.cityofloveland.org/Apps/?viewer=Floodplain>

City of Loveland Natural Area Sites. 2008. Cedar Creek Associates, Inc. City of Loveland.

Colorado Parks and Wildlife Recommended Buffer Zones and Seasonal Restrictions for Raptors.

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Earth Engineer Consultants. 2015. Geologic Hazards and Mineral Extraction Evaluation of Boedecker Hill.

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PARKSIDE VILLAGE SUBDIVISION  
TRAFFIC IMPACT STUDY

LOVELAND, COLORADO

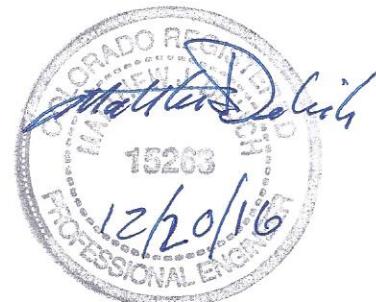
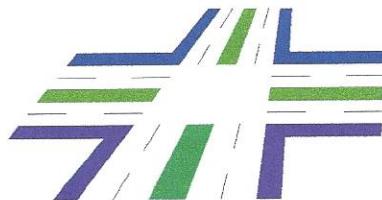
DECEMBER 2016

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Project #1697

PC ATTACHMENT D

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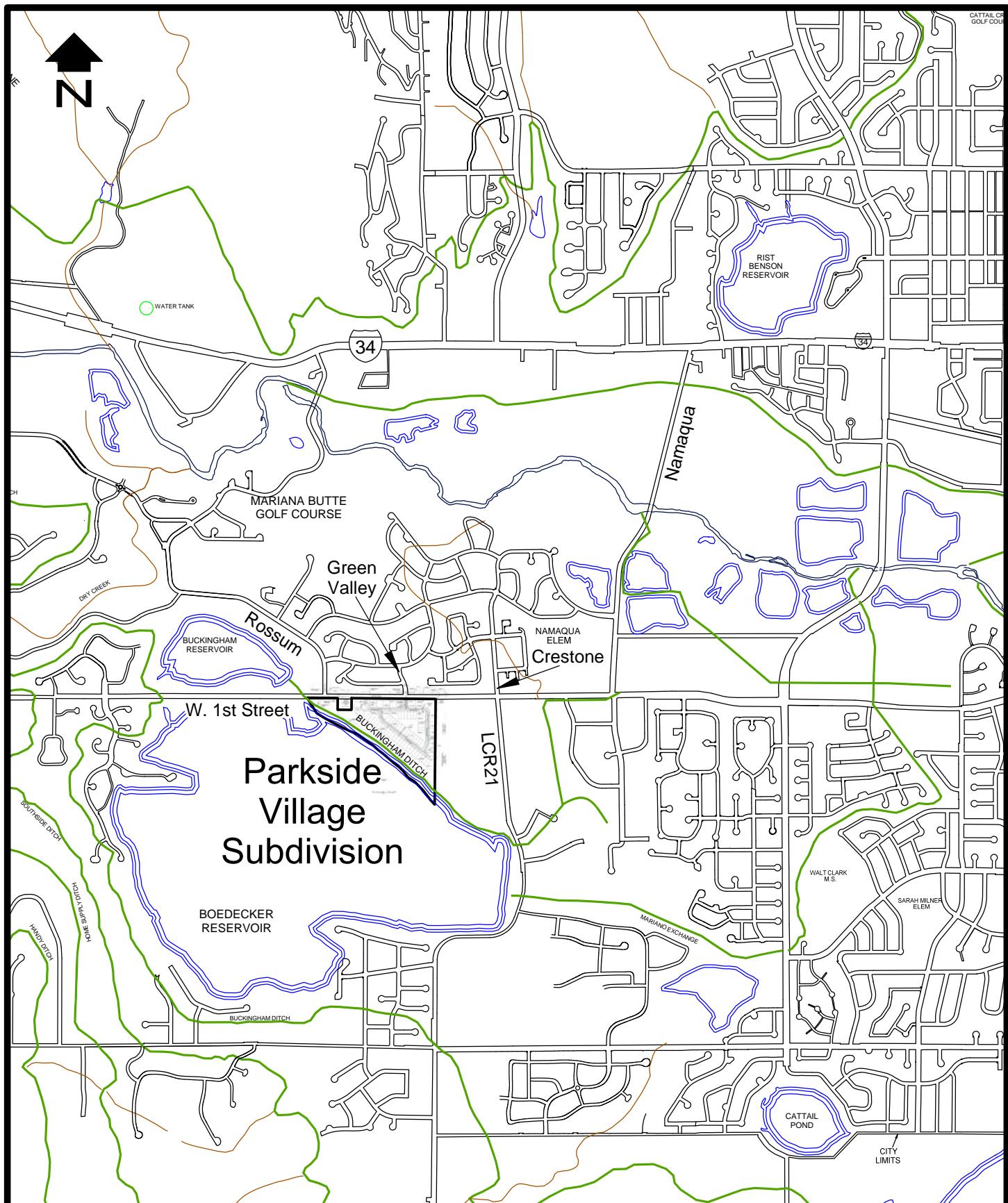
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## I. INTRODUCTION

This traffic impact study (TIS) is for the proposed Parkside Village Subdivision, located south of (adjacent to) W. 1<sup>st</sup> Street and west of Larimer County Road 21 (LCR21) in Loveland, Colorado. The location of this site is shown in Figure 1. This TIS addresses the operation at the key intersections for the short range (2021) and long range (2035) futures.

This study involved the collection of data, a review of previous developments and studies in the area, trip generation, trip distribution, trip assignment, and the operation analyses of the key intersections in the area for the existing conditions, the short range (2021) future, and the long range (2035) future. During the course of this analysis, numerous contacts were made with City staff, the project developer (Boedecker Partners LLC/Mosaic Real Estate LLC), the project engineer (Northern Engineering), and the project planner (TB Group). Since this land is within the City of Loveland, the traffic impact study guidelines for Loveland, as contained in the "Larimer County Urban Area Street Standards" (LCUASS) were used.

The following intersections, as agreed to in the scoping discussions, were addressed in this traffic study: W. 1<sup>st</sup> Street/Namaqua (major intersection), W. 1<sup>st</sup> Street/Crestone-LCR21 (major intersection), and W. 1<sup>st</sup> Street/Green Valley (minor intersection) intersections. Appendix A contains the Transportation Impact Study Base Assumptions form and related attachments for the Parkside Village Subdivision.



SCALE: 1"=2000'

## SITE LOCATION

Figure 1

## II. EXISTING CONDITIONS

### Land Use

The project site is currently vacant. The land surrounding the site consists of primarily residential uses. There are residential uses to the north, east, and west of the site. Boedecker Lake is to the south of the site. The center of Loveland lies to the east of the Parkside Village Subdivision site.

### Roads

A schematic of the current geometry at the W. 1<sup>st</sup> Street/Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley intersections is shown in Figure 2. The key existing streets are W. 1<sup>st</sup> Street, Namaqua Road, LCR21, Crestone Drive, and Green Valley Drive.

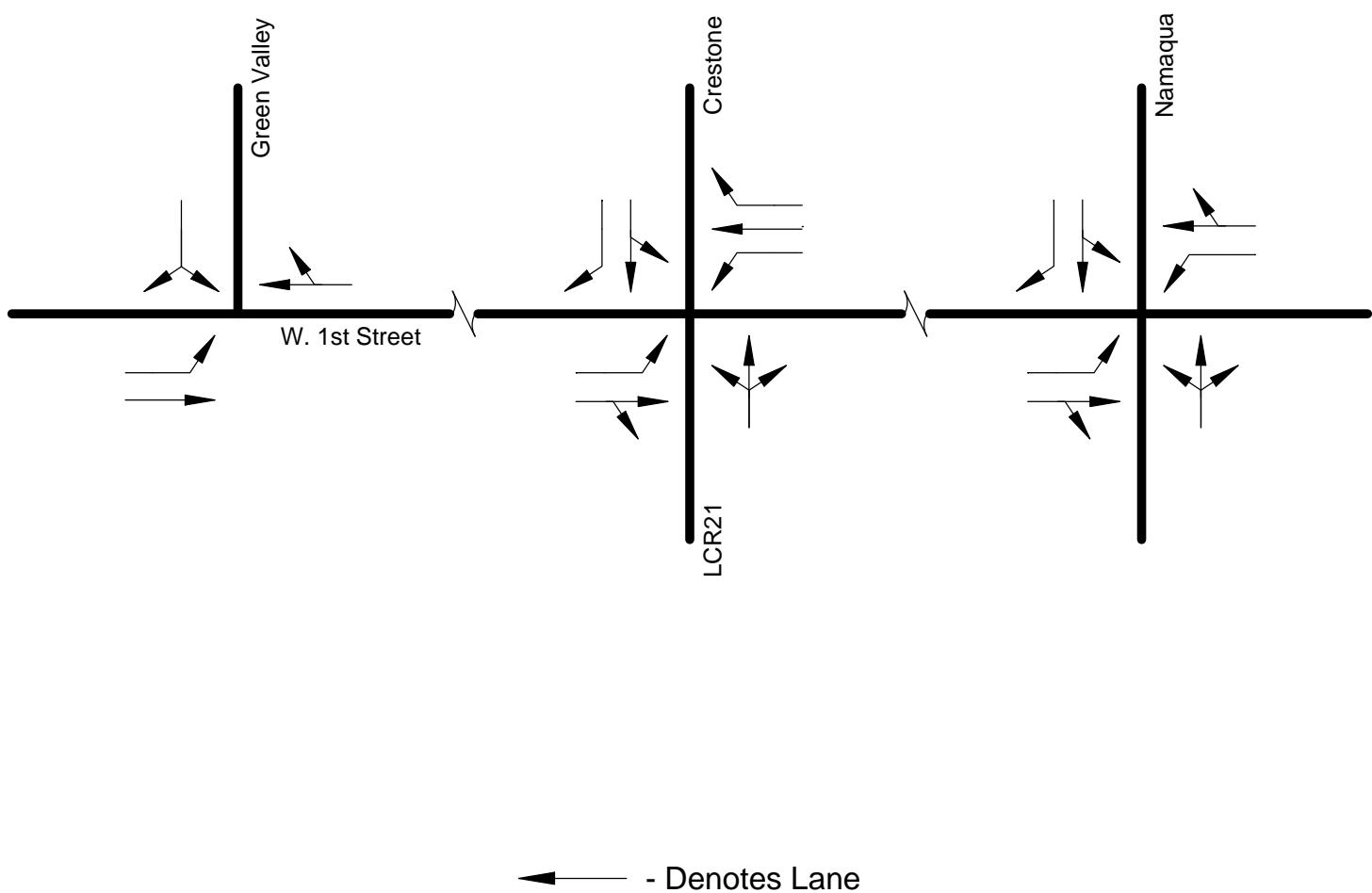
West 1<sup>st</sup> Street is classified as a two-lane arterial street. Currently, W. 1<sup>st</sup> Street has a two-lane cross section with auxiliary turn lanes at intersections. The posted speed on W. 1<sup>st</sup> Street, east of Namaqua Road, is 40 mph. The posted speed on W. 1<sup>st</sup> Street, west of Namaqua Road, is 45 mph. At the W. 1<sup>st</sup> Street/Namaqua intersection, W. 1<sup>st</sup> Street has eastbound and westbound left-turn lanes and one through lane in each direction. The W. 1<sup>st</sup> Street/Namaqua intersection has all-way stop sign control. At the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection, W. 1<sup>st</sup> Street has eastbound and westbound left-turn lanes, one through lane in each direction, and a westbound right-turn lane. The W. 1<sup>st</sup> Street/Crestone-LCR21 intersection has stop sign control on Crestone Drive/LCR21. At the W. 1<sup>st</sup> Street/Green Valley intersection, W. 1<sup>st</sup> Street has an eastbound left-turn lane and one through lane in each direction. The W. 1<sup>st</sup> Street/Green Valley intersection has stop sign control on Green Valley Drive.

Namaqua Road is classified as a major collector street north of W. 1<sup>st</sup> Street. South of W. 1<sup>st</sup> Street, Namaqua Road is a local street. At the W. 1<sup>st</sup> Street/Namaqua intersection, Namaqua Road has a combined northbound left-turn/through lane and a southbound right-turn lane. The south leg of Namaqua Road has all movements combined in a single lane. The north and south legs of Namaqua Road are off-set slightly ( $\approx$ 25 feet) at this intersection. The posted speed on Namaqua Road, north of W. 1<sup>st</sup> Street, is 35 mph.

Crestone Drive/LCR21 is classified as a major collector street. At the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection, Crestone Drive has a combined northbound left-turn/through lane and a southbound right-turn lane with a posted speed of 30 mph. Larimer County Road 21 has all movements combined in a single lane with a posted speed of 35 mph.

Green Valley Drive is classified as a local street. At the W. 1<sup>st</sup> Street/Green Valley intersection, Green Valley Drive has all southbound movements combined into a single lane. The posted speed on Green Valley Drive is 20 mph.

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## EXISTING INTERSECTION GEOMETRY

Figure 2

## Existing Traffic

Recent weekday peak hour traffic counts at the W. 1<sup>st</sup> Street/Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley intersections are shown in Figure 3. Raw traffic counts are provided in Appendix B. Traffic counts at the W. 1<sup>st</sup> Street/Namaqua intersection were obtained in December 2016. Traffic counts at the W. 1<sup>st</sup> Street/Crestone-LCR21 and W. 1<sup>st</sup> Street/Green Valley intersections were obtained in November 2016. Since counts were performed on different days, the volumes between the intersections were averaged/balanced and are shown in Figure 4.

## Existing Operation

Using the volumes shown in Figure 4, the current peak hour operation at the W. 1<sup>st</sup> Street/Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley intersections is shown in Table 1. Calculation forms for these analyses are provided in Appendix C. The key intersections were analyzed using the unsignalized intersection techniques from the 2010 Highway Capacity Manual (2010 HCM). Acceptable operation is defined by the City of Loveland as level of service (LOS) C or better overall. At major intersections, any leg can operate at level of service D and any movement can operate at level of service E. At minor intersections, any leg can operate at level of service E and any movement can operate at level of service F. A description of level of service at unsignalized intersections is provided in Appendix C. The Loveland Motor Vehicle LOS Standards are also provided in Appendix C. As can be seen in Table 1, the key intersections are currently operating acceptably with existing control and geometry. It is important to note that a westbound right-turn lane is currently warranted at the W. 1<sup>st</sup> Street/Namaqua intersection and a northbound right-turn lane is currently warranted at the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection in the morning peak hour with the existing traffic. In addition to this, a westbound right-turn lane is just over the threshold of being required at the W. 1<sup>st</sup> Street/Green Valley intersection in the afternoon peak hour. Since the development that Green Valley Drive serves is built-out, it is not likely that this right-turn lane will be constructed.

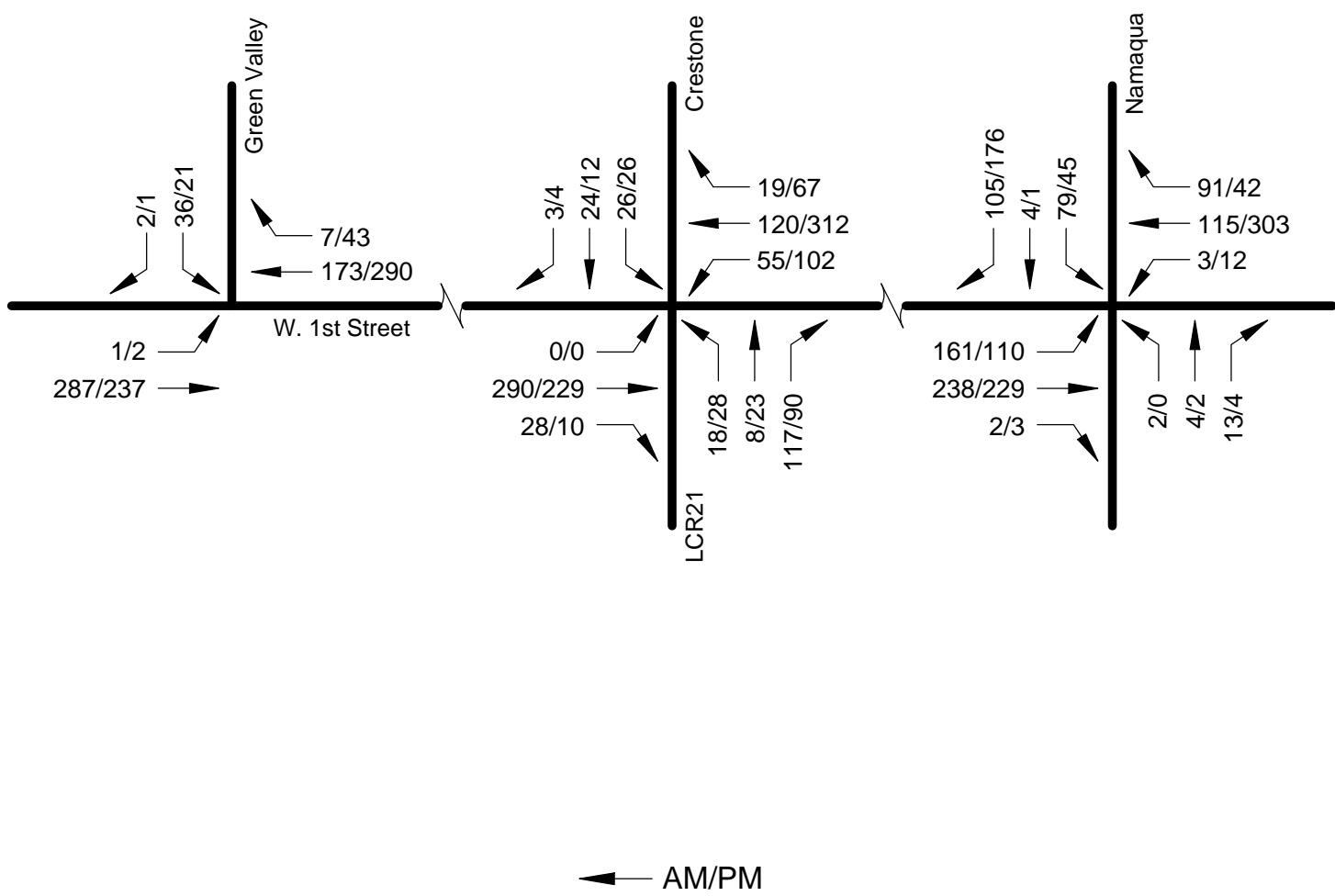
## Existing Pedestrians and Bicycles

During the vehicular traffic counts, pedestrian and bicycle traffic volumes were observed. The number of pedestrians and bicyclists was moderately low. Since the traffic counts were conducted in the winter season, it is likely that the number of pedestrians and bicycles would increase in warmer weather due to the proximity of Namaqua Elementary School.

## Accident Analysis

Accident data was obtained from the City of Loveland for the W. 1<sup>st</sup> Street/Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley intersections for a four year, eight month period (1/1/12 to 8/31/16). At the W. 1<sup>st</sup> Street/Namaqua

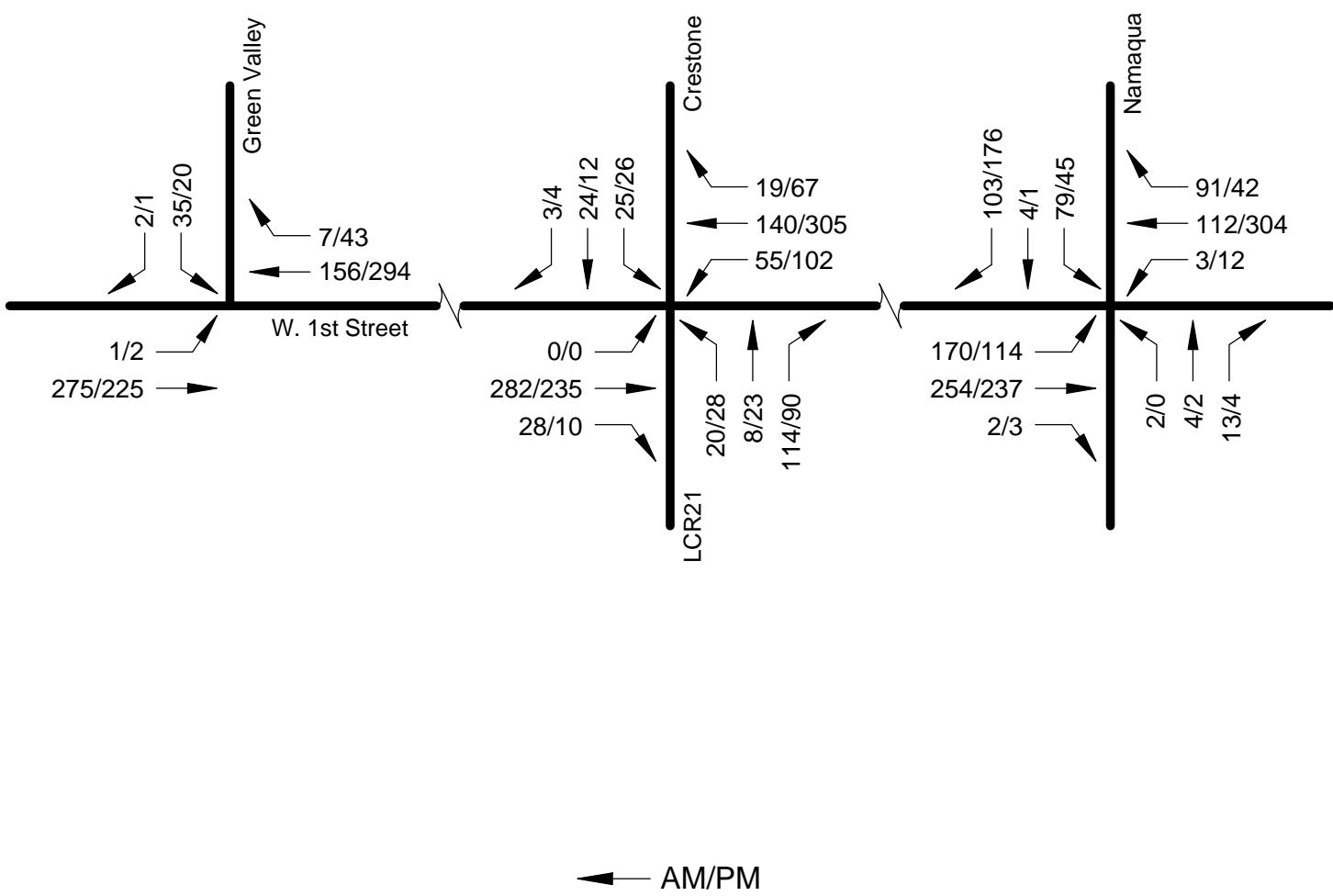
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## RECENT PEAK HOUR TRAFFIC

Figure 3

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## AVERAGED/BALANCED RECENT PEAK HOUR TRAFFIC

Figure 4

**TABLE 1**  
**Current Peak Hour Operation**

Intersection	Movement	Level of Service	
		AM	PM
W. 1 <sup>st</sup> Street/Namaqua (all-way stop)	EB LT	B	B
	EB T/RT	B	B
	EB APPROACH	B	B
	WB LT	A	A
	WB T/RT	B	C
	WB APPROACH	B	C
	NB LT/T/RT	A	A
	SB LT/T	B	B
	SB RT	A	B
	SB APPROACH	B	B
	OVERALL	B	B
W. 1 <sup>st</sup> Street/Crestone-LCR21 (stop sign)	NB LT/T/RT	B	C
	SB LT/T	C	D
	SB RT	A	B
	SB APPROACH	C	C
	EB LT	A	A
	WB LT	A	A
	OVERALL	A	A
W. 1 <sup>st</sup> Street/Green Valley (stop sign)	SB LT/RT	B	B
	EB LT	A	A
	OVERALL	A	A

intersection, there were five reported accidents: three rear-end accidents (one accident involved DUI) and one accident involving a vehicle striking a fixed object. One accident involved a single vehicle, but did not have any other information. At the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection, there were two reported accidents: one accident involving turning vehicles and one accident involving a vehicle striking a bicycle. At the W. 1<sup>st</sup> Street/Green Valley intersection, there was one reported accident: a single vehicle rollover accident. The number and type of accidents at the W. 1<sup>st</sup> Street/ Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley intersections are typical and are not considered to "high" accident locations.

### III. PROPOSED DEVELOPMENT

The Parkside Village Subdivision is a proposed residential development. Figure 5 shows the site plan for the Parkside Village Subdivision site. The Parkside Village Subdivision, as analyzed in this TIS, will consist of a 76 single family dwelling units. The analyses in this TIS assumed that Parkside Village Subdivision will be built out over the next 3-4 years, following approval. The analysis year for the short range future was assumed to be the year 2021 and the long range future was assumed to be the year 2035. There will be one full-movement access to/from W. 1<sup>st</sup> Street that lines up with Green Valley Drive.

#### Trip Generation

Trip generation is important in considering the impact of a development on the existing and proposed street system. Trip Generation, 9<sup>th</sup> Edition, ITE was used to determine the trips that would be generated by the Parkside Village Subdivision. A trip is defined as a one-way vehicle movement from origin to destination. Table 2 shows the expected trip generation from the site on a daily and peak hour basis. The full development trip generation resulted in 816 daily trip ends, 63 morning peak hour trip ends, and 82 afternoon peak hour trip ends.

**TABLE 2**  
**Trip Generation**

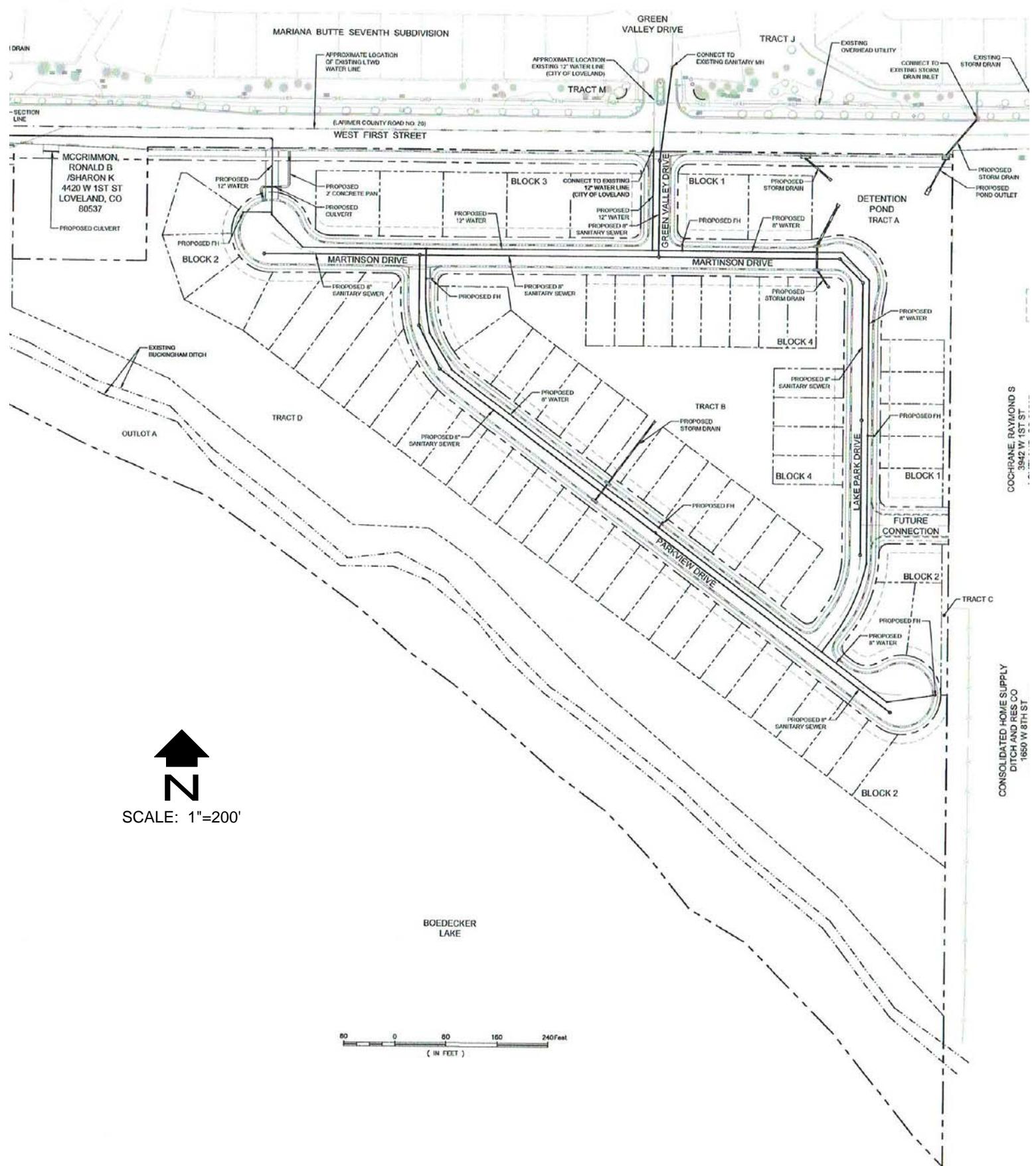
Code	Use	Size	AWDTE		AM Peak Hour				PM Peak Hour			
			Rate	Trips	Rate	In	Rate	Out	Rate	In	Rate	Out
210	Single Family	76 D.U.	EQ	816	EQ	16	EQ	47	EQ	52	EQ	30

#### Trip Distribution

Trip distribution for the Parkside Village Subdivision was estimated using knowledge of the existing and planned street system, existing traffic patterns, development trends, and engineering judgment. Figure 6 shows the trip distribution used in the following analyses. The trip distribution analysis was agreed to in the scoping discussions and is contained in Appendix A.

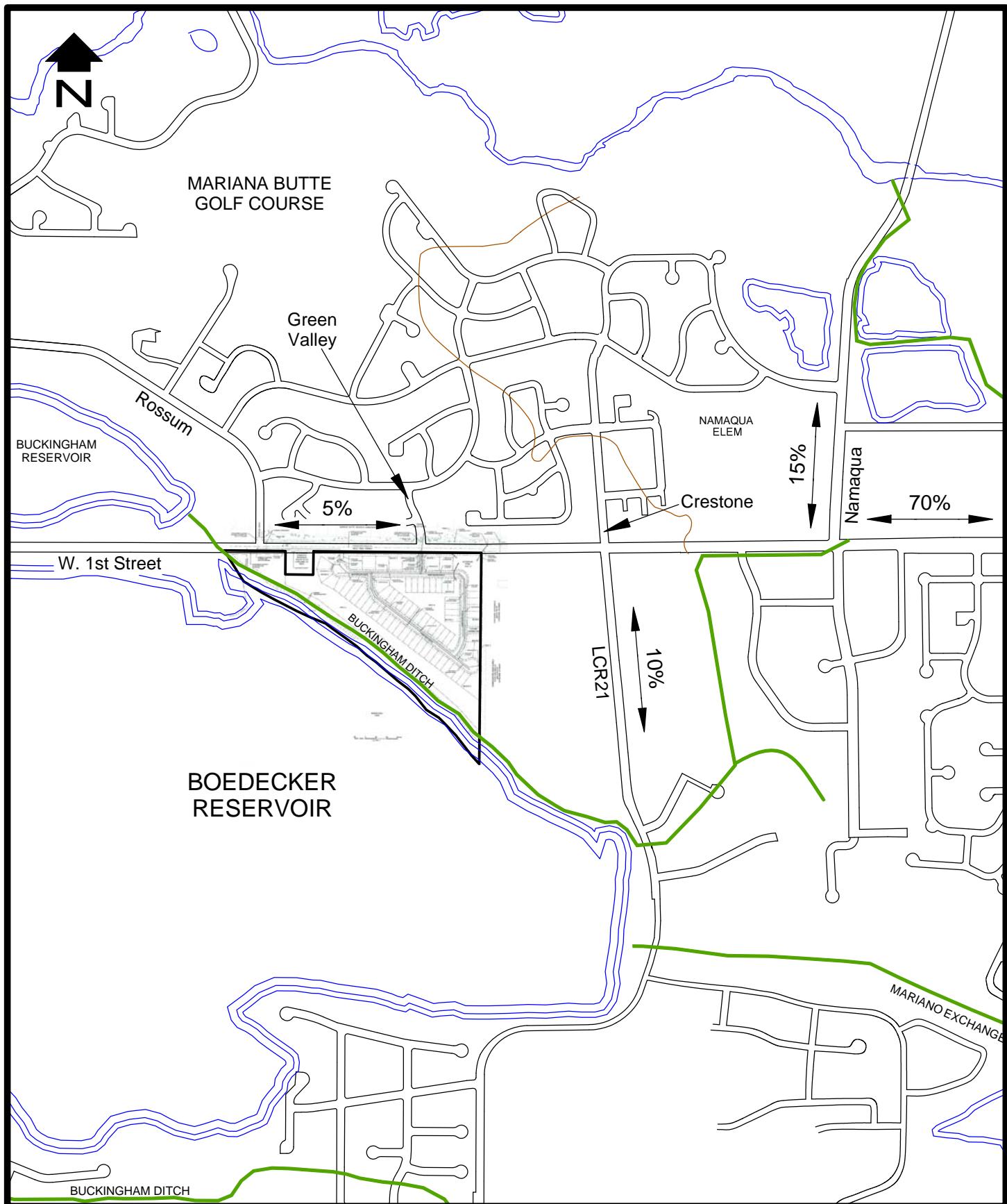
#### Traffic Assignment

Trip assignment is the product of both the trip generation and trip distribution processes. Figure 7 shows the site generated peak hour traffic at the key intersections.



## SITE PLAN

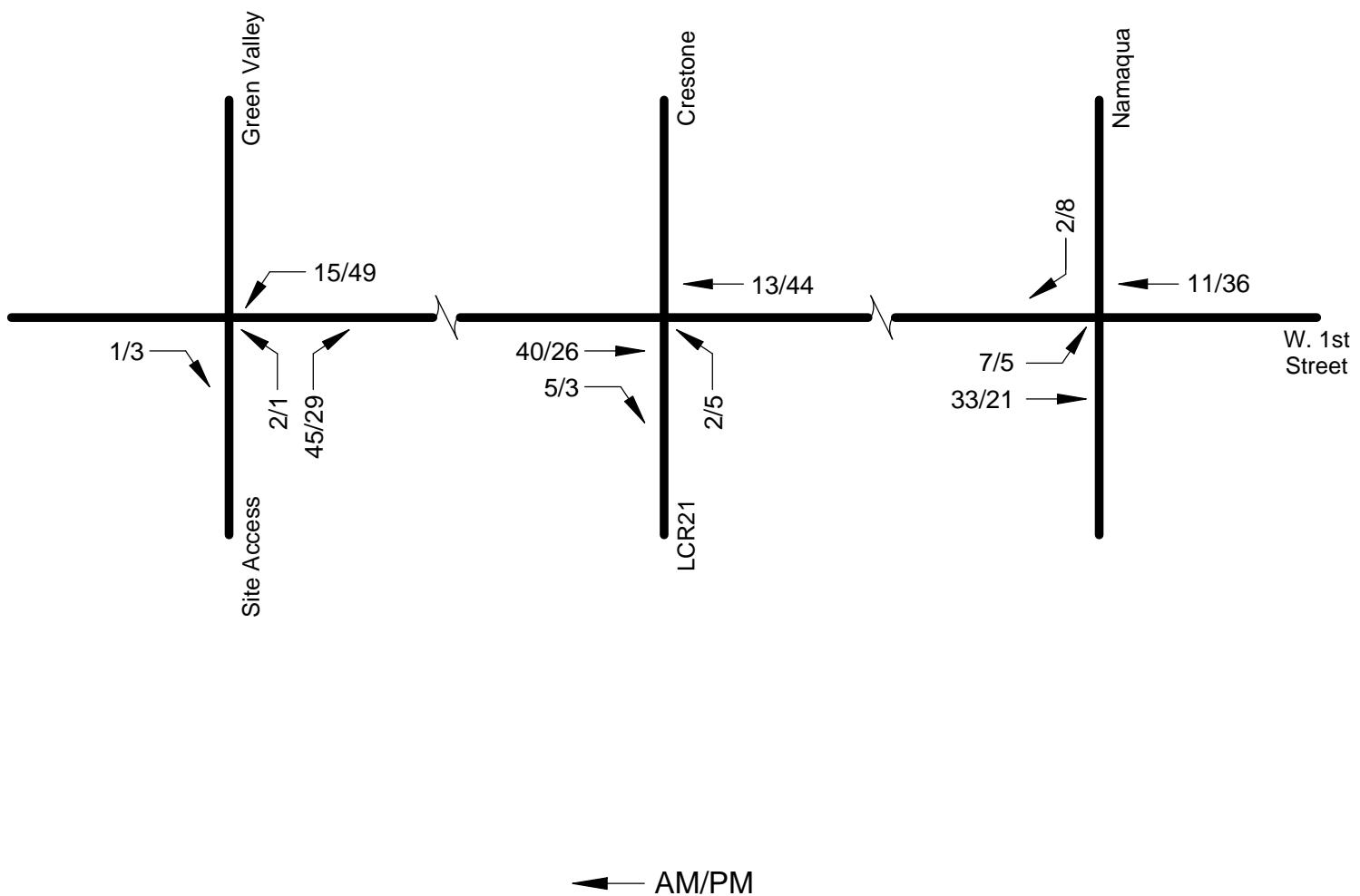
Figure 5



## TRIP DISTRIBUTION

Figure 6

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## SITE GENERATED PEAK HOUR TRAFFIC

Figure 7

## Background Traffic Projections

Background traffic projections for the short range (2021) and long range (2035) future horizons were obtained by factoring the existing traffic volumes on W. 1<sup>st</sup> Street, Namaqua Road (north leg), and LCR21 by two percent per year. There is a development under construction in the area of the Parkside Village Subdivision site called Mountain Gate. The site generated traffic from Mountain Gate was added to the short range (2021) background projections. Figures 8 and 9 show the short range (2021) and long range (2035) background peak hour traffic at the key intersections, respectively.

## Total Traffic

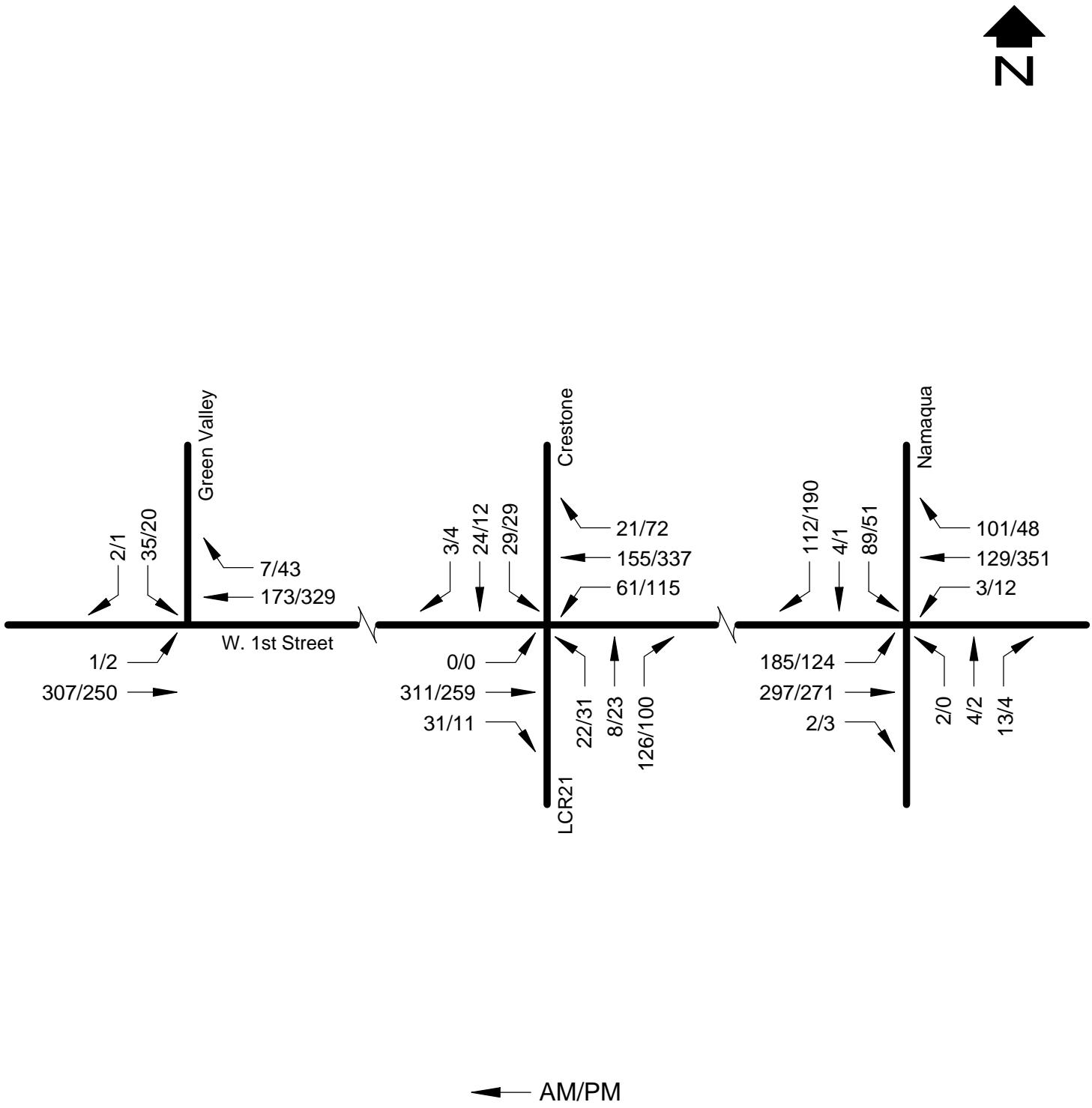
The traffic volumes generated by the proposed Parkside Village Subdivision were added to the background traffic volumes to produce the total traffic volume forecasts for the short range (2021) and long range (2035) futures. Figures 10 and 11 show the short range (2021) and long range (2035) total peak hour traffic, respectively.

Table 3 shows the short range (2021) link volumes for various key street segments with the Parkside Village Subdivision. Table 3 also shows the ACF volume thresholds for each street segment and whether that segment meets the Adequate Community Facilities Ordinance. The threshold volumes shown were calculated for this study. Calculations for the ACF threshold volumes are provided in Appendix D. Table 3 indicates that all links meet the requirements of the Adequate Community Facilities Ordinance.

## Signal Warrants

As a matter of policy, traffic signals are not installed at any location until such time that signal installation warrants are met according to the Manual on Uniform Traffic Control Devices. It is acknowledged that peak hour signal warrants should not be applied, but since the peak hour forecasts are readily available in a traffic impact study, it is reasonable to use them to estimate whether other signal warrants may be met. If peak hour signal warrants will not be met at a given intersection, it is reasonable to conclude that it is not likely that other signal warrants would be met. If peak hour signal warrants are met, it merely indicates that further evaluation should occur in the future as the development occurs. However, a judgment can be made that some intersections will likely meet other signal warrants.

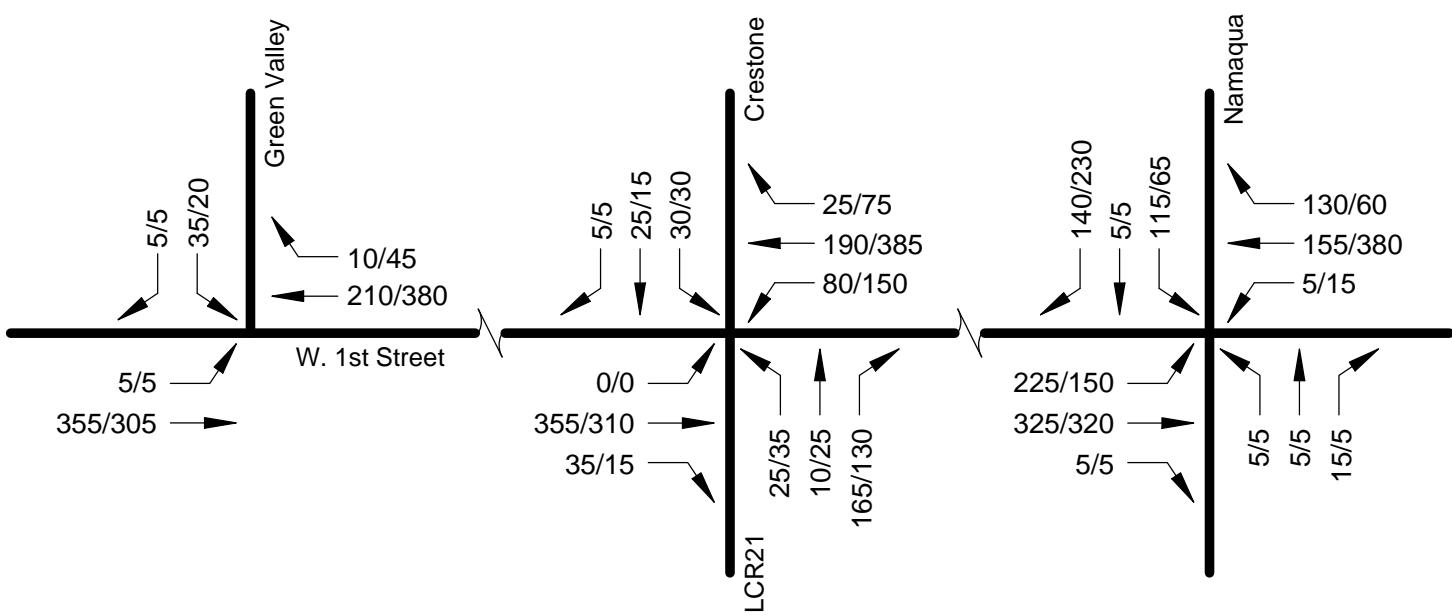
Using the long range (2035) total peak hour traffic (Figure 11), the W. 1<sup>st</sup> Street/ Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley intersections will not meet peak hour signal warrants. However, based upon the traffic volume on W. 1<sup>st</sup> Street, the north leg of Crestone Drive will not meet the City of Loveland level of service criteria. Peak hour signal warrants are provided in Appendix E.



# SHORT RANGE (2021) BACKGROUND PEAK HOUR TRAFFIC

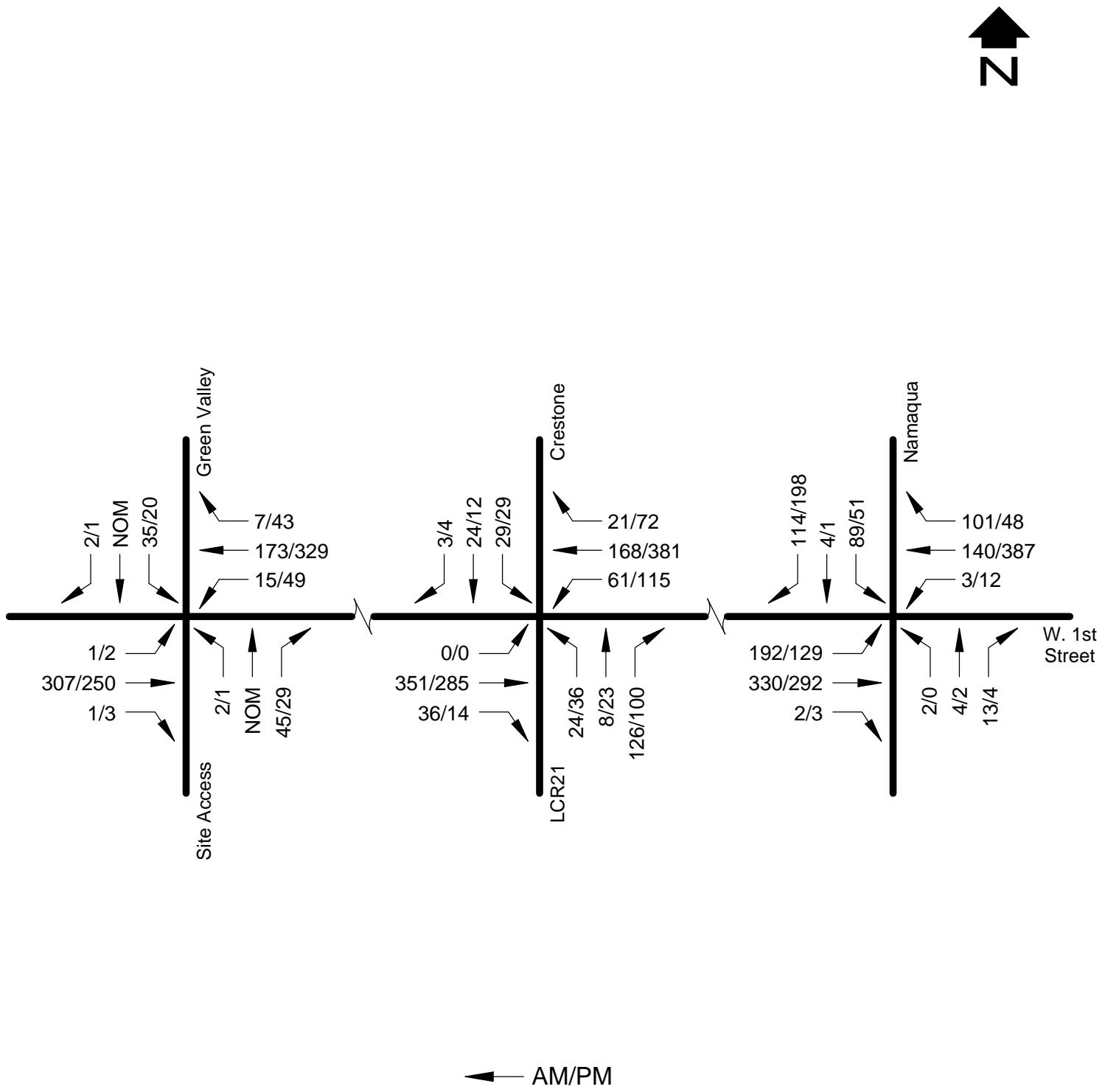
Figure 8

↑  
N



## LONG RANGE (2035) BACKGROUND PEAK HOUR TRAFFIC

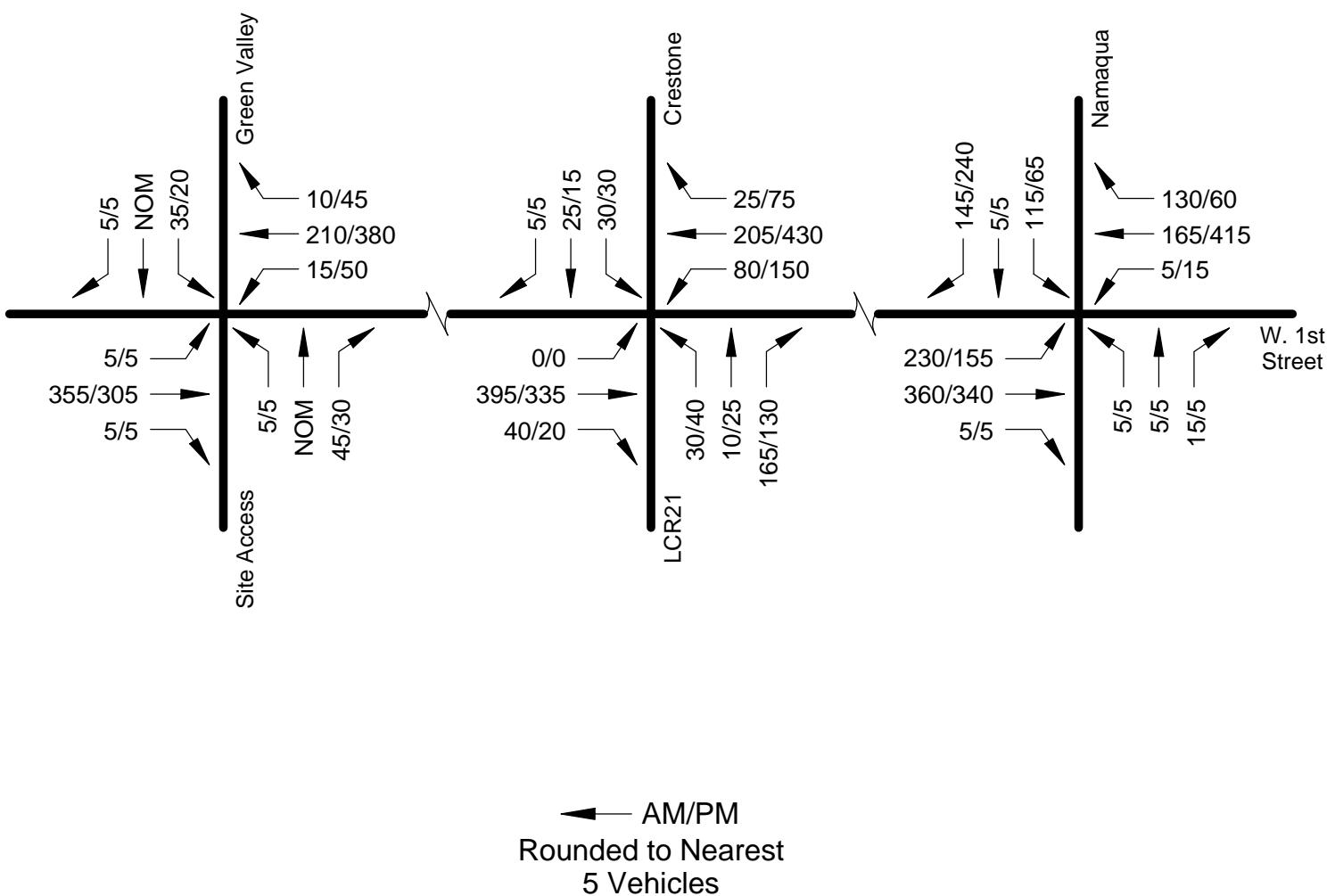
Figure 9



## SHORT RANGE (2021) TOTAL PEAK HOUR TRAFFIC

Figure 10

N  
↑



## LONG RANGE (2035) TOTAL PEAK HOUR TRAFFIC

Figure 11

**TABLE 3**  
**Street Traffic Volume Summary for the Existing and Short Range (2021) Conditions**

Street Segment		Direction	Existing Traffic Volume (AM/PM)	Date Existing Volume Taken	Regional Growth and Traffic from Build-out of Other Proposed Development* (AM/PM)	Site Generated Traffic (AM/PM)	Total Traffic (AM/PM)	ACF Traffic Threshold	ACF Compliance (AM/PM)	
1	W. 1 <sup>st</sup> Street east of Namaqua Road	EB	346/286	12/2016	399/326	33/21	432/347	810	Y/Y	
		WB	206/358	12/2016	233/411	11/36	244/447	810	Y/Y	
2	W. 1 <sup>st</sup> Street west of Namaqua Road	EB	426/354	12/2016	484/398	40/26	524/424	810	Y/Y	
		WB	217/480	12/2016	243/541	13/44	256/585	810	Y/Y	
3	W. 1 <sup>st</sup> Street west of Green Valley Drive	EB	276/227	11/2016	308/252	1/3	309/255	880	Y/Y	
		WB	158/295	11/2016	175/330	2/1	177/331	880	Y/Y	
4	Namaqua Road north of W. 1 <sup>st</sup> Street	NB	265/158	12/2016	290/174	7/5	297/179	590	Y/Y	
		SB	186/222	12/2016	205/242	2/8	207/250	590	Y/Y	
5	LCR21 south of W. 1 <sup>st</sup> Street	NB	142/141	11/2016	156/154	2/5	158/159	580	Y/Y	
		SB	107/124	11/2016	116/138	5/3	121/141	580	Y/Y	
6	Crestone Drive north of W. 1 <sup>st</sup> Street	NB	27/90	11/2016	29/95	0/0	29/95	600	Y/Y	
		SB	52/42	11/2016	56/45	0/0	56/45	600	Y/Y	
* Approved developments, not yet built:		Mountain Gate								
* Proposed developments, not yet approved:										
Notes/Comments										

## Geometric Requirements

Figure 12 shows the short range (2021) approach geometry at the key intersections. The geometry at the W. 1<sup>st</sup> Street/Namaqua and W. 1<sup>st</sup> Street/Crestone-LCR21 intersections can continue to remain as it exists today. However, as was mentioned earlier, a westbound right-turn lane is currently warranted at the W. 1<sup>st</sup> Street/Namaqua intersection and a northbound right-turn lane is currently warranted at the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection with the existing traffic. The Parkside Village Subdivision contributes no traffic to the westbound right-turn movement at the W. 1<sup>st</sup> Street/Namaqua intersection and contributes no traffic to the northbound right-turn movement at the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection. The westbound right-turn lane at the W. 1<sup>st</sup> Street/Namaqua intersection is not likely to be constructed until the property in the northeast quadrant is developed. The northbound right-turn lane at the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection is not likely to be constructed until the properties in the southwest and southeast quadrants are developed. In addition to this, a westbound right-turn lane is just over the threshold of being required at the W. 1<sup>st</sup> Street/Green Valley intersection in the afternoon peak hour. Since the development that Green Valley Drive serves is built-out, it is not likely that this right-turn lane will be constructed. At the W. 1<sup>st</sup> Street/Green Valley-Site Access intersection, a westbound left-turn lane is required.

Figure 13 shows the long range (2035) approach geometry at the key intersections. The long range (2035) geometry was developed based upon the operation of the key intersections. It is assumed that the westbound right-turn lane at the W. 1<sup>st</sup> Street/Namaqua and the northbound right-turn lane at the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection would be constructed by/before the long range (2035) future.

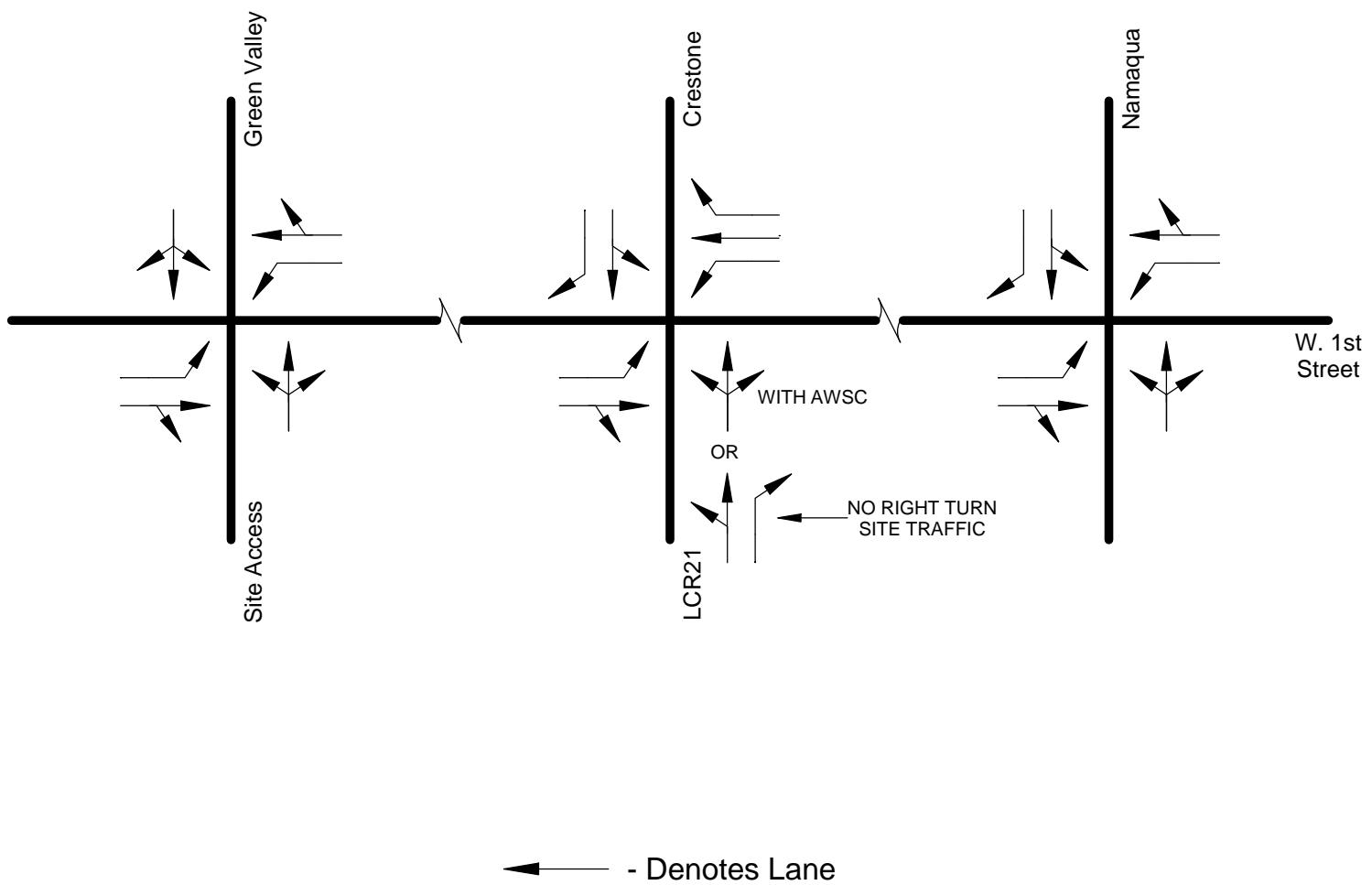
## Operation Analysis

Operation analyses were performed at the W. 1<sup>st</sup> Street/Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley-Site Access intersections. The operations analyses were conducted for the short range future, reflecting a year 2021 condition, and long range future, reflecting a year 2035 condition. The long range (2035) operational analyses are used for planning and informational purposes only.

Table 4 shows the short range (2021) background peak hour operation at the key intersections. The key intersections meet the Loveland level of service standards in the peak hours with the existing control. Calculation forms for these analyses are provided in Appendix F.

Table 5 shows the long range (2035) background peak hour operation at the key intersections. Calculation forms are provided in Appendix G.

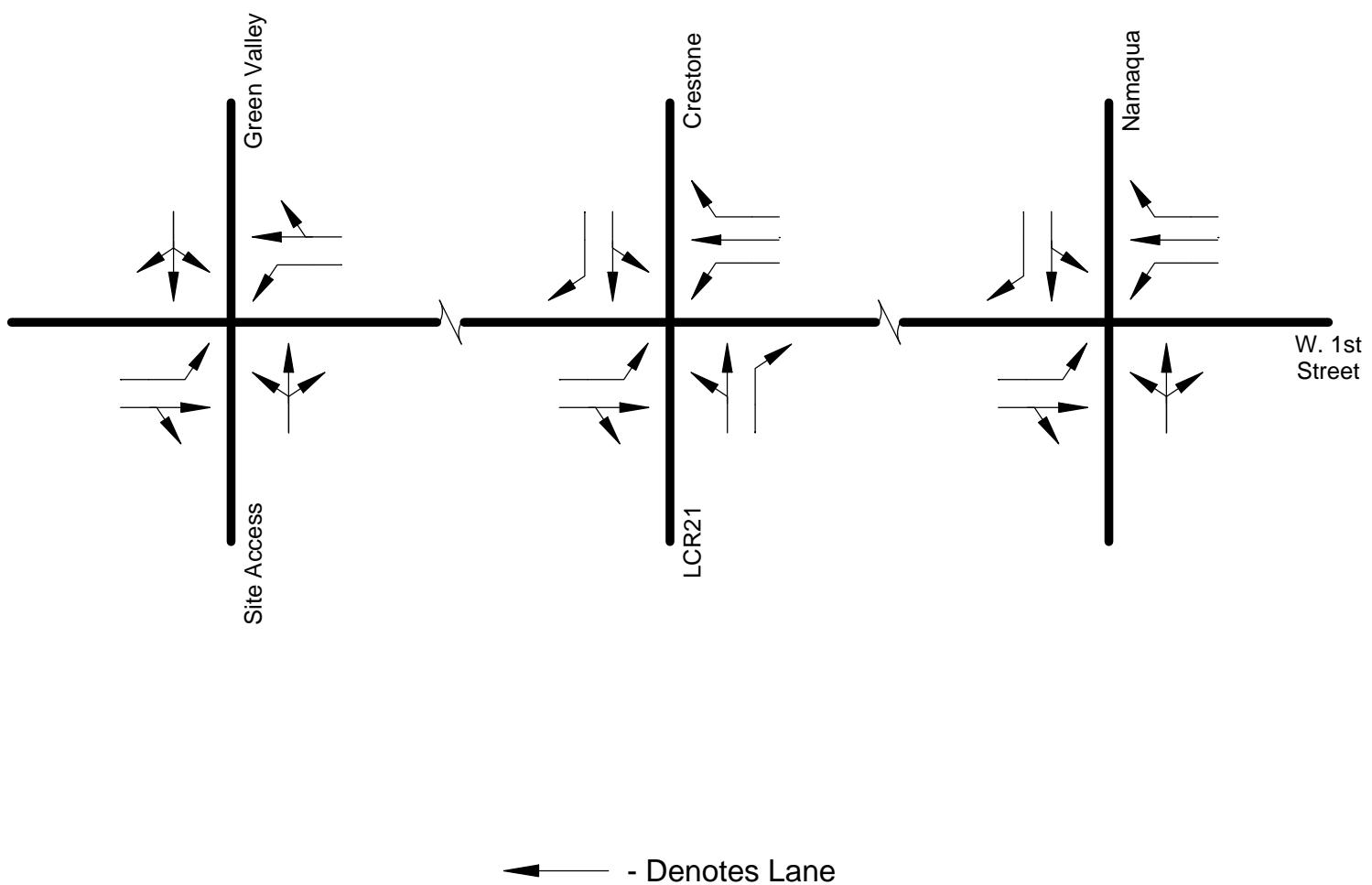
N



## SHORT RANGE (2021) GEOMETRY

Figure 12

N



LONG RANGE (2035) GEOMETRY

Figure 13

**TABLE 4**  
**Short Range (2021) Background Peak Hour Operation**

Intersection	Movement	Level of Service	
		AM	PM
W. 1 <sup>st</sup> Street/Namaqua (all-way stop)	EB LT	B	B
	EB T/RT	C	B
	EB APPROACH	B	B
	WB LT	A	A
	WB T/RT	B	C
	WB APPROACH	B	C
	NB LT/T/RT	B	B
	SB LT/T	B	B
	SB RT	B	B
	SB APPROACH	B	B
	OVERALL	B	C
W. 1 <sup>st</sup> Street/Crestone-LCR21 (stop sign)	NB LT/T/RT	B	C
	SB LT/T	C	D
	SB RT	A	B
	SB APPROACH	C	D
	EB LT	A	A
	WB LT	A	A
	OVERALL	A	A
W. 1 <sup>st</sup> Street/Green Valley (stop sign)	SB LT/RT	B	B
	EB LT	A	A
	OVERALL	A	A

**TABLE 5**  
**Long Range (2035) Background Peak Hour Operation**

Intersection	Movement	Level of Service	
		AM	PM
W. 1 <sup>st</sup> Street/Namaqua (all-way stop)	EB LT	C	B
	EB T/RT	C	C
	EB APPROACH	C	C
	WB LT	B	B
	WB T	B	D
	WB RT	B	B
	WB APPROACH	B	D
	NB LT/T/RT	B	B
	SB LT/T	B	B
	SB RT	B	C
	SB APPROACH	B	C
	OVERALL	C	C
W. 1 <sup>st</sup> Street/Crestone-LCR21 (stop sign)	NB LT/T	C	E
	NB RT	B	B
	NB APPROACH	B	C
	SB LT/T	C	E
	SB RT	A	B
	SB APPROACH	C	E
	EB LT	A	A
	WB LT	A	A
	OVERALL	A	A
W. 1 <sup>st</sup> Street/Green Valley (stop sign)	SB LT/RT	B	B
	EB LT	A	A
	OVERALL	A	A

Using the traffic volumes shown in Figure 10, Table 6 shows the short range (2021) total peak hour operation at the key intersections. Calculation forms for these analyses are provided in Appendix H. The key intersections meet the Loveland level of service standards in the peak hours, except for the north leg (Crestone Drive) of the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection during the afternoon peak hour. The W. 1<sup>st</sup> Street/Crestone-LCR21 intersection will meet the Loveland level of service standards with either of two mitigation measures: 1) all-way stop sign control with the current geometry or 2) installation of the required northbound right-turn lane on LCR21. The Parkside Village Subdivision contributes no traffic to the northbound right-turn movement nor does it contribute any traffic to the southbound traffic on Crestone Drive. Therefore, the most appropriate mitigation measure at this intersection is all-way stop sign control.

Using the traffic volumes shown in Figure 11, Table 7 shows the long range (2035) total peak hour operation at the key intersections. Calculation forms for these analyses are provided in Appendix I.

### Sight Distance

To the west of the W. 1<sup>st</sup> Street/Crestone-LCR21 and W. 1<sup>st</sup> Street/Green Valley-Site Access intersections, there are down sloping vertical curves. As a two-lane arterial, W. 1<sup>st</sup> Street has a design speed of 45 mph. According to LCUASS, Table 7-4 and Table 7-6, the stopping sight distance on a two-lane arterial at 45 mph is 325 feet. The sight distance was verified in the field where the stopped vehicle on the minor road is 13 feet back from the edge of W. 1<sup>st</sup> Street and measured from a height of eye at 3.5 feet. The height of the vehicle on W. 1<sup>st</sup> Street is 4.25 feet. The sight distance to the west of the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection was measured to be 618 feet, which is adequate. The sight distance to the west of the W. 1<sup>st</sup> Street/Green Valley-Site Access intersection was measured to be 670 feet, which is adequate. The sight distance at the W. 1<sup>st</sup> Street/Namaqua intersection is adequate.

### Bicycle/Pedestrian Facilities

Bicycle lanes exist along W. 1<sup>st</sup> Street, Namaqua Road (north leg), and Crestone Drive. It is expected that as streets are built and/or improved, bicycle lanes will be incorporated into the street cross sections where appropriate.

The sidewalk system in this area exists adjacent to developed parcels of land. Sidewalks will be built along the frontage of and within the Parkside Village Subdivision. As shown in Appendix J, one potential pedestrian destination within 1320 feet of the Parkside Village Subdivision site was identified: 1) the residential neighborhood (Mariana Butte) to the north of the site. Appendix J contains a graphic depicting the pedestrian influence area and these two pedestrian destinations. A pedestrian level of service worksheet is also provided in Appendix J. This worksheet shows the level of service for each quality indicator. Since the City of Loveland has no minimum level of service criteria, this level of service could not be indicated.

**TABLE 6**  
**Short Range (2021) Total Peak Hour Operation**

Intersection	Movement	Level of Service	
		AM	PM
W. 1 <sup>st</sup> Street/Namaqua (all-way stop)	EB LT	B	B
	EB T/RT	C	C
	EB APPROACH	C	C
	WB LT	A	A
	WB T/RT	B	D
	WB APPROACH	B	D
	NB LT/T/RT	B	B
	SB LT/T	B	B
	SB RT	B	B
	SB APPROACH	B	B
	OVERALL	B	C
W. 1 <sup>st</sup> Street/Crestone-LCR21 (stop sign with existing geometry)	NB LT/T/RT	C	C
	SB LT/T	D	E
	SB RT	A	B
	SB APPROACH	C	E
	EB LT	A	A
	WB LT	A	A
	OVERALL	A	A
W. 1 <sup>st</sup> Street/Crestone-LCR21 (stop sign with NB RT Lane)	NB LT/T	C	D
	NB RT	B	B
	NB APPROACH	B	C
	SB LT/T	C	E
	SB RT	A	B
	SB APPROACH	C	D
	EB LT	A	A
	WB LT	A	A
	OVERALL	A	A
W. 1 <sup>st</sup> Street/Crestone-LCR21 (all-way stop with existing geometry)	NB LT/T/RT	B	B
	SB LT/T	B	B
	SB RT	A	B
	SB APPROACH	B	B
	EB LT	A	A
	EB T/RT	D	C
	EB APPROACH	D	C
	WB LT	B	B
	WB T	B	D
	WB RT	A	A
	WB APPROACH	B	C
	OVERALL	C	C
W. 1 <sup>st</sup> Street/Green Valley-Site Access (stop sign)	NB LT/T/RT	B	B
	SB LT/T/RT	B	C
	EB LT	A	A
	WB LT	A	A
	OVERALL	A	A

**TABLE 7**  
**Long Range (2035) Total Peak Hour Operation**

Intersection	Movement	Level of Service	
		AM	PM
W. 1 <sup>st</sup> Street/Namaqua (all-way stop)	EB LT	C	C
	EB T/RT	C	D
	EB APPROACH	C	D
	WB LT	B	B
	WB T	B	E
	WB RT	B	B
	WB APPROACH	B	E
	NB LT/T/RT	B	B
	SB LT/T	B	B
	SB RT	B	C
	SB APPROACH	B	C
	OVERALL	C	D
W. 1 <sup>st</sup> Street/Crestone-LCR21 (stop sign)	NB LT/T	C	E
	NB RT	B	B
	NB APPROACH	B	C
	SB LT/T	D	E
	SB RT	A	B
	SB APPROACH	D	E
	EB LT	A	A
	WB LT	A	A
	OVERALL	A	A
W. 1 <sup>st</sup> Street/Green Valley-Site Access (stop sign)	NB LT/T/RT	B	B
	SB LT/T/RT	C	C
	EB LT	A	A
	WB LT	A	A
	OVERALL	A	A

## Adequate Community Facilities

All of the key intersections meet the City of Loveland criteria of level of service during the peak hours with regard to operation, except for the north leg (Crestone Drive) of the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection during the afternoon peak hour. The W. 1<sup>st</sup> Street/Crestone-LCR21 intersection will meet the Loveland level of service standards with either of two mitigation measures: 1) all-way stop sign control with the current geometry or 2) installation of the required northbound right-turn lane on LCR21. Therefore, all intersections, with the control devices that either exist or will be installed and with the existing geometry, meet requirements of the Loveland Adequate Community Facilities Ordinance. As noted on Table 3, all street links meet the level of service C standard of the Loveland Adequate Community Facilities Ordinance.

#### IV. CONCLUSIONS/RECOMMENDATIONS

This study assessed the transportation impacts associated with the development of the Parkside Village Subdivision in Loveland, Colorado. This study analyzed the transportation impacts in the short range (2021) and long range (2035) futures. As a result of these analyses, the following is concluded:

- Development of the Parkside Village Subdivision is feasible from a traffic engineering standpoint. The full development trip generation resulted in 816 daily trip ends, 63 morning peak hour trip ends, and 82 afternoon peak hour trip ends.
- Current operation at the W. 1<sup>st</sup> Street/Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley intersections is acceptable based upon City of Loveland evaluation criteria. It is important to note that a westbound right-turn lane is currently warranted at the W. 1<sup>st</sup> Street/Namaqua intersection and a northbound right-turn lane is currently warranted at the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection in the morning peak hour with the existing traffic. In addition to this, a westbound right-turn lane is just over the threshold of being required at the W. 1<sup>st</sup> Street/Green Valley intersection in the afternoon peak hour. Since the development that Green Valley Drive serves is built-out, it is not likely that this right-turn lane will be constructed
- Using the long range (2035) total peak hour traffic (Figure 11), the W. 1<sup>st</sup> Street/Namaqua, W. 1<sup>st</sup> Street/Crestone-LCR21, and W. 1<sup>st</sup> Street/Green Valley intersections will not meet peak hour signal warrants.
- The short range (2021) range and long range (2035) geometry is shown in Figures 12 and 13, respectively. In the short range (2021) future, the geometry at the W. 1<sup>st</sup> Street/Namaqua and W. 1<sup>st</sup> Street/Crestone-LCR21 intersections can continue to remain as it exists today. At the W. 1<sup>st</sup> Street/Green Valley-Site Access intersection, a westbound left-turn lane is required. The long range (2035) geometry was developed based upon the operation of the key intersections. It is assumed that the westbound right-turn lane at the W. 1<sup>st</sup> Street/Namaqua intersection and the northbound right-turn lane at the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection would be constructed by/before the long range (2035) future.
- Using the short range (2021) background traffic, the key intersections operate at acceptable levels of service in the peak hours.
- With short range (2021) background traffic and the Parkside Village Subdivision, the key intersections meet the Loveland level of service standards in the peak hours, except for the north leg (Crestone Drive) of the W. 1<sup>st</sup> Street/Crestone-LCR21 intersection during the afternoon peak hour. The W. 1<sup>st</sup> Street/Crestone-LCR21 intersection will meet the Loveland level of service standards with either

of two mitigation measures: 1) all-way stop sign control with the current geometry or 2) installation of the required northbound right-turn lane on LCR21. The Parkside Village Subdivision contributes no traffic to the northbound right-turn movement nor does it contribute any traffic to the southbound traffic on Crestone Drive. Therefore, the most appropriate mitigation measure is all-way stop sign control.

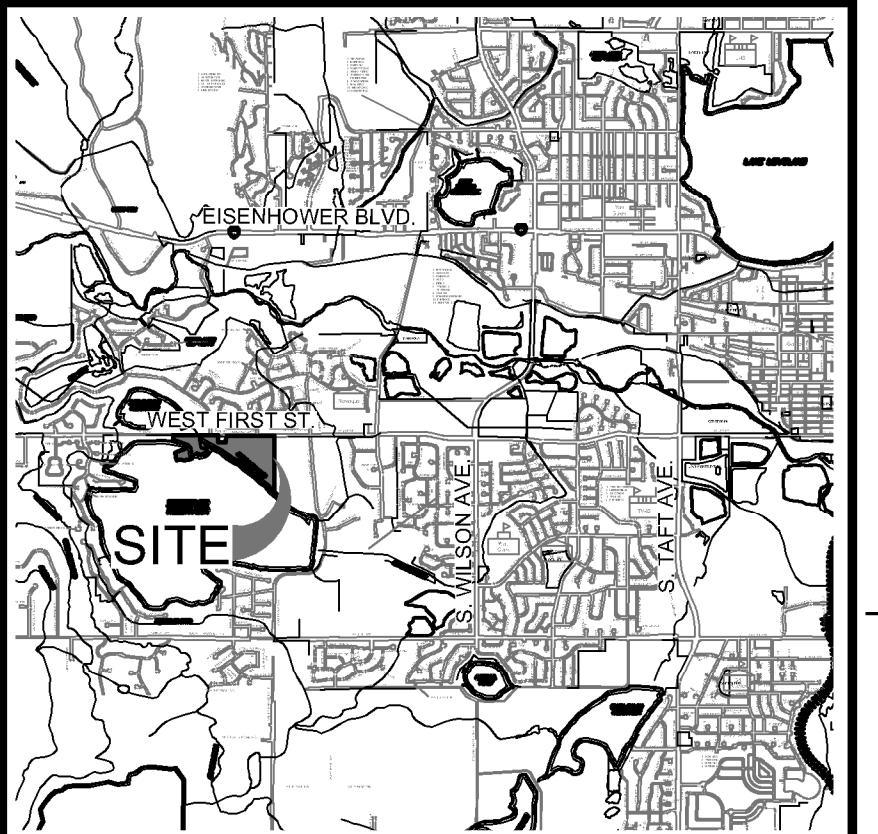
- Bicycle lanes exist along W. 1<sup>st</sup> Street, Namaqua Road (north leg), and Crestone Drive. The sidewalk system in this area exists adjacent to developed parcels of land. As streets are improved, sidewalks will be incorporated to the standard cross sections.
- Table 8 shows a summary of the recommended improvements and the responsibility for that improvement.
- No environmental or special studies are required with this development.

**TABLE 8**  
**Recommended Improvements Summary**

<b>Improvement Description and Location</b>	<b>Responsible Party</b>		
	<b>Applicant</b>	<b>Background</b>	<b>Master Planned</b>
<b>SHORT RANGE (2021)</b>			
On-site infrastructure.	X		
Stripe westbound left-turn lane at W. 1 <sup>st</sup> Street/Green Valley-Site Access intersection. This will also entail widening on the south side of W. 1 <sup>st</sup> Street along the site frontage.	X		
All-way stop sign control at W. 1 <sup>st</sup> Street/Crestone-LCR21 intersection.	X		
<b>LONG RANGE (2035)</b>			
Construct a westbound right-turn lane at the W. 1 <sup>st</sup> Street/Namaqua intersection.		X	

## PARKSIDE VILLAGE ADDITION

BEING AN ANNEXATION OF THE NORTH HALF OF SECTION 20, TOWNSHIP 5 NORTH,  
RANGE 69 WEST OF THE 6TH P.M., TO THE CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO



## VICINITY MAP

1" = 1000'

## SURVEY NOTES:

1. No rights-of-way or easements, except those shown hereon, were determined by this survey, nor was any research conducted to determine the existence of additional easements, per the request of the client.
2. This survey does not constitute a title search by the surveyor. Any information regarding record easements, adjoiners, and other documents that might affect the quality of title to this tract of land was obtained from Title Commitment No. 459-H0426929-08TMY, Amendment No. 3, dated April 1, 2016 by Land Title Guarantee Company.
3. Floodplain Note: According to FIRM Map No. 08069CT1169F, December 19, 2006 for Larimer County, this parcel does not lie within a FEMA designated 100-year floodplain.
4. Basis of Bearings: The North line of the Northeast Quarter of Section 20, Township 5 North, Range 69 West of the 6th P.M. as bearing South 89°46'24" East (assumed bearing).
5. Unit of measure is U.S. Survey Feet.

FND #6 REBAR W/3' ALUM. CAP  
LARIMER COUNTY ENGINEERING  
DEPARTMENT, IN MONUMENT BOX

## DESCRIPTION:

The NW 1/4 and part of NE 1/4 lying North of the Home Supply Ditch Company's Reservoir (according to Loesher Survey) all in Section 20, Township 5 North, Range 69 West of the 6th P.M., County of Larimer, State of Colorado.

EXCEPT Book 1804, Page 368: Commencing at a point on North line of said Section 20, a distance of 1264 feet West of the NE corner of said Section, thence South 206.4 feet, thence West 211 feet, thence North 206.4 feet, thence East 211 feet to point of beginning.

ALSO EXCEPT those parcels conveyed by Warranty Deed recorded in Book 1405 at Page 605, Quit Claim Deed recorded in Book 1434 at Page 358 and land described in Contract recorded in Book 1470 at Page 77, and parcel conveyed by Warranty Deed recorded April 9, 1973 in Book 1548 at Page 595, records of Larimer County, Colorado.

ALSO LESS West First Street right of way.

Said parcel of land contains 1,795.947 square feet, or 41.229 acres, more or less (±), and is subject to any rights-of-way or other easements of record or as now existing on said described parcel of land.

Being more particularly described as:

A tract of land located in the Northeast Quarter of Section 20, Township 5 North, Range 69 West of the 6th Principal Meridian, County of Larimer, State of Colorado.

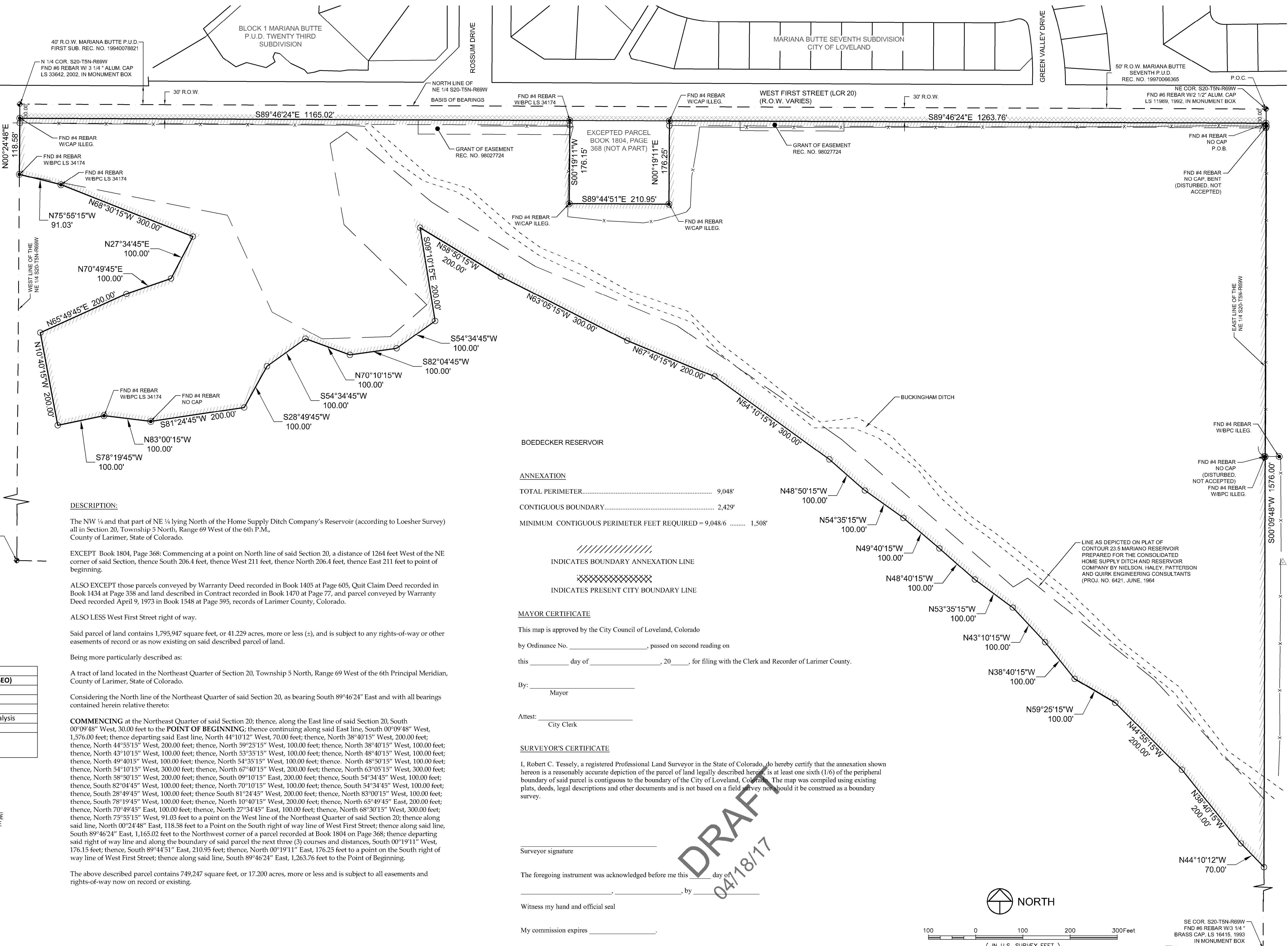
Considering the North line of the Northeast Quarter of said Section 20, as bearing South 89°46'24" East and with all bearings contained herein relative thereto:

COMMENCING at the Northeast Quarter of said Section 20; thence, along the East line of said Section 20, South 00°09'48" West, 1,576.00 feet; thence departing said East line, North 44°10'12" West, 70.00 feet; thence, North 38°40'15" West, 200.00 feet; thence, North 44°55'15" West, 200.00 feet; thence, North 59°25'15" West, 100.00 feet; thence, North 38°40'15" West, 100.00 feet; thence, North 43°10'15" West, 100.00 feet; thence, North 53°35'15" West, 100.00 feet; thence, North 48°40'15" West, 100.00 feet; thence, North 54°10'15" West, 300.00 feet; thence, North 67°40'15" West, 200.00 feet; thence, North 63°03'15" West, 300.00 feet; thence, North 58°50'15" West, 200.00 feet; thence, South 09°10'15" East, 200.00 feet; thence, South 54°34'45" West, 100.00 feet; thence, South 52°04'45" West, 100.00 feet; thence, North 70°10'15" West, 100.00 feet; thence, South 54°34'45" West, 100.00 feet; thence, South 28°49'45" West, 100.00 feet; thence, South 81°24'45" West, 200.00 feet; thence, North 83°00'15" West, 100.00 feet; thence, South 78°19'45" West, 100.00 feet; thence, North 10°40'15" West, 200.00 feet; thence, North 45°49'45" East, 200.00 feet; thence, North 70°49'45" East, 100.00 feet; thence, North 27°34'45" East, 100.00 feet; thence, North 68°30'15" West, 300.00 feet; thence, North 75°55'15" West, 91.03 feet to a point on the West line of the Northeast Quarter of said Section 20; thence along said line, North 00°24'48" East, 118.58 feet to a Point on the South right of way line of West First Street; thence along said line, South 89°46'24" East, 1,165.02 feet to the Northwest corner of a parcel recorded at Book 1804 on Page 368; thence departing said right of way line and along the boundary of said parcel the next three (3) courses and distances, South 00°19'11" West, 176.15 feet; thence, South 89°45'11" East, 210.95 feet; thence, North 00°19'11" East, 176.25 feet to a point on the South right of way line of West First Street; thence along said line, South 89°46'24" East, 1,263.76 feet to the Point of Beginning.

The above described parcel contains 749,247 square feet, or 17.200 acres, more or less and is subject to all easements and rights-of-way now on record or existing.

Line Legend  
— BOUNDARY LINE  
— EASEMENT LINE  
— SECTION LINE  
— FENCE LINE  
— DITCH LINE

Symbol Legend  
● FND CORNER  
◆ FND SECTION CORNER  
▲ CONTROL POINT  
■ BENCHMARK  
○ CALCULATED POSITION





## PDF NARRATIVE

**OVERALL PROJECT PROFILE:** PARKSIDE IS DESIGNED TO BE A LOW-MAINTENANCE OPEN SPACE NEIGHBORHOOD THAT UTILIZES ABUNDANT COMMUNITY OPEN SPACE IN LIEU OF LARGE PRIVATE BACKYARDS. OPEN SPACE WILL BE VERY SENSITIVE TO WATER USE AND WILL EITHER BE LEFT IN A NATURAL STATE OR DEVELOPED WITH NATIVE PLANTS AND ORNAMENTAL VEGETATION WITH MINIMAL TO MODERATE WATER NEEDS. DRY STREAMBEDS, SPACIOUS WALKWAYS AND NATURAL STONE RETAINING WALLS WILL FURTHER LOWER THE WATER BUDGET FOR PARKSIDE.

**THE TARGET BUYER FOR PARKSIDE IS LOOKING FOR A HOME THAT HAS MINIMAL EXTERIOR MAINTENANCE NEEDS, AND A "LOCK AND LEAVE" LIFESTYLE ALLOWING ABUNDANT OPPORTUNITY FOR PERSONAL OR BUSINESS TRAVEL FREE OF THE NEED TO MAINTAIN A LARGE PRIVATE LANDSCAPE. DOWN-SIZING BUYERS WILL ALSO BE ATTRACTED TO A SMALLER BUILDING FOOTPRINT WITH A VERY HIGH QUALITY FINISH LEVEL.**

**DRAMATIC EXTERIOR VIEWS ARE VIEWED TO BE AN IMPORTANT ASPECT OF THE PARKSIDE BUYER PROFILE, SO SPACIOUS OPEN SPACE AND NATURAL BUFFER AREAS ARE PROVIDED.**

THESE ARE SEVERAL LOT TYPES BEING OFFERED, PROVIDING THE COMMUNITY A DIVERSITY THAT IS UNUSUAL FOR A SMALL SITE. PRICING WILL RANGE FROM THE HIGH 300'S TO THE LOW \$700,000 RANGE, DEPENDING ON LOT SIZE, LOT PRODUCT, AND LOT LOCATION WITHIN THE COMMUNITY.

**GENERAL ARCHITECTURAL STANDARDS**  
THE COMMUNITY STANDARD FOR ARCHITECTURE WILL INVOLVE DIVERSITY OF FORM AND TEXTURE, INTEGRITY OF THE STREETSCAPE, INCORPORATION OF NATURAL LANDSCAPING AT AN APPROPRIATE SCALE AND CONTINUITY OF AN OVERALL THEME OF COHESIVE ARCHITECTURAL STANDARDS THAT ENSURE LONG LASTING VALUE TO THE NEIGHBORHOOD AND THE SURROUNDING COMMUNITY.

**IMPORTANCE OF THE WATER BUDGET**  
THE COMMUNITY WILL SEEK TO BE CONSCIOUS OF ITS WATER BUDGET BY ENCOURAGING THE PLANTING OF NATIVE MATERIALS, THE USE OF NATURAL Hardscape MATERIALS AND REQUIRING EFFICIENT IRRIGATION SYSTEMS.

**ACTION BY ACC**  
ALL CONSTRUCTION ACTIVITY ON THE SITE WILL BE MONITORED BY AN ARCHITECTURAL CONTROL COMMITTEE ('ACC') THAT WILL REVIEW AND APPROVE ARCHITECTURAL PLANS FOR HOME CONSTRUCTION OR REMODELING, LANDSCAPING AND IRRIGATION AND FENCES AND OTHER EXTERIOR ELEMENTS VISIBLE FROM ANY INTERIOR OR ADJOINING STREET.

**OWNER'S RESPONSIBILITY**  
A SET OF COVENANTS, CONDITIONS AND RESTRICTIONS WILL BE FILED OF RECORD AND ENFORCED THROUGH A HOMEOWNER'S ASSOCIATION ('HOA') IT WILL BE THE RESPONSIBILITY OF THE OWNER TO MAINTAIN ITS PROPERTY IN A MANNER CONSISTENT WITH COMMUNITY RULES & REGULATIONS AND THE COVENANTS, PROVIDED THAT IF THIS WORK IS NOT PROPERLY PERFORMED BY THE OWNER, THE HOA MAY, AT ITS SOLE OPTION, PERFORM IT AND ASSESS THE OWNER AFTER REASONABLE NOTICE TO CORRECT ANY DEFICIENCY.

**SPECIFIC TYPES OF IMPROVEMENTS/GUIDELINES**  
**ACCESSORY BUILDINGS, STORAGE BUILDINGS:** ACCESSORY BUILDINGS AND STORAGE BUILDINGS SHALL BE SUBMITTED TO THE ACC FOR APPROVAL PRIOR TO CONSTRUCTION. ALL SUCH BUILDINGS SHALL BE CONSISTENT WITH THE ARCHITECTURAL STYLE, MATERIALS AND COLOR OF THE PRIMARY RESIDENCE. ALL ACCESSORY BUILDINGS AND STORAGE BUILDINGS SHALL BE LOCATED TO THE REAR OF THE PRIMARY RESIDENCE. DETACHED "CASITAS" ARE ALLOWED IF THE INTERIOR AND EXTERIOR ARE FINISHED TO THE STANDARDS OF THE MAIN DWELLING.

**AIR CONDITIONING AND HEATING EQUIPMENT/EVAPORATIVE COOLERS/ATTIC VENTILATORS:** NO HEATING, AIR CONDITIONING, AIR MOVEMENT OR REFRIGERATION EQUIPMENT MAY BE PLACED OR INSTALLED ON ROOFTOPS OR EXTENDED FROM WINDOWS. GROUND MOUNTED AIR CONDITIONING EQUIPMENT INSTALLED IN THE SIDE YARD SHALL BE INSTALLED IN A MANNER SO AS TO MINIMIZE VISIBILITY FROM THE STREET AND TO MINIMIZE ANY NOISE TO ADJACENT PROPERTY OWNERS. SWAMP COOLERS ARE NOT ALLOWED UNDER ANY CIRCUMSTANCES.

**ASTROTURF:** ASTROTURF IS NOT ALLOWED IN LIEU OF ANY REQUIRED FRONT YARD SOD REQUIREMENTS.

**BOATS:** BOATS AND BOAT TRAILERS SHALL NOT BE PARKED ON THE STREETS OF THE PROPERTY OR STORED ON ANY LOT UNLESS FULLY CONTAINED WITHIN A GARAGE. NOTWITHSTANDING THE FOREGOING, RECREATIONAL VEHICLES/CAMPERS MAY BE PARKED UPON THE DRIVEWAY OF THE LOT OR UPON THE STREET IMMEDIATELY ADJACENT TO THE LOT FOR A PERIOD NOT TO EXCEED 24 CONTINUOUS HOURS AT A TIME. NO SUCH PARKING SHALL BE ALLOWED WHICH EXCEEDS MORE THAN A TOTAL OF FIVE DAYS DURING A 30 DAY PERIOD. NO VEHICLES OR CAMPERS SHALL BE PARKED IN A DRIVEWAY IN A WAY THAT BLOCKS THE CITY SIDEWALK.

**CARPETS:** FREE STANDING CARPETS ARE NOT ALLOWED.

**FIRE PITS/ OUTDOOR FIREPLACES ETC.:** APPROVAL IS REQUIRED FOR ALL PERMANENT INSTALLATIONS. GAS-FIRED FIRE PITS AND FIREPLACES WILL BE THE ONLY OPTION, WITH NO OUTSIDE WOOD BURNING.

**DECKS & BALCONIES:** APPROVAL WILL BE REQUIRED FOR ALL DECKS. THE DECK MUST BE CONSTRUCTED OF MATERIAL THAT WOULD BE CONSIDERED SIMILAR TO THE MATERIAL OF THE RESIDENCE OR MAINTENANCE-FREE TYPE DECKING PRODUCTS AS MAY BE APPROVED AND BE LOCATED SO AS NOT TO OBSTRUCT OR SUBSTANTIALLY DIMINISH THE VIEW OR CREATE AN UNREASONABLE LEVEL OF NOISE FOR ADJACENT OWNERS. MAINTENANCE-FREE DECKING PRODUCTS MAY BE USED PROVIDED THE DECKING IS LOCATED ON THE REAR, SIDE OR REAR ELEVATION OF A BUILDING AND MUST BE OF A COLOR THAT COMPLEMENTS EITHER THE EXISTING BODY OR TRIM COLOR OF THE HOME. DECKS MAY NOT COVER MORE THAN 50% OF THE REAR YARD OF THE HOME. CONSTRUCTION SHALL NOT OCCUR OVER EASEMENTS BEYOND THE SIDE PLANE OF THE HOME AND MUST BE SET BACK A MINIMUM OF FIVE (5) FEET FROM THE PROPERTY LINE. THE DECK MUST BE INSTALLED AS AN INTEGRAL PART OF THE HOME AND/OR PATIO AREA.

**DOG HOUSES & DOG RUNS:** DOG HOUSE(S) AND DOG RUN(S) WILL REQUIRE PLAN SUBMITTAL TO THE ACC PRIOR TO INSTALLATION/CONSTRUCTION. DOG HOUSES AND/OR DOG RUNS MUST NOT PRESENT A VISUAL OR NOISE NUISANCE TO THE STREET AND NEIGHBORING YARDS, MUST BE INSTALLED AT GROUND LEVEL, AND SHALL NOT BE VISIBLE ABOVE THE FENCE. METHODS OF CONCEALMENT INCLUDE LANDSCAPING OR OTHER METHODS APPROVED BY THE ACC. ALL DOG RUNS SHALL BE FLOOR-DRAINED AND SHALL NOT DISCHARGE URINE OR DILOD WASTE ONTO ABUTTING PROPERTIES. DOG RUNS SHALL BE LIMITED TO ONE HUNDRED (100) SQUARE FEET AND FENCING SHALL NOT BE HIGHER THAN FOUR FEET (FOUR FEET). TARPULINS AND CHAIN-LINK FENCING SHALL NOT BE PERMITTED.

**DRAINAGE/GRADING & GRADE CHANGES:** THERE BE NO INTERFERENCE WITH THE ESTABLISHED DRAINAGE PATTERN OVER ANY PROPERTY. THE "ESTABLISHED DRAINAGE PATTERN" MEANS THE DRAINAGE PATTERN AS ENGINEERED AND CONSTRUCTED BY THE ORIGINAL DEVELOPER OR HOMEBUILDER PRIOR (OR IN SOME CASES, IMMEDIATELY FOLLOWING) CONVEYANCE OF TITLE FROM THE HOMEBUILDER TO THE INDIVIDUAL OWNER. LANDSCAPING SHOULD CONFORM TO THE ESTABLISHED DRAINAGE PATTERN. SUM PUMP DRAINAGE SHOULD BE VENTED AT LEAST SIX FEET (6') FROM THE PROPERTY LINE, ON THE OWNER'S PROPERTY, TO ALLOW FOR ABSORPTION.

**DRIVEWAYS:** THERE SHALL BE NO EXTENSION OR EXPANSION OF DRIVEWAYS WITHOUT PRIOR ACC APPROVAL. DRIVEWAYS TO THE GARAGE SHALL BE CONSTRUCTED ONLY WITH NEUTRAL TONE CONCRETE OR STONE PAVERS. IF DRIVEWAY EXTENSION OR EXPANSION IS DESIRED, DRAWINGS SHALL BE SUBMITTED TO THE ACC SHOWING THE DESIRED CHANGES/ADDITIONS. ANY APPROVED DRIVEWAY EXPANSION SHALL NOT BE INTENDED TO PROMOTE OR FACILITATE THE PARKING OR STORAGE OF ANY VEHICLE ON THE DRIVEWAY OR SIDE YARD.

**ELEVATIONS:** ELEVATIONS SHOULD BE SIMPLE, BUT INTERESTING, WITH VARYING ROOF PLANES THAT PROVIDE DIVERSITY TO THE STREETSCAPE. THE FRONT SETBACK WILL BE VARIED TO ALLOW THE MAIN STRUCTURE OF THE HOUSE AND/OR FRONT COURTYARDS AND PORCHES TO PROJECT FORWARD OF GARAGE DOORS. CONTEMPORARY OR MEDITERRANEAN STYLES ARE DISCOURAGED.

FOR THIS REASON, A 15 FOOT FRONT YARD SETBACK FOR THE MAIN STRUCTURE IS PROVIDED SO THAT THE VIEW FROM THE STREET WOULD NOT BE DOMINATED BY GARAGE DOORS. GARAGE DOOR OPENINGS WOULD BE SET BACK A MINIMUM OF 20 FEET FROM THE PROPERTY LINE.

VARYING ROOF PLANES AS WELL AS THE PROJECTION OF THE FRONT ELEVATION CLOSER TO THE STREET THAN THE GARAGE DOORS, WILL BE REQUIRED. IN ADDITION, THE FRONT YARD STREETSCAPE WILL REQUIRE AT LEAST TWO EVERGREEN SHRUBS TO SOFTEN THE DORMANT APPEARANCE OF WINTER LANDSCAPES.

ROOF MATERIAL WILL BE WEAVERED WOOD IN APPEARANCE AND WILL AVOID BRIGHT COLORS OR SOLID WHITE OR BLACK. EXAMPLES OF MULTIPLE STREETSCAPE ELEMENTS THAT ARE A PART OF THE DESIRED ELEVATION PALETTE INCLUDE:

1. VARIED ROOF ANGLES
2. FRONT PORCH PROJECTING FORWARD OF THE GARAGE DOORS.
3. FRONT COURTYARD PROJECTING FORWARD OF THE GARAGE DOORS.
4. STONE OR BRICK PLACEMENT ON THE FAÇADE AT LEAST UP TO THE SILL LEVEL OF THE PREDOMINANT WINDOW TREATMENT.

HOMES THAT ADJOIN TRACT "D" WILL BE LIMITED TO SINGLE STORY OR STORY AND A HALF AT THE FRONT ELEVATION, AS ALL OF THESE HOMES WILL MOST LIKELY HAVE WALK-OUT BASEMENTS. TRACT D PROVIDES A 200 FOOT BUFFER FROM THE EDGE OF BOEDECKER LAKE.

LOTS 31 AND 32, BLOCK 2 WILL BE LIMITED TO SINGLE STORY ELEVATIONS DUE TO THEIR PROXIMITY TO THE MCKRIMMON OUTPARCEL.

DARK SKY LIGHTING FIXTURES WILL BE REQUIRED ON ALL EXTERIOR FAÇADES AND INTERIOR COURTYARDS.

FRONT PORCHES AND COURTYARDS ARE ENCOURAGED AND WILL BE CONSIDERED A PART OF THE FRONT ELEVATION WHEN CALCULATING A MAXIMUM OF 50% OF THE FRONT ELEVATION USED FOR GARAGE DOORS.

GARAGE DOORS WHICH FACE THE STREET WILL HAVE A DECORATIVE WINDOW TREATMENT IN THE TOP PANEL.

FRONT ELEVATIONS WILL HAVE STONE, CULTURED STONE OR BRICK PLACED AT A MINIMUM TO THE HEIGHT OF THE PREDOMINANT GROUND FLOOR WINDOW SILL. STONE, CULTURED STONE OR BRICK WILL BE WRAPPED AROUND ALL FRONT HOUSE CORNERS A MINIMUM OF FOUR FEET TO A HEIGHT MATCHING THE FRONT ELEVATION. FRONT FACING WINDOWS WILL BE DIVIDED LIGHT AND NO SINGLE PANE WINDOWS ARE PERMITTED.

**ENERGY DEVICES:**

1. SOLAR: APPROVAL IS REQUIRED BY THE ACC. NON-GLARE OR NON-REFLECTIVE FINISHES ARE PREFERRED. INSTALLATIONS BEING MADE FOR ENERGY-EFFICIENT MEASURES MAY BE FURTHER DEFINED BY STATE OR FEDERAL REGULATIONS. DEVICES MUST BE DESIGNED TO APPEAR AS IF THEY ARE AN INTEGRAL PART OF THE ROOF. EVERY ATTEMPT SHALL BE MADE TO MOUNT SUCH DEVICES ON THE BACK SIDE OF ROOF IF POSSIBLE, TO MINIMIZE VISIBILITY FROM FRONT OF RESIDENCE. BLOCK OUT SCREENS ARE REQUIRED FOR APPROVAL (I.E. ROOFTOP SOLAR PANELS)
2. ELECTRIC AUTOMOBILE/HOUSE CHARGES/STORAGE BATTERIES: MUST BE LOCKED WITHIN THE HOME.
3. GENERATORS: APPROVAL IS REQUIRED FOR ALL TYPES OF STAND-BY POWER GENERATORS.

**EVAPORATIVE COOLERS:** EVAPORATIVE COOLERS ARE NOT ALLOWED.

**FENCES:** FENCING IS ALLOWED ONLY IN ACCORDANCE WITH THE COMMUNITY STANDARD, WHICH, EXCEPT WHERE LOTS BACK UP TO 1ST STREET, IS A MAXIMUM HEIGHT OF 4' (NOT INCLUDING COLUMNS) WITH THREE RAILS.

THE COMMUNITY FENCE MUST BE INSTALLED IN ANY PORTION OF THE LOT WHICH ADJOINS COMMON AREA, PER THE APPROVED FENCING PLAN FOR PARKSIDE. FENCES MUST BE MAINTAINED ON BOTH SIDES BY THE HOMEOWNER, EXCEPT FOR ANY FENCE ADJOINING 1ST STREET, WHICH WILL BE MAINTAINED BY THE HOMEOWNER'S ASSOCIATION.

FENCES REQUIRE ACC APPROVAL PRIOR TO INSTALLATION. DRAWINGS SHOWING FENCE LOCATION, LAYOUT, DESIGN, HEIGHT AND MATERIAL SHALL BE SUBMITTED TO THE ACC. NO FENCING WILL BE ALLOWED ANYWHERE WITHIN THE FRONT YARD OF RESIDENCE UNLESS ADJOINING A COMMON AREA, UPON REVIEW AND APPROVAL BY THE ACC. INSTALLATION OF WIRE MESH TO THE THREE RAIL FENCE MAY BE ALLOWED FOR PET CONTAINMENT PURPOSES. FENCING MAY EXTEND FROM THE REAR YARD ALONG EACH SIDE LOT LINE OF THE RESIDENCE TO NO MORE THAN 2' BEYOND THE FRONT ELEVATION OF THE RESIDENCE ON BOTH SIDES, EXCEPT WHERE THE SIDE LOT LINE ADJOINS COMMON AREA, IN WHICH CASE THE FENCE MAY EXTEND TO THE FRONT PROPERTY LINE. NOTWITHSTANDING THE FOREGOING, IN THOSE INSTANCE WHERE A HOME HAS AN EXTERIOR DOOR ON THE SIDE OF A GARAGE, FENCING MAY EXTEND UP TO 2' BEYOND THE FRONT EDGE OF SAID EXTERIOR DOOR (BUT IN NO EVENT BEYOND THE FRONT EDGE OF THE HOUSE). OWNERS ARE ADVISED THAT VARIOUS UTILITY COMPANIES AND OTHER SERVICE VENDORS MAY HAVE THE RIGHT TO ACCESS THE LOT BECAUSE OF ESTABLISHED EASEMENTS (INCLUDING REMOVAL OF FENCE IF NECESSARY TO RESTORE).

LOTS THAT BACK UP TO 1ST STREET ARE ALLOWED A 6 FOOT HIGH CEDAR FENCE ALONG THE REAR BOUNDARY LINE. SUCH FENCES SHALL BE STAINED TO THE COMMUNITY STANDARD AND SHALL HAVE A BORDER TRIM AT THE TOP. SIDE YARDS FOR THESE LOTS SHALL HAVE THE COMMUNITY STANDARD FENCE (4 FOOT TALL WITH THREE RAILS), WITH A "STEPDOWN" TRANSITION ALLOWED WITH PRIOR APPROVAL OF THE ACC.

**FIREWOOD/WOOD STORAGE:** IS NOT PERMITTED.

**FLAGPOLES:** FREESTANDING FLAGPOLES ARE NOT ALLOWED. ONE WALL MOUNTED FLAG BRACKET PER RESIDENCE SHALL BE ALLOWED.

**GARDENS:** VEGETABLE GARDENS SHALL BE ALLOWED ONLY WITHIN THE HOMEOWNER'S BACK YARD, AND NOT ON THE SIDES OF THE HOME OR IN THE FRONT YARD. FLOWER BEDS (ESPECIALLY ANNUALS), VEGETABLE GARDENS AND OTHER LANDSCAPING WHICH REQUIRES REGULAR WATERING SHOULD NOT BE PLANTED WITHIN FIVE (5) FEET OF THE FOUNDATION WALL OR SLAB AND SHOULD AVOID EXCESSIVE WATERING. FURTHER, PIPING AND HEADS FOR SPRINKLER SYSTEMS SHOULD NOT BE INSTALLED WITHIN FIVE (5) FEET OF FOUNDATION WALLS AND SLABS. ALL FLOWER GARDENS MUST BE WEEDED, CARE FOR AND CAREFULLY MAINTAINED.

**GAZEBOS:** APPROVAL IS REQUIRED BY THE ACC. GAZEBOS MUST BE SIMILAR IN MATERIAL AND DESIGN TO THE RESIDENCE, MUST BE PAINTED OR STAINED THE BODY OR TRIM COLOR OF THE RESIDENCE AND MUST COMPLY WITH ALL EXISTING SET-BACK REQUIREMENTS. ROOF MATERIAL MUST MATCH THAT OF HOME, IF APPLICABLE.

**GREENHOUSES:** GREENHOUSES REQUIRE PRIOR ACC APPROVAL.

**HARD SURFACES, INCLUDING DRIVEWAYS, PATIOS, FRONT PORCHES, STAIRWAYS, PADS AND OWNER-OWNED WALKWAYS:** OWNERS ARE RESPONSIBLE FOR ENSURING THE PROPER CONDITION OF HARD SURFACES ON THEIR PROPERTY, INCLUDING DRIVEWAYS AND SIDEWALKS (SUCH THAT SURFACES ARE INTACT AND FREE OF EXCESSIVE CRACKS, CRUMBLING, AND SETTLING). APPROVAL IS REQUIRED FOR ANY REVISIONS OR ADDITIONS TO HARD SURFACES:

1. APPROVED REPLACEMENT OR REPAIR WILL BE REQUIRED IF ANY DISPLACEMENTS, CRACKS OR PORTION OF A CRACK ARE MORE THAN 6" LONG AND IS 1/2" WIDE OR GREATER.
2. APPROVED REPLACEMENT OR REPAIR WILL BE REQUIRED IF ANY DISPLACEMENTS OR CRACK THAT RESULT IN AN UNLEVEL SURFACE OF 1/2" OR MORE ON EITHER SIDE OF THE CRACK OR NEXT TO A GARAGE SLAB.
3. APPROVED REPLACEMENT OR REPAIR WILL BE REQUIRED IF ANY SURFACE CRUMBLING/SPALLING/PITTING OCCURS, REGARDLESS OF REASON, THAT COVERS MORE THAN 50% OF THE SURFACE AREA OF THE PARTICULAR HARD SURFACE.

**HOT TUBS/SPA TUBS:** HOT TUBS/SPA TUBS SHALL REQUIRE ACC PRIOR APPROVAL. HOT TUBS/SPA TUBS SHOULD BE AN INTEGRAL PART OF THE DECK OR PATIO AREA AND OF THE REAR YARD LANDSCAPING. ALSO, HOT TUBS/SPA TUBS MUST BE INSTALLED SO AS TO NOT BE IMMEDIATELY VISIBLE FROM THE FRONT YARD/STREET. HOT TUBS/SPA TUBS SHALL BE INSTALLED IN SUCH A MANNER THAT THEY WILL NOT CAUSE NOISE DISTURBANCE FOR ADJACENT PROPERTY OWNERS.

**HOME BUSINESS/OFFICE:** HOME BUSINESS/OFFICES ARE ALLOWED IF THEY ARE NOT VISIBLE AND DO NOT DISRUPT THEIR NEIGHBORS WITH SIGNAGE, TRAFFIC OR DELIVERIES, AS SET FORTH IN THE COVENANTS.

**HOUSEHOLD PETS:** NO ANIMALS, LIVESTOCK, BIRDS, POULTRY, REPTILES OR INSECTS OF ANY KIND SHALL BE RAISED, BRED, KEPT OR BOARDED IN THE PROPERTY; PROVIDED, HOWEVER, THAT THE OWNERS AND RESIDENTS OF EACH LOT MAY KEEP A REASONABLE NUMBER OF BONA FIDE HOUSEHOLD PETS (INCLUDING DOGS, CATS OR OTHER DOMESTIC ANIMALS), SO LONG AS SUCH PETS ARE NOT KEPT FOR ANY COMMERCIAL PURPOSE, INCLUDING BREEDING AND ARE NOT KEPT IN SUCH NUMBER OR IN SUCH MANNER AS TO CREATE A NUISANCE TO ANY RESIDENT OF THE PROPERTY.

**LANDSCAPING:** LANDSCAPING PLANS MUST BE APPROVED BY THE ACC PRIOR TO INSTALLATION. ONCE THE LANDSCAPING PLAN HAS BEEN APPROVED, NO ALTERATIONS CAN BE MADE UNLESS APPROVED BY THE ACC. PLANS SHALL DEPICT TYPES AND LOCATIONS OF: FENCING, DECKS, PLAYING AREAS, SOD, SEEDED AREAS, EDGING, RETAINING WALLS, ROCK AREAS, SPRINKLER SYSTEMS, FLOWER BEDS/GARDENS, OTHER PLANTING BEDS, MULCH AREAS, LANDSCAPE LIGHTING AND ANY OTHER COMPONENTS OF THE PROPOSED PLAN. OWNERS OF A LOT MUST MAINTAIN THE BUILDER INSTALLED TREE(S) SHOWN ON THE LANDSCAPE PLAN FOR PARKSIDE. OWNERS OF A LOT ADJACENT TO A STREET TREE LAWN ARE REQUIRED TO PLANT AND MAINTAIN A TREE OR TREES OF THE SAME SIZE AND SPECIES AS THE BUILDER INSTALLED TREE(S) ON THE FRONT YARD LAWN ADJACENT TO THE STREET. THE ACC MAY REQUIRE ADDITIONAL INFORMATION (INCLUDING, BUT NOT LIMITED TO, THE SIZE, AGE, AND MATERIAL) OF THE SPRINKLER SYSTEM. LOT OWNERS ARE RESPONSIBLE FOR REPLACING DISEASED TREES. NO MORE THAN 40% OF THE FRONT YARD LANDSCAPING SHALL BE NON-LIVING MATERIALS, UNLESS OTHERWISE APPROVED BY THE ACC. EACH OWNER SHALL INSTALL LANDSCAPING ON THEIR LOT WITHIN ONE HUNDRED EIGHTY (180) DAYS AFTER ACQUISITION OF SUCH LOT BY SUCH OWNER IF SAID ACQUISITION OCCURS BETWEEN APRIL 1 AND OCTOBER 1. THE ACC MAY REQUEST ADDITIONAL INFORMATION AND/OR RESUBMITTAL, AS IT DEEMS APPROPRIATE. SPRINKLER SYSTEMS MUST BE INSTALLED TO IRRIGATE LANDSCAPING IN THE FRONT YARD. WEEDS ON ALL LOTS (INCLUDING, WITHOUT LIMITATION, LOTS OWNED BY BUILDERS) SHALL BE KEPT MOWED/CUT WEEDS MUST BE MOWED/CUT WHEN THEY EXCEED 4 INCHES IN HEIGHT.

**LATTICE WORK:** LATTICE WORK SHALL REQUIRE PRIOR ACC APPROVAL. ANY LATTICE WORK SHALL BE INCORPORATED WITHIN THE "GENERAL" LANDSCAPING THEME AND MUST BE MAINTAINED IN GOOD CONDITION AND APPEARANCE. LATTICE MAY NOT BE ATTACHED TO ANY FENCING. LATTICE WORK IS PERMITTED ON REAR DECKS OR TRELLISES BUT SHALL NOT BE PERMITTED ON THE FRONT ELEVATION OF THE LOT/HOME.

**LIGHTS AND LIGHTING:** LIGHTS AND LIGHTING SHALL BE OF CONVENTIONAL STYLE WITH ILLUMINATION PATTERNS WHICH DO NOT CAUSE A NUISANCE TO NEIGHBORING PROPERTIES. ALL EXTERIOR LIGHTING SHALL BE A "DARK SKY" STANDARD AND NO FLOODLIGHTS MAY BE UTILIZED. LIGHT/LIGHTING FOR USE IN LANDSCAPING SHALL REQUIRE PRIOR ACC APPROVAL.

**ORNAMENTS:** ORNAMENTS, SUCH AS FOUNTAINS, STATUARY, ARTIFICIAL PLANTS, WAGON WHEELS OR EQUIPMENT, OR OTHER ORNAMENTAL FEATURES COMMONLY KNOWN AS "YARD ART" SHALL NOT BE PLACED OR ALLOWED TO REMAIN WHERE VISIBLE FROM STREETS UNLESS THE SAME HAVE BEEN APPROVED BY THE ACC. THE ACC MAY REQUIRE REMOVAL OF ANY ORNAMENTAL FEATURE PLACED IN THE FRONT YARD WHICH IS NOT IN KEEPING WITH COMMUNITY STANDARDS FOR GOOD TASTE AND ARCHITECTURAL INTEGRITY.

**PAINTING:** EXTERIOR PAINTING OF RESIDENCES WILL BE KEPT IN GOOD CONDITION AND IN THE COLOR APPROVED WITH THE ORIGINAL CONSTRUCTION SUBMITTAL. COLOR CHANGES THEREAFTER MUST BE PRIOR APPROVED BY THE ACC. NO TWO ADJOINING HOUSES MAY HAVE THE IDENTICAL PAINT COLOR SCHEME. COLORS SHOULD BE PREDOMINATELY EARTH TONES.

**PATIO COVERINGS:** PATIO COVERINGS MUST BE APPROVED BY ACC. THEY MUST BE CONSTRUCTED OF WOOD OR MATERIALS GENERALLY RECOGNIZED AS COMPLEMENTARY TO THE RESIDENCE AND MUST BE SIMILAR OR GENERALLY RECOGNIZED AS COMPLEMENTARY IN COLOR TO THE PRIMARY RESIDENCE COLOR.

**PAVING:** PAVING CAN BE PERFORMED WITH A VARIETY OF MATERIALS AND USED TO ENHANCE THE RESIDENT LANDSCAPING OR TO CREATE PATIO AREAS, WALKWAYS, STOOPS, PORCHES, LANDSCAPING TRIM, ETC. APPROVED PAVING MATERIALS ARE AS FOLLOWS: NEUTRAL TONE CONCRETE, FLAGSTONE, PRE-CAST PATTERNED OR EXPOSED AGGREGATE CONCRETE PAVERS, AND COLORED OR NATURAL ROCK. ALL MATERIALS MUST BE INSTALLED LOCATED SO AS NOT TO BLOCK ANY EXISTING DRAINAGE PATTERN OF THE LOT. ALL PAVING WILL REQUIRE PRIOR ACC APPROVAL.

**PLAY AND SPORTS EQUIPMENT:** PLAY AND SPORTS EQUIPMENT REQUIRES PRIOR ACC APPROVAL. IN GENERAL, ALL PLAY EQUIPMENT, TRAMPOLINES AND OTHER SPORTS EQUIPMENT SHALL ONLY BE ALLOWED IN THE REAR OF A RESIDENCE AND THE ACC MAY REQUIRE REASONABLE SCREENING FROM THE STREET.

**PLAYHOUSES:** PLAYHOUSES REQUIRE PRIOR ACC APPROVAL. PLAYHOUSES SHALL NOT EXCEED 8' IN HEIGHT AT THE PEAK, SHALL HAVE NO MORE THAN 100 SQUARE FEET OF INTERIOR FLOOR SPACE, AND SHALL BE LOCATED IN THE REAR YARD OF THE RESIDENCE. BASIC DESIGN, MATERIALS, COLORS AND ROOF MATERIALS MUST MATCH THE RESIDENCE. PLAYHOUSES ARE TO BE INCORPORATED INTO, AND AT LEAST PARTIALLY SCREENED BY, LANDSCAPING AND SHOULD NOT UNREASONABLY OBSTRUCT ADJACENT NEIGHBOR'S CORRIDOR VIEWS.

**POOLS:** ALL SWIMMING POOLS SHALL REQUIRE PRIOR ACC APPROVAL. MOVEABLE SMALL CHILDREN TYPE (12' IN DIAMETER OR LESS AND 18" IN DEPTH OR LESS) SWIMMING POOLS NEED NOT RECEIVE ACC APPROVAL. SWIMMING POOLS MAY BE LOCATED ONLY IN THE REAR OF THE RESIDENCE AND MUST BE AN INTEGRAL PART OF THE DECK OR PATIO AREA. THEY SHOULD BE LOCATED IN SUCH A WAY THAT THEY ARE NOT IMMEDIATELY VISIBLE TO ADJACENT PROPERTY OWNERS.

**RECREATIONAL VEHICLES/ CAMPERS:** RECREATIONAL VEHICLES AND/OR CAMPERS SHALL NOT BE PARKED ON THE STREETS OF THE PROPERTY OR STORED ON ANY LOT UNLESS FULLY CONTAINED WITHIN A GARAGE. NOTWITHSTANDING THE FOREGOING, RECREATIONAL VEHICLES/CAMPERS MAY BE PARKED UPON THE DRIVEWAY OF THE LOT OR UPON THE STREET IMMEDIATELY ADJACENT TO THE LOT FOR A PERIOD NOT TO EXCEED 48 CONTINUOUS HOURS AT A TIME, AND NO SUCH PARKING SHALL BE ALLOWED WHICH EXCEEDS MORE THAN A TOTAL OF SIX DAYS DURING A 30 DAY PERIOD.

**RETAINING WALLS:** RETAINING WALLS REQUIRE PRIOR APPROVAL OF ACC. RETAINING WALLS MAY BE USED TO ACCOMMODATE OR CREATE ABRUPT CHANGES IN GRADE. SUCH WALLS SHOULD BE PROPERLY ANCHORED TO WITHSTAND OVERTURNING FORCES. STONE WALLS SHOULD BE MADE THICKER AT THE BOTTOM THAN AT THE TOP TO ACHIEVE STABILITY. TO AVOID DESTRUCTIVE FREEZE-THAW ACTION, ALL RETAINING WALLS SHOULD INCORPORATE WEEP HOLES INTO THE WALL DESIGN TO PERMIT WATER TRAPPED BEHIND THEM TO BE RELEASED. WALLS SHALL NOT BE LOCATED SO AS TO ALTER THE EXISTING DRAINAGE PATTERNS, AND SHALL PROVIDE FOR ADEQUATE DRAINAGE OVER OR THROUGH (BY MEANS OF WEEP HOLES) THE WALL STRUCTURE.

**ROOFING:** THE STANDARD ROOFING WILL BE A "WEATHERED WOOD" ASPHALT WITH A MINIMUM MANUFACTURER'S WARRANTY OF 25 YEARS OR MORE. ALL ROOFING MATERIALS SHALL BE A NEUTRAL, EARTH TONE COLOR EXCEPT THAT NO ALL-WHITE OR DARK BLACK ROOFING WILL BE ALLOWED.

**ROOF TOP EQUIPMENT:** NO ROOF TOP EQUIPMENT IS ALLOWED (SEE ALSO "AIR CONDITIONING EQUIPMENT" AND "ANTENNAE"). LIGHTING ROD(S) ARE PERMISSIBLE. DECORATIVE ROOF MOUNTED WEATHER VANES REQUIRE PRIOR ACC APPROVAL.

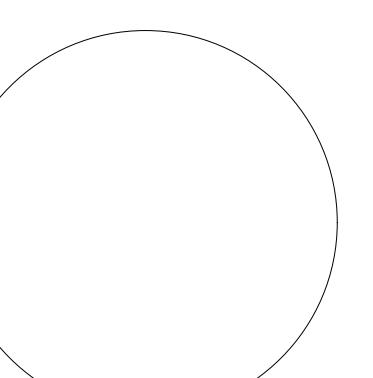
**SATELLITE DISHES:** MICROWAVE AND/OR SATELLITE TELEVISION DISHES ARE PERMITTED AS LONG AS THEY ARE MOUNTED OR PLACED ON THE SIDE OR REAR OF RESIDENCE WITH PRIOR APPROVAL BY ACC AND ARE APPROPRIATELY SCREENED, OR ARCHITECTURALLY INTEGRATED INTO RESIDENCE

**SETBACKS:** THE FOLLOWING ARE THE FRONT, SIDE AND REAR YARD SETBACKS FOR LOTS WITHIN PARKSIDE:

1. FRONT YARD SETBACKS SHALL BE 15 FEET FOR THE MAIN STRUCTURE AND 20' FOR GARAGE DOORS. ENCLOSED PATIOS, FRONT PORCHES AND COURTYARDS SHALL ALL BE CONSIDERED AS MAIN STRUCTURE ELEMENTS.
2. SIDE YARD SETBACK WILL BE FIVE (5) FEET, WITH A MINIMUM SEPARATION OF TEN (10) FEET BETWEEN DWELLINGS.
3. REAR YARD SETBACK WILL BE 15 FEET, EXCEPT FOR LOTS THAT ADJOIN COMMON AREA IN THE REAR, IN WHICH CASE THE REAR YARD SETBACK WILL BE 10 FEET.

**STREET TREES:** STREET TREE REQUIREMENTS SHALL BE THE PLANS AND SPECIFICATIONS PREPARED BY TB GROUP, A LANDSCAPE ARCHITECTURAL FIRM LOCATED AT 444 MOUNTAIN AVENUE, BERTHOUD, COLORADO 80513.

1. THE OVERALL SITE AND LANDSCAPE PLAN FOR PARKSIDE IS PROVIDED WITHIN THE APPROVAL DOCUMENTS AS REQUIRED BY THE PRELIMINARY AND FINAL DEVELOPMENT PLANS FOR THE COMMUNITY. IN ACCORDANCE WITH SUCH PLAN, THERE ARE TREES REQUIRED TO BE INSTALLED BY THE DEVELOPER IN COMMON AREAS OF PARKSIDE, AND ADDITIONAL TREES TO BE INSTALLED BY THE CONTRACTOR IN THE STREET RIGHT OF WAY ADJACENT TO CONTRACTOR LOTS PRIOR TO THE CERTIFICATE OF OCCUPANCY FOR ANY RESIDENCE. DUE TO THE FRONT YARD DEPTH AND THE SIZE OF LOTS IN PARKSIDE, ONLY ORNAMENTAL TREES WILL BE ALLOW



# PARKSIDE FIRST SUBDIVISION

LOVELAND, CO

Boedecker Partners,  
LLC.

1021 NIGHTINGALE DRIVE  
FORT COLLINS, CO. 80525  
(970) 685-1575  
CONTACT: KENNETH MITCHELL

REVISIONS DATE  
Staff Comments 4.18.17

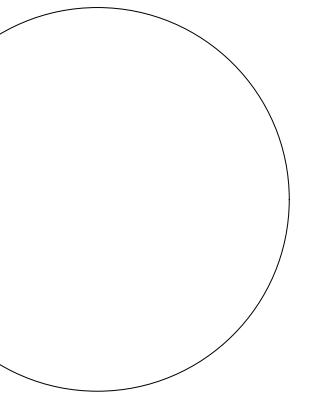
NOVEMBER 21, 2016

# Site Plan

## SHEET INFORMATION

Sheet Number: **3**

## ATTACHMENT E

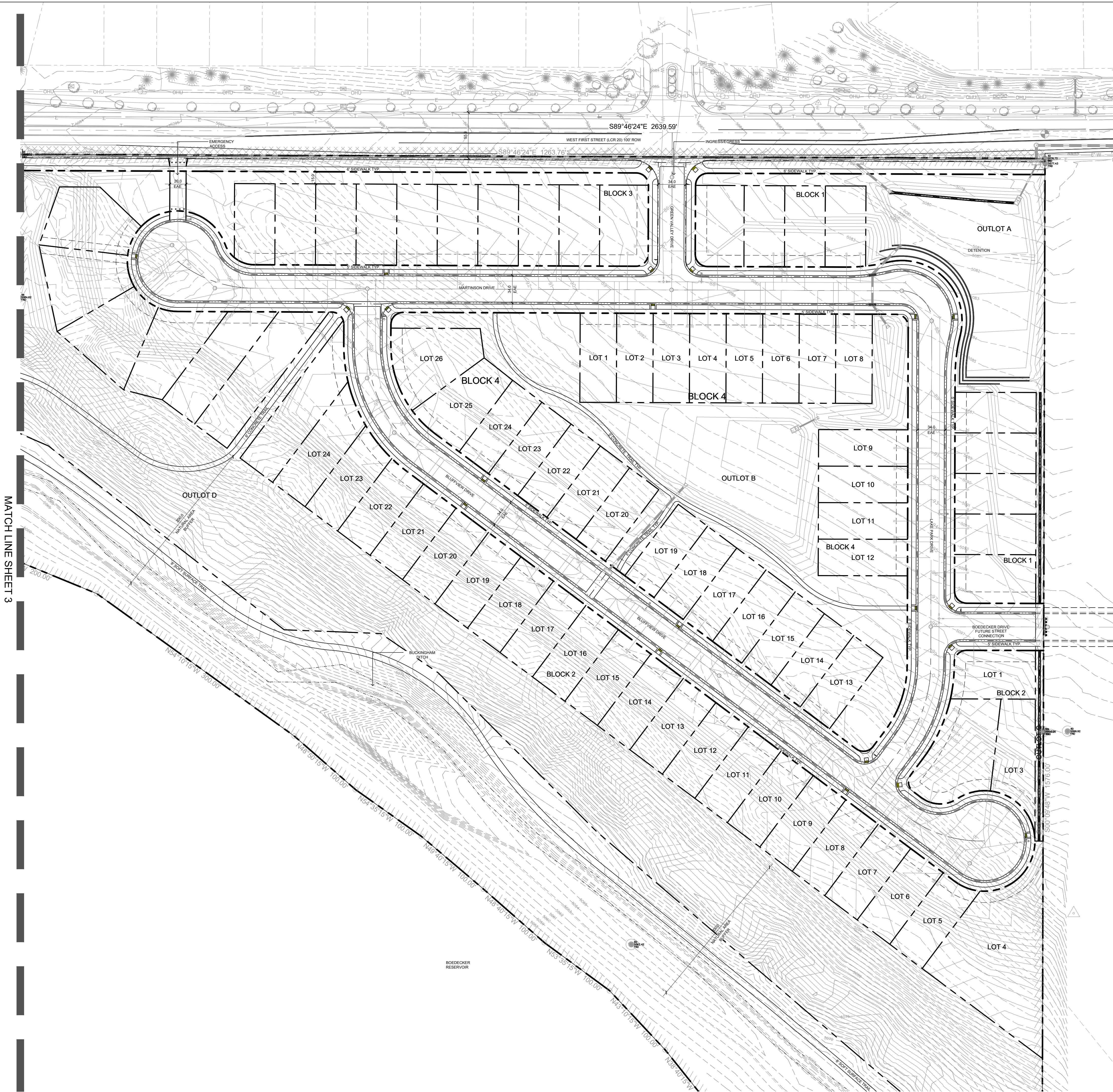


**PARKSIDE  
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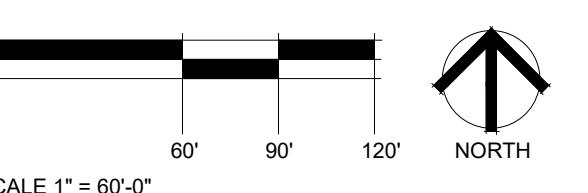
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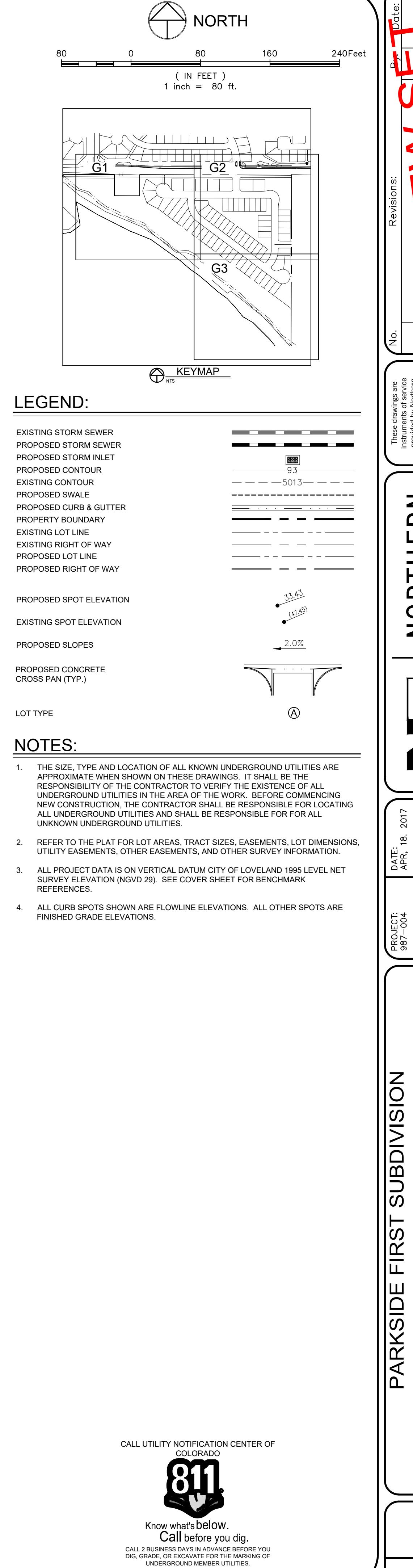
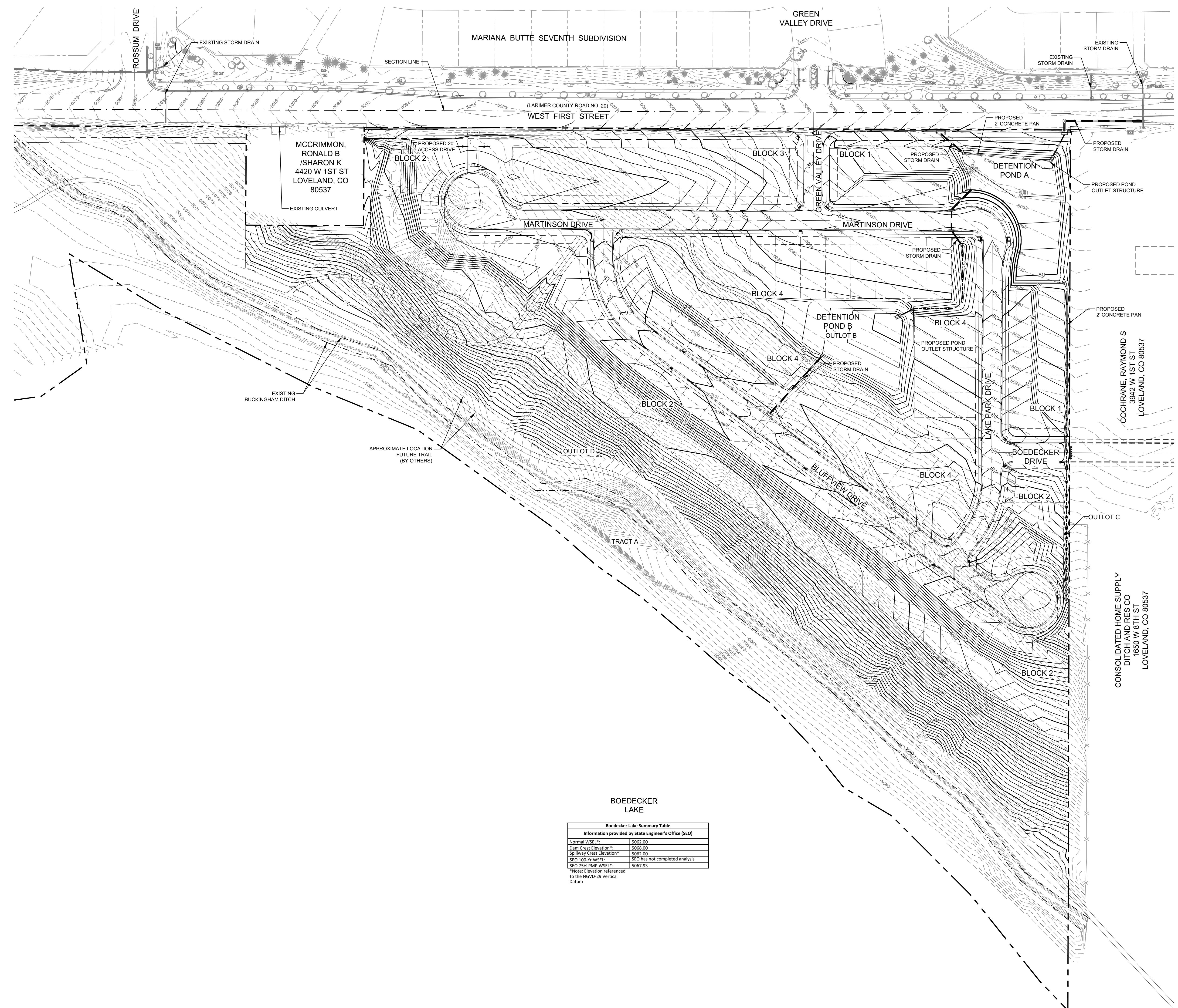
DATE  
NOVEMBER 21, 2016

SHEET TITLE  
Site Plan

SHEET INFORMATION

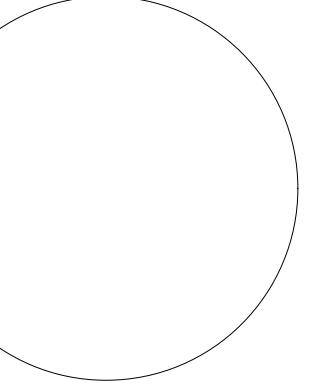
Sheet Number: 4











## PARKSIDE FIRST SUBDIVISION

LOVELAND, CO  
PREPARED FOR

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1021 NIGHTINGALE DRIVE  
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(970) 685-1575  
CONTACT: KENNETH MITCHELL

### PLANT LIST

KEY	QTY	DEVELOPER QTY	BUILDER QTY	RATIO	COMMON NAME	BOTANICAL NAME	HEIGHT	WIDTH	SIZE	INSTALLATION NOTES
<b>SHADE / CANOPY TREES -</b>										
										172
○	27	8	19	9.4%	CATALPA	Catalpa speciosa	60'	50'	2" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
△	30	19	11	11.9%	COFFEETREE, KENTUCKY	Gymnocladus dioicus	60'	50'	2" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
+	25	7	18	8.7%	HACKBERRY, WESTERN	Celtis occidentalis	60'	50'	2" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
○	25	7	18	8.7%	HONEYLOCUST, IMPERIAL	Gleditsia triacanthos inermis 'Imperial'	35'	35'	2" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
○	32	11	21	11.1%	LINDEN, REDMOND	Tilia americana 'Redmond'	50'	40'	2" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
○	33	14	19	11.5%	OAK, BUR	Quercus macrocarpa	60'	50'	2" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
<b>EVERGREEN TREES -</b>										
○	24	24	0	8.4%	SPRUCE, FASTIGIATE NORWAY	Picea abies 'Cupressina'	15'	6'	6' BB	FULL SPECIMEN, EVENLY AND WELL BRANCHED W/ STRAIGHT TRUNK & TOP LEADER
○	12	12	0	4.2%	SPRUCE, COLORADO BLUE	Picea pungens glauca	40'	30'	6' BB	FULL SPECIMEN, EVENLY AND WELL BRANCHED W/ STRAIGHT TRUNK & TOP LEADER
○	27	27	0	9.4%	SPRUCE, BAKERI	Picea pungens 'Bakeri'	35'	15'	6' BB	FULL SPECIMEN, EVENLY AND WELL BRANCHED W/ STRAIGHT TRUNK & TOP LEADER
○	16	16	0	5.6%	PINE, PONDEROSA	Pinus ponderosa	60'	25'	6' BB	FULL SPECIMEN, EVENLY AND WELL BRANCHED W/ STRAIGHT TRUNK & TOP LEADER
<b>ORNAMENTAL TREES -</b>										
△	11	11	0	3.8%	CRABAPPLE, CARDINAL	Malus 'Cardinal'	15'	20'	1.5" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
○	10	10	0	3.5%	LILAC, JAPANESE	Syringa reticulata	20'	20'	1.5" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
○	15	15	0	5.2%	SERVICEBERRY, AUTUMN BRILLIANCE	Amelanchier grandiflora 'Autumn Brilliance'	20'	20'	1.5" cal. BB	BALANCED, WELL BRANCHED W/ STRAIGHT TRUNK & CENTRAL LEADER
<b>DECIDUOUS SHRUBS -</b>										
○	12	12	0	-	BUTTERFLY BUSH, COMPACT PURPLE	Buddleja davidii nahoensis	5'	5'	5 Gallon	24" (h) FULL SPECIMEN, EVENLY AND WELL BRANCHED
○	19	19	0	-	DOGWOOD, REDTWIG	Cornus baileyi	4'	4'	5 Gallon	24" (h) FULL SPECIMEN, EVENLY AND WELL BRANCHED
○	13	13	0	-	LILAC, DWARF KOREAN	Syringa meyeri 'Palibin'	4'	4'	5 Gallon	24" (h) FULL SPECIMEN, EVENLY AND WELL BRANCHED
○	12	12	0	-	SPIREA, BLUE MIST	Caryopteris x clandonensis 'Blue Mist'	4'	3'	5 Gallon	24" (h) FULL SPECIMEN, EVENLY AND WELL BRANCHED
<b>PERENNIALS / GRASSES -</b>										
○	47	47	0	-	HYSSOP, SONORAN SUNSET	Agastache cana Sonoran Sunset	15"	15"	1 Gallon	WELL ROOTED AND ESTABLISHED
○	40	40	0	-	GRASS, HEAVY METAL BLUE SWITCH	Panicum virgatum 'Heavy Metal'	3'	18"	1 Gallon	WELL ROOTED AND ESTABLISHED

### NOTES:

THE PLANT LIST ABOVE ILLUSTRATES TYPICAL LANDSCAPE PLANTINGS THAT MAY BE INCLUDED WITH THE PARKSIDE VILLAGE LANDSCAPE PLAN. THE FINAL LANDSCAPE PLANT LIST MAY VARY.

### LANDSCAPE NOTES

- LANDSCAPE CONTRACTOR MUST CONTACT CITY OF LOVELAND AND RECREATION DEPARTMENT PRIOR TO PLANTING IN ORDER TO VERIFY PROPER PLANTING.
- CONTRACTOR SHALL ENSURE THAT THE LANDSCAPE PLAN IS COORDINATED WITH THE PLANS DONE BY OTHER CONSULTANTS SO THAT THE PROPOSED GRADING, STORM DRAINAGE, OR OTHER CONSTRUCTION DOES NOT CONFLICT NOR PRECLUDE INSTALLATION AND MAINTENANCE OF LANDSCAPE ELEMENTS ON THIS PLAN.
- JOB SITE TO BE KEPT CLEAN AT ALL TIMES AND CONSTRUCTION AREAS ARE TO BE MAINTAINED FOR SAFETY.
- SOILS DISTURBED ADJACENT TO WORK AREA, INCLUDING AREAS OUTSIDE OF CONSTRUCTION LIMITS, DUE TO NEW CONSTRUCTION ARE TO BE REGRADED AND SURFACE CONDITIONS REPAIRED EQUIVALENT TO THAT CONDITION PRIOR TO START OF WORK.
- PROTECT EXISTING SURFACES AND SOILS, BOTH INSIDE AND OUTSIDE OF CONSTRUCTION LIMITS, DURING CONSTRUCTION. IF GRADES, CONCRETE OR ASPHALT ARE DAMAGED DUE TO CONSTRUCTION OPERATIONS OR WEATHER THE CONTRACTOR IS RESPONSIBLE FOR REPAIR TO THAT EQUIVALENT TO EXISTING CONDITIONS AT NO EXPENSE TO THE OWNER / CITY.
- CONTRACTOR IS RESPONSIBLE FOR SETUP OF BARRICADES, WARNING SIGNAGE, OR OTHER PROTECTIVE DEVICES IF ANY EXCAVATIONS ARE LEFT EXPOSED AFTER ON-SITE WORK HOURS.
- THE CONTRACTOR SHALL NOT PURPOSEFULLY PROCEED WITH ANY CONSTRUCTION PER PLANS PROVIDED WHEN OBSTRUCTIONS AND/OR GRADE DIFFERENCES EXIST THAT WERE NOT CONSIDERED OR CHANGED AFTER PLANS WERE SUBMITTED. CONTRACTOR SHALL NOTIFY OWNER OR OWNER'S REPRESENTATIVE AND THE CITY OF LOVELAND IF SITUATION ARISES AND REVISIONS ARE NECESSARY.
- THE CONTRACTOR SHALL PREVENT SEDIMENT, DEBRIS AND OTHER POLLUTANTS FROM ENTERING ANY STORM WATER SEWER SYSTEM OR, ADJACENT WATER WAYS, ETC., DURING THE DEMOLITION OR CONSTRUCTION OPERATIONS THAT ARE PART OF THIS PROJECT. THE CONTRACTOR SHALL BE HELD RESPONSIBLE AND EXPENSE FOR THE CORRECTION OF ANY ADVERSE IMPACTS TO THE STORM WATER SEWER SYSTEM OR, ADJACENT WATER WAYS, WETLANDS ETC., RESULTING FROM THE WORK DONE AS PART OF THIS PROJECT/CONTRACT.
- THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE PRIOR TO BIDDING AND CONSTRUCTION, OF BECOMING AWARE OF ALL EXISTING AND PROPOSED UTILITIES, PIPES, STRUCTURES, ETC. CALL UNCC THREE DAYS BEFORE SCHEDULED WORK AT 811 OR 1-800-922-1987.
- STREET AND ORNAMENTAL TREES SHALL BE PLANTED NO CLOSER THAN FORTY (40) FEET AND FIFTEEN (15) FEET RESPECTIVELY FROM STREET LIGHTS. NO TREES SHALL BE PLANTED WITHIN TEN (10) FEET FROM WATER AND SEWER LINES, FOUR (4) FEET FROM GAS, TELEPHONE AND ELECTRIC UTILITIES, AND TEN (10) FEET FROM ANY DRIVEWAY.
- MINIMUM CLEARANCE OF THREE (3) FEET ON EACH SIDE OF FIRE DEPARTMENT CONNECTION (FDC). NO VEGETATION OTHER THAN TURF OR GROUND COVERS PLANTED IN FRONT OF FDC.

11. NO SUBSTANTIAL IMPEDIMENT TO VISIBILITY BETWEEN THE HEIGHTS OF THREE (3) FEET AND EIGHT(8) FEET SHALL BE CREATED OR MAINTAINED AT STREET INTERSECTIONS WITHIN A SITE TRIANGLE DESCRIBED AS FOLLOWS: BEGINNING AT THE POINT OF INTERSECTION OF THE EDGES OF THE DRIVING SURFACE, THEN TO FORTY (40) FEET ALONG BOTH INTERSECTING EDGES AND THEN ALONG A TRANSVERSE LINE CONNECTING THESE POINTS.

12. IF TREES OR SHRUBS ARE LOCATED ON TOP OF FIELD VERIFIED UTILITIES, CONTRACTOR SHALL NOTIFY OWNER BEFORE ANY DIGGING HAS COMMENCED. VERIFY WITH OWNER IF AND WHICH SHRUBS/TREES SHALL BE TAKEN OUT OF PROJECT/CONTRACT.

13. ALL LANDSCAPE AREAS SHALL BE MAINTAINED, INCLUDING MOWING, WATERING AND FERTILIZING BY CONTRACTOR, UP TO FINAL ACCEPTANCE. AT SUCH TIME OWNER WILL BE RESPONSIBLE FOR ALL MAINTENANCE. LANDSCAPE AND IRRIGATION WILL BE WARRANTED FOR ONE (1) FULL YEAR AFTER FINAL ACCEPTANCE BY OWNER

14. TO THE MAXIMUM EXTENT FEASIBLE, TOPSOIL THAT IS REMOVED DURING CONSTRUCTION ACTIVITY SHALL BE CONSERVED FOR LATER USE ON AREAS REQUIRING REVEGETATION AND LANDSCAPING.

15. EXCAVATED MATERIAL TO BE USED AS FILL WILL HAVE ALL ROCKS, DEBRIS, WASTE MATERIAL, FROZEN MATERIAL, VEGETATION LARGER THAN 3" IN ANY DIMENSION REMOVED BEFORE PLACEMENT AND COMPACTION OF SOIL.

16. PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDING FOUNDATIONS AND A SMOOTH TRANSITION BETWEEN ALL ADJACENT EXISTING GRADES AND PROPOSED GRADES

17. PRIOR TO FINE GRADING, SOD AREAS AND PLANTING BEDS, SHALL BE THOROUGHLY LOOSENED AND TILLED TO A 6" DEPTH. REMOVE ALL UNSUITABLE TOPSOIL, INCLUDING ALL ROCKS LARGER THAN 3 INCHES IN ANY DIRECTION, ALL CONCRETE, TRASH, DEBRIS, WEEDS, ROOTS AND OTHER WASTE MATERIALS. AFTER THAT TIME ORGANIC AMENDMENTS SHALL THEN BE THOROUGHLY TILLED AND INCORPORATED TO A MINIMUM 6" DEPTH IN THESE AREAS AT THE MINIMUM OF RATE OF 3 CU. YDS. / 1,000 SQ.FT.

18. 10 - UNIFORMLY COMPACT AND FINISH GRADE THESE SOD / GRASS AREAS AND PLANTING BEDS TO A SMOOTH SURFACE, FREE FROM IRREGULAR SURFACE CHANGES. CUT OUT SOFT SPOTS, FILL IN LOW SPOTS AND TRIM HIGH SPOTS TO COMPLY WITH REQUIRED GRADE TOLERANCES.

19. ONCE COMPACTED AND FINELY GRADED ALL ROCKS, DEBRIS, WASTE MATERIAL AND VEGETATION MATERIAL LARGER THAN 1/2" WILL BE RAKED FROM THE SURFACE AND REMOVED FROM SITE.

20. SOD TO BE 100% COLORADO GROWN TALL FESCUE GRASS SOD BLEND SPECIFICALLY GROWN FOR LOW WATER LAWN APPLICATIONS WITH MINIMUM THREE (3) IMPROVED VARIETIES, HAVING A HEALTHY VIGOROUS ROOT SYSTEM. ONCE TURF IS LAID IT SHALL BE PROPERLY ROLLED, COMPACTED AND PUSHED TOGETHER TO ELIMINATE ANY GAPS BETWEEN ROLL EDGES. APPLY FERTILIZER IN THESE AREAS PER SOD FARM'S RECOMMENDATIONS.

21. SEEDED AREAS IF APPLICABLE: ADEQUATE IRRIGATION WILL BE PROVIDED FOR THE ESTABLISHMENT AND MAINTENANCE FOR THESE SEDED AREAS, AND THAT NATIVE GRASSES SHALL BE MAINTAINED IN A CONDITION OF ACCEPTABLE HEIGHT, FREE OF WEEDS AND DEBRIS, AND SHALL NOT REPRESENT A FIRE HAZARD NOR BECOME A NUISANCE SITE FOR WATER OR WIND EROSION.

22. SECOND DOMESTIC WATER METER TO BE SHOWN ON LANDSCAPE PLAN FOR IRRIGATION CONNECTION OR NON-POTABLE WATER SOURCE CONNECTION.

23. ALL PLANT MATERIALS ARE SIZED AND OUTLINED IN PLANT LIST. ALL PLANTS TO BE PLANTED IN AMENDED SOIL AND STAKED AS SHOWN IN DETAILS, INCLUDING 18" RADIUS WOOD MULCH RING IF PLANTED WITHIN GRASS AREAS PER DETAILS. ALL PLANT MATERIAL SHALL MEET OR EXCEED THE CODE OF STANDARDS CURRENTLY RECOMMENDED BY THE COLORADO NURSERY ACT FOR NUMBER ONE GRADE.

24. IF PLANTS ARE IN NEED OF REPLACEMENT DUE TO DECLINING HEALTH, DISEASE, OR DEATH, THE PLANTS MUST BE REPLACED WITH THE ORIGINAL SPECIES UNLESS APPROVED BY THE TOWN FORESTER.

25. CHANGES IN PLANT SPECIES OF PLANT LOCATIONS FROM WHAT IS LISTED ON THE LANDSCAPE PLAN WILL REQUIRE THE APPROVAL OF THE TOWN FORESTER PRIOR TO INSTALLATION OF REPLACEMENT. OVERALL QUANTITY AND QUALITY TO BE CONSISTENT WITH THE APPROVED PLANS. IN THE EVENT OF CONFLICT WITH THE QUANTITIES INCLUDED IN THE PLANT LIST, SPECIES AND QUANTITIES SHALL BE PROVIDED.

26. ALL TREES AND SHRUBS TO BE BALLED AND BURLAPPED, OR CONTAINERIZED AND SHALL HAVE ALL WIRE, TWINE, BASKETS, BURLAP, AND ALL OTHER NON-BiodeGRADABLE CONTAINMENT MATERIAL REMOVED FROM THE TRUNK AND/OR ROOT BALL OF THE PLANT, PRIOR TO PLANTING.

27. ALL SHRUB BEDS SHALL HAVE MINIMUM 4" DEPTH SHREDDED CEDAR MULCH - NATURAL COLOR AND/OR WASHED SMOOTH COBBLE. PLANTS LOCATED IN ROCK MULCH SHALL HAVE WOOD MULCH AROUND BASE. A CONTINUOUS LAYER OF TYPAR LANDSCAPE FABRIC OR APPROVED EQUAL SHALL BE INSTALLED IN ALL SHRUB BEDS WITH 6" OVERLAP AT SEAMS WITH 4" STAPLES 4 O.C. IN ALL DIRECTIONS.

28. EDGING BETWEEN GRASS TYPES AND SHRUB BEDS SHALL BE HEAVY DUTY STEEL EDGER MIN. 14 GA x 4" WITH ROLLED TOP AND SHALL BE SET LEVEL WITH THE TOP OF THE ADJACENT SOD. NO EDGING SHALL BE USED BETWEEN CEDAR MULCH AND COBBLE TRANSITIONS

29. NON IRRIGATED NATIVE GRASS -

FOOTHILLS NATIVE GRASS MIX: SEED SHALL BE A MIXTURE THAT MATCHES THE FOLLOWING:

NON IRRIGATED NATIVE GRASS -

COMMON NAME %

ANNUAL RYEGRASS 20%

SLENDER WHEATGRASS 15%

MOUNTAIN BROME 10%

PUBESCENT WHEATGRASS 10%

HARD FESCUE 10%

KENTUCKY BLUEGRASS 10%

INDIAN GRASS 8%

BIG BLUESTEM 7%

BLUE GRAMA 5%

SWITCHGRASS 5%

1. SEED SHALL BE AS MANUFACTURED BY ARKANSAS VALLEY SEED SOLUTIONS, 4625 COLORADO BOULEVARD, DENVER, CO 80216, (877) 597-3337.

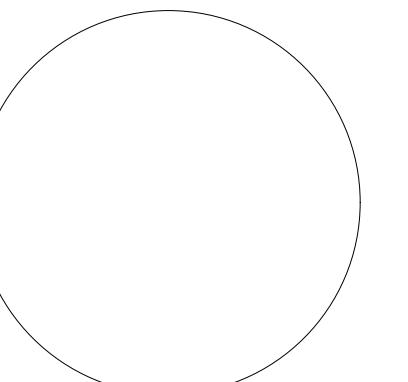
2. DRILLED IN TWO PERPENDICULAR DIRECTIONS WITH AN APPLICATION RATE: 25.0 LBS (PLS) PER ACRE (0.57 LBS / 1000 SF)

3. ADEQUATE TEMPORARY IRRIGATION OR BY WATER TRUCK, OR BY OTHER ACCEPTABLE MEANS, WILL BE PROVIDED BY CONTRACTOR FOR THE WATERING, ESTABLISHMENT AND MAINTENANCE OF THESE SEDED AREAS, AND THAT NATIVE GRASSES SHALL BE MAINTAINED IN A CONDITION OF ACCEPTABLE HEIGHT, FREE OF WEEDS, TRASH AND DEBRIS, AND SHALL NOT REPRESENT A FIRE HAZARD NOR BECOME A NUISANCE SITE FOR WATER OR WIND EROSION.

MULCH IN ALL NATIVE SEED AREAS:

1. IMMEDIATELY FOLLOWING THE RAKING / SEEDING OPERATION, ADD STRAW MULCH TO THE SEDED AREAS.

2. APPLY STRAW MULCH AT A MINIMUM OF 1.5 TONS PER ACRE OF AIR DRY MATERIAL. SPREAD STRAW MULCH UNIFORMLY OVER THE AREA WITH MECHANICAL MULCH SPREADER / CRIMPER. DO NOT MULCH WHEN W



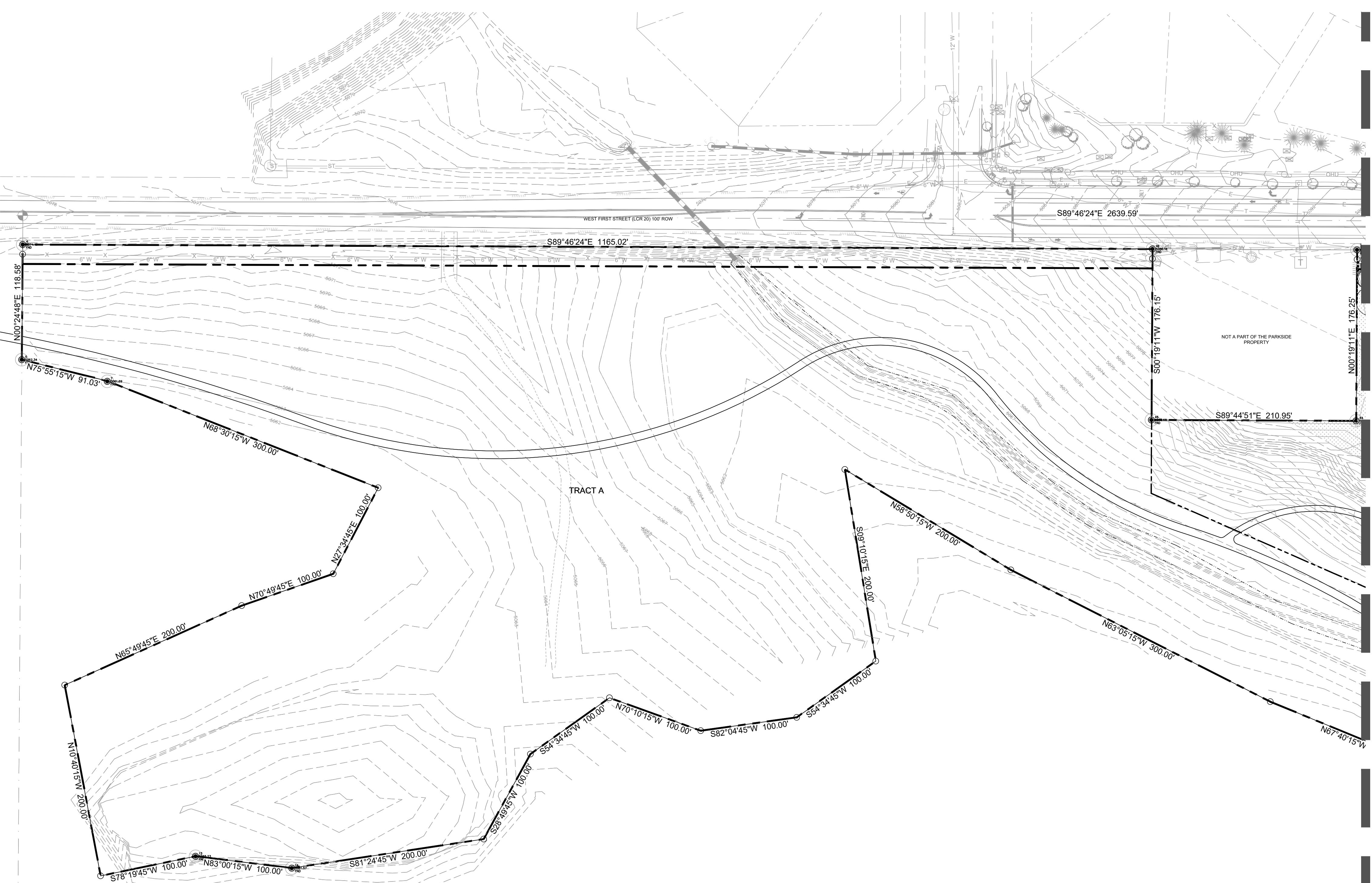
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### PLANT LIST

KEY	QTY	DEVELOPER QTY	BUILDER QTY	RATIO	COMMON NAME
<b>SHADE / CANOPY TREES -</b>					
○	27	8	19	9.4%	CATALPA
△	30	19	11	11.9%	COFFEE TREE, KENTUCKY
+	25	7	18	8.7%	HACKBERRY, WESTERN
○	25	7	18	8.7%	HONEYLOCUST, IMPERIAL
○	32	11	21	11.1%	LINDEN, REDMOND
+	33	14	19	11.5%	OAK, BUR
<b>EVERGREEN TREES -</b>					
◊	24	24	0	8.4%	SPRUCE, FASTIGIATE NORWAY
●	12	12	0	4.2%	SPRUCE, COLORADO BLUE
○	27	27	0	9.4%	SPRUCE, BAKERI
○	16	16	0	5.6%	PINE, PONDEROSA
<b>ORNAMENTAL TREES -</b>					
○	11	11	0	3.8%	CRABAPPLE, CARDINAL
○	10	10	0	3.5%	LILAC, JAPANESE
○	15	15	0	5.2%	SERVICEBERRY, AUTUMN BRILLIANCE
<b>DECIDUOUS SHRUBS -</b>					
○	12	12	0	-	BUTTERFLY BUSH, COMPACT PURPLE
○	19	19	0	-	DOGWOOD, REDTWIG
○	13	13	0	-	LILAC, DWARF KOREAN
○	12	12	0	-	SPIREA, BLUE MIST
<b>PERENNIALS / GRASSES -</b>					
○	47	47	0	-	HYSSOP, SONORAN SUNSET GRASS, HEAVY METAL BLUE SWITCH
*	40	40	0	-	

### NOTES:

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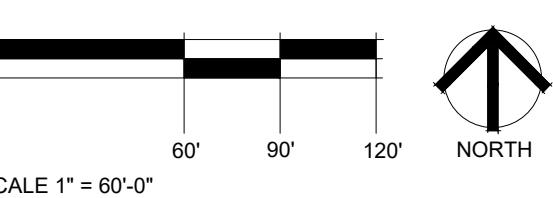
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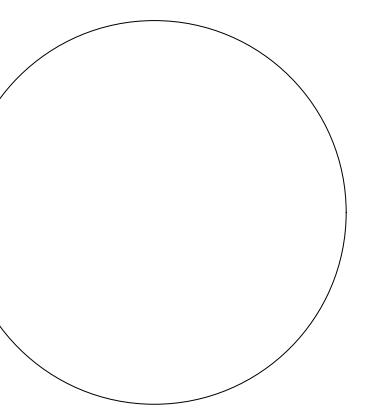
DATE  
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SHEET TITLE  
Landscape Plan

SHEET INFORMATION

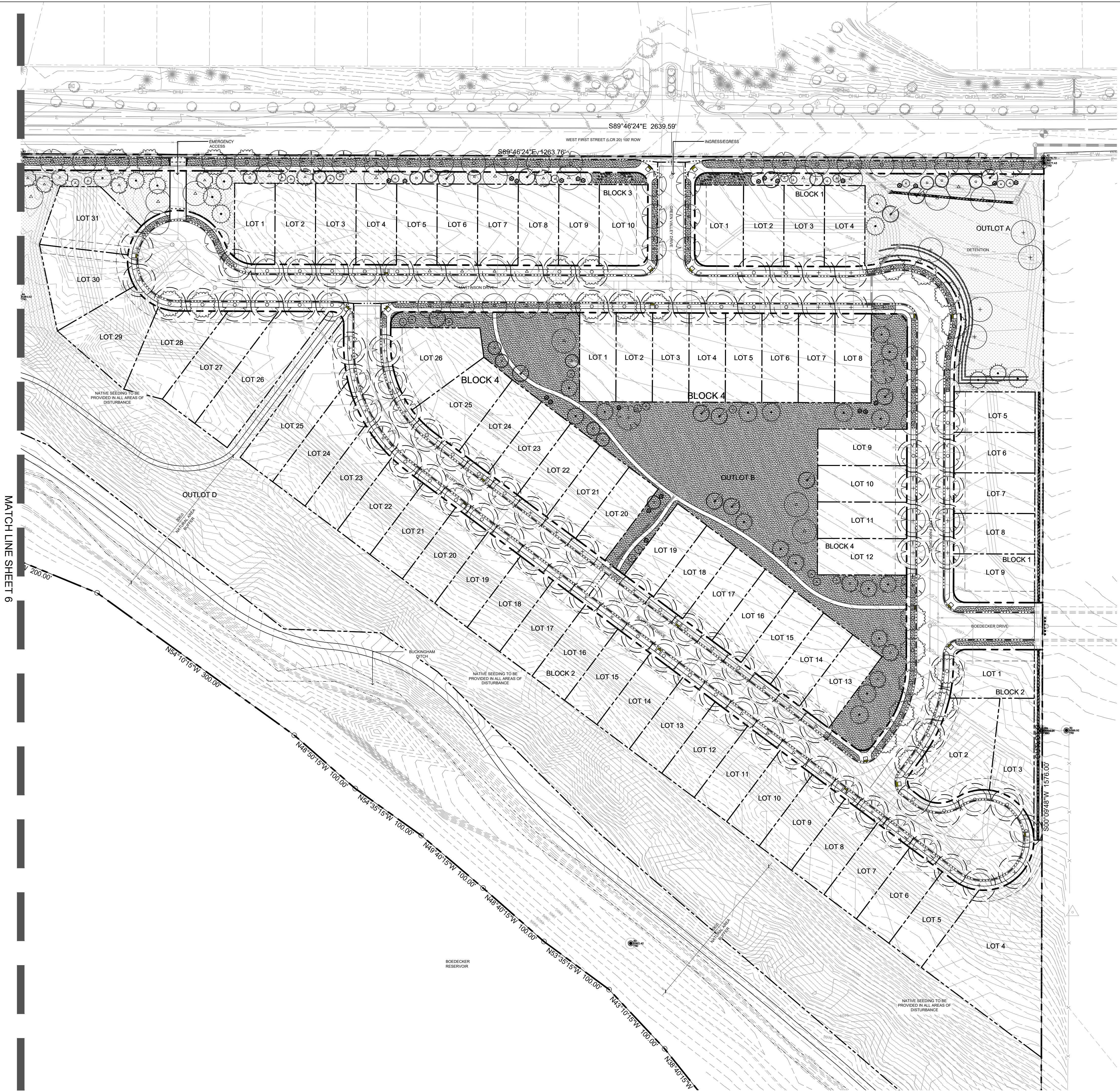
Sheet Number: 9





## PARKSIDE FIRST SUBDIVISION

LOVELAND, CO

PREPARED FOR  
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LLC.1021 NIGHTINGALE DRIVE  
FORT COLLINS, CO. 80525  
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<b>DECIDUOUS SHRUBS -</b>					
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※	19	19	0	-	DOGWOOD, REDTWIG
○	13	13	0	-	LILAC, DWARF KOREAN
○	12	12	0	-	SPIREA, BLUE MIST
<b>PERENNIALS / GRASSES -</b>					
○	47	47	0	-	HYSSOP, SONORAN SUNSET GRASS, HEAVY METAL BLUE SWITCH
※	40	40	0	-	

## **SUMMARY OF ARCHITECTURAL REQUIREMENTS FOR PARKSIDE**

### **SETBACKS**

The minimum front yard setback from the street is 15 feet for the main structure of the house, which shall include any porch, casita, courtyard fencing or other dominant architectural feature. Garage door fronts are not included in the 15 foot setback.

Garage doors will be set back a minimum of 20 feet from the front property line and a minimum of 3 feet from any front elevation feature, including the main structure of the house, any porch, casita, courtyard fencing or other dominant architectural element.

All side yards shall have a minimum setback of 5 feet from the side lot line.

The rear yard setback shall be a minimum of 15 feet from the rear property line.

The side yard setback on a dwelling where the side yard adjoins a city street shall be 10 feet adjoining such street.

Cornices, eaves and similar architectural features may extend 3 feet into the required setbacks except that no cornice, eave or architectural feature shall extend over a lot line or easement.

### **GENERAL ARCHITECTURAL FEATURES**

Roof Fascia shall have a minimum width of eight (8) inches.

Trim boards will be installed around all front and rear yard windows, unless surrounded by masonry.

Trim boards will be installed at all exterior corners unless such corner are masonry.

Front windows shall have a grid (divided light) feature.

Gable ends which face a street shall have a material change from the dominant front elevation exterior material, including lap siding, vertical board and batten siding, fish scale siding, or stucco.

Any gable which faces a street shall have a louvered vent or window.

The first floor of the front elevation shall include 30% masonry, not including the garage door. Where masonry extends to the outside corner of the dwelling unit or garage, it shall be wrapped around the corner a minimum of two (2) feet.

There will be a brick, cultured stone or natural stone soldier course or ledge at the top edge of all masonry.

Roof pitch will be a minimum of five in twelve. Roof material will be weathered wood in appearance and will not be bright colors or solid white or black.

Roofs will have at least two of the following architectural elements:

Dormer window(s)

Gable ends

Varying roof pitch and/or roof planes

Exterior materials and colors shall be harmonious with other existing homes in the neighborhood.

#### **GARAGE DOOR TREATMENT**

Garage doors will have raised panel doors with windows in the upper panel. Garage doors will have either a trim board around the frame, or stone at least 36" high on either side of the opening. "Dark Sky" accent lighting will be installed on either side of the garage door.

#### **CASITA**

Each home may have a free-standing "casita" that serves as a separate guest quarters, "mother-in-law room", home office or general living space with or without bath and kitchen facilities. The casita will be a minimum of 150 square feet and shall not exceed 750 square feet. The casita may be forward of the main façade of the house as long as it follows the same architectural requirements for front elevations and may be located to the rear of the main façade of the house as long as it allows for the appropriate rear yard setback. The casita may be tied to the main structure of the home with either an enclosed or open breezeway. Casitas in the front of the home will be utilized to create a front entry courtyard for outdoor living space.

#### **MATERIALS AND COLORS**

Exterior materials and colors shall be harmonious with other existing homes in the neighborhood.

#### **FENCING**

Solid rear yard fencing with a maximum height of 6 feet will be allowed only along 1<sup>st</sup> Street and the east boundary lots. Fencing in this area will be cedar, stained a standard color for the entire community. The same fence standard will apply to all 6 foot fencing.

Any rear yard fence that adjoins a common area will be an open cedar three rail fence with the same stain color as the 1<sup>st</sup> Street fence. The common area fence will be a maximum of four feet high at the top of the top rail, and stone columns will be allowed as long as they do not exceed a height of six (6) inches over the highest portion of any wood rail or vertical slat. All stone columns that are a part of any fence will be identical for the entire community.

Front courtyard fencing must match the siding treatment on the main dwelling and will have columns a maximum spacing of eight (8) feet apart. Front courtyard fencing will be a maximum of six (6) feet tall. Front courtyard fencing will be considered the same as a front elevation, with the appropriate first floor masonry requirements.

Side yard fencing to the rear of the main elevation shall match the common area fencing standards.

In all cases, the "finished" side of the fence shall face either the street or any common area boundary.

# PARKSIDE FIRST SUBDIVISION

A PARCEL OF LAND LOCATED IN THE NORTHEAST QUARTER OF SECTION 20,  
TOWNSHIP 5 NORTH, RANGE 69 WEST OF THE 6TH PRINCIPAL MERIDIAN, CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO

## CERTIFICATE OF OWNERSHIP AND DEDICATION:

KNOW ALL PERSONS BY THESE PRESENTS that \_\_\_\_\_, being all the owners and lienholders of PARKSIDE SUBDIVISION, except any existing public streets, roads or highways, which property is located in Section 20, Township 5 North, Range 69 West of the 6th Principal Meridian, City of Loveland, County of Larimer, State of Colorado being more particularly described as follows:

The NW ¼ and that part of NE ¼ lying North of the Home Supply Ditch Company's Reservoir (according to Loesher Survey) all in Section 20, Township 5 North, Range 69 West of the 6th P.M., County of Larimer, State of Colorado.

EXCEPT Book 1804, Page 368: Commencing at a point on North line of said Section 20, a distance of 1264 feet West of the NE corner of said Section, thence South 206.4 feet, thence West 211 feet, thence North 206.4 feet, thence East 211 feet to point of beginning.

ALSO EXCEPT those parcels conveyed by Warranty Deed recorded in Book 1405 at Page 605, Quit Claim Deed recorded in Book 1434 at Page 358 and land described in Contract recorded in Book 1470 at Page 77, and parcel conveyed by Warranty Deed recorded April 9, 1973 in Book 1548 at Page 595, records of Larimer County, Colorado.

ALSO LESS West First Street right of way.

Said parcel of land contains 1,795,947 square feet, or 41.229 acres, more or less (±), and is subject to any rights-of-way or other easements of record or as now existing on said described parcel of land.

Being more particularly described as:

A tract of land located in the Northeast Quarter of Section 20, Township 5 North, Range 69 West of the 6th Principal Meridian, County of Larimer, State of Colorado.

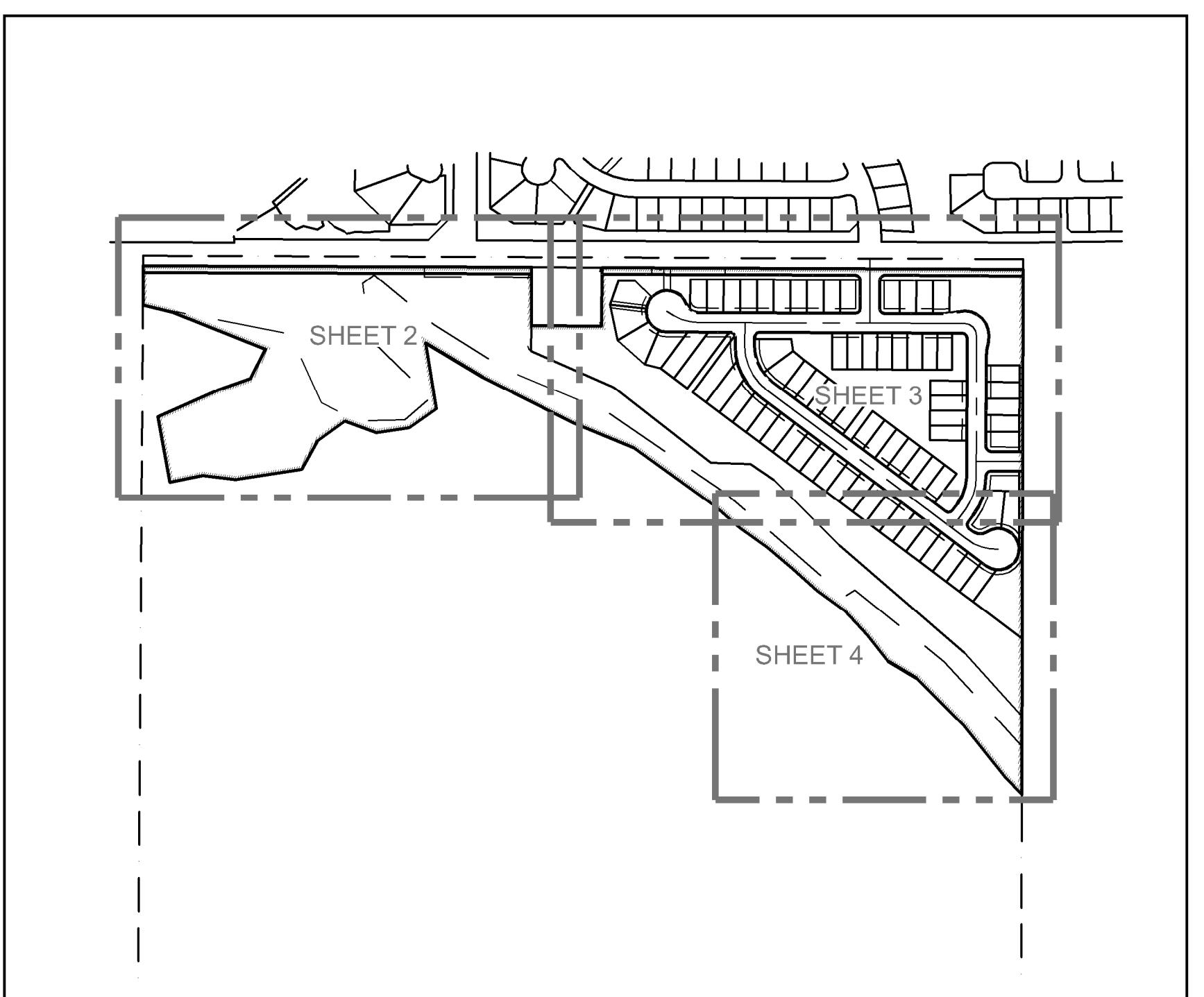
Considering the North line of the Northeast Quarter of said Section 20, as bearing South 89°46'24" East and with all bearings contained herein relative thereto:

COMMENCING at the Northeast Quarter of said Section 20; thence, along the East line of said Section 20, South 00°09'48" West, 30.00 feet to the POINT OF BEGINNING; thence continuing along said East line, South 00°09'48" West, 1,576.00 feet; thence departing said East line, North 44°10'12" West, 70.00 feet; thence, North 38°40'15" West, 200.00 feet; thence, North 44°55'15" West, 200.00 feet; thence, North 59°25'15" West, 100.00 feet; thence, North 38°40'15" West, 100.00 feet; thence, North 43°10'15" West, 100.00 feet; thence, North 53°35'15" West, 100.00 feet; thence, North 48°40'15" West, 100.00 feet; thence, North 49°40'15" West, 100.00 feet; thence, North 54°35'15" West, 100.00 feet; thence, North 48°50'15" West, 100.00 feet; thence, North 54°10'15" West, 300.00 feet; thence, North 67°40'15" West, 200.00 feet; thence, North 33°05'15" West, 300.00 feet; thence, North 58°50'15" West, 200.00 feet; thence, South 09°10'15" East, 200.00 feet; thence, South 54°34'45" West, 100.00 feet; thence, South 82°04'45" West, 100.00 feet; thence, South 09°10'15" East, 200.00 feet; thence, South 54°34'45" West, 100.00 feet; thence, South 28°49'45" West, 100.00 feet; thence, South 81°24'45" West, 200.00 feet; thence, North 83°00'15" West, 100.00 feet; thence, South 78°19'45" West, 100.00 feet; thence, North 10°40'15" West, 200.00 feet; thence, North 65°49'45" East, 200.00 feet; thence, North 70°49'45" East, 100.00 feet; thence, North 27°34'45" East, 100.00 feet; thence, North 68°30'15" West, 300.00 feet; thence, North 75°55'15" West, 91.03 feet to a point on the West line of the Northeast Quarter of said Section 20; thence along said line, North 00°24'48" East, 118.58 feet to a Point on the South right of way line of West First Street; thence along said line, South 89°46'24" East, 1,165.02 feet to the Northwest corner of a parcel recorded at Book 1804 on Page 368; thence departing said right of way line and along the boundary of said parcel the next three (3) courses and distances, South 00°19'11" West, 176.15 feet; thence, South 89°44'51" East, 210.95 feet; thence, North 00°19'11" East, 176.25 feet to a point on the South right of way line of West First Street; thence along said line, South 89°46'24" East, 1,263.76 feet to the Point of Beginning and do hereby designate the same as "PARKSIDE FIRST SUBDIVISION" to the City of Loveland, Colorado.

All expenses involving necessary improvements for water system, sanitary sewer system, storm sewer system, curbs and gutters, sidewalks, street improvements, street signs, grading and landscaping shall be paid by owner.



**VICINITY MAP**  
NOT TO SCALE



**KEY MAP**  
SCALE: 1" = 400'

LAND USE TABLE					
PARCEL	AREA	PERCENT	USE	OWNED & MAINTAINED BY	
TRACT A	749.247	S.F. 17.20	AC. 41.72%	Open Space	City of Loveland
OUTLOT A	47.554	S.F. 1.09	AC. 2.65%	Drainage Easement	HOA
OUTLOT B	83.016	S.F. 1.91	AC. 4.62%	Access, Utility & Drainage Easement	HOA
OUTLOT C	2.350	S.F. 0.05	AC. 0.13%	Drainage Easement	HOA
OUTLOT D	240.672	S.F. 5.53	AC. 13.40%	Drainage Easement, Access & Emergency Access	HOA
LOTS	435.334	S.F. 9.99	AC. 24.24%	Development	Owner
RIGHT OF WAY	237.774	S.F. 5.46	AC. 13.24%	Public Access	City of Loveland
<b>TOTAL</b>	<b>1,795,947</b>	<b>S.F. 41.23</b>	<b>AC. 100.00%</b>		

## PLANNING COMMISSION CERTIFICATE:

This preliminary plat of PARKSIDE FIRST SUBDIVISION is hereby approved by the Planning Commission of the City of Loveland, Larimer County, Colorado, this \_\_\_\_\_ day of \_\_\_\_\_, \_\_\_\_\_, for the preliminary conveyance to the public of the dedications shown herein, which are accepted; subject to the provision that approval in no way constitutes a final plat.

Planning Commission Chair

## NOTES:

1. Basis of Bearings is the North line of the Northeast Quarter as bearing South 89°46'24" East (assumed bearing) and monumented as shown on drawing.

2. The lineal unit of measurement for this drawing is U.S. Survey Feet.

3. Easement note:

Total area in square feet of easements dedicated to the public or the city by this plat. Total area in square feet - 439,319 s.f.

Total area in square feet of easements dedicated to the public or the city that is being vacated by this plat. Total area in square feet - 0 s.f.

4. For all information regarding easements, rights-of-way or title of record, Northern Engineering relied upon File No. 459-H042692-081-TMY, Amendment No. 3, dated April 1, 2016 at 7:00 A.M., prepared by Heritage Title Company.

5. FLOOD ZONE DESIGNATION: According to FIRM Map No. 08069C1169F, December 19, 2006 for Larimer County, this parcel does not lie within a FEMA designated 100-year floodplain.

6. This project is subject to a development agreement which has been recorded in the real property records of Larimer County.

7. Maintenance and upkeep of Stormwater detention ponds, storm sewer systems, swales, and permanent Stormwater quality improvements are required by the City of Loveland and are a continuing obligation of the homeowner association (HOA), business owner association (BOA), or private property owner. The owner(s) or responsible parties (HOA, BOA) shall provide ongoing maintenance to the private Stormwater improvements as needed to maintain compliance with the approved construction plans and reports.

8. Unless otherwise approved by the City, all unsatisfied conditions of approval for the original subdivision shall continue to apply to this property.

## SURVEYOR'S CERTIFICATE:

I, Robert C. Tessely, being a registered Professional Land Surveyor in the State of Colorado, do hereby certify that the survey of "PARKSIDE FIRST SUBDIVISION" was made by me or under my supervision and that the survey is accurately represented on this plat and that the statements contained herein were read by me and the same are true to the best of my knowledge.

Robert C. Tessely  
Colorado Registered Professional Land Surveyor No. 38470  
For and on Behalf of Northern Engineering Services, Inc.

DRAFT  
04/18/17

NOTICE:  
According to Colorado law you must commence any legal action based upon any defect in this survey within three years after you discover such defect. In no event may any action based upon any defect in this survey be commenced more than ten years after the date of the certificate shown herein.

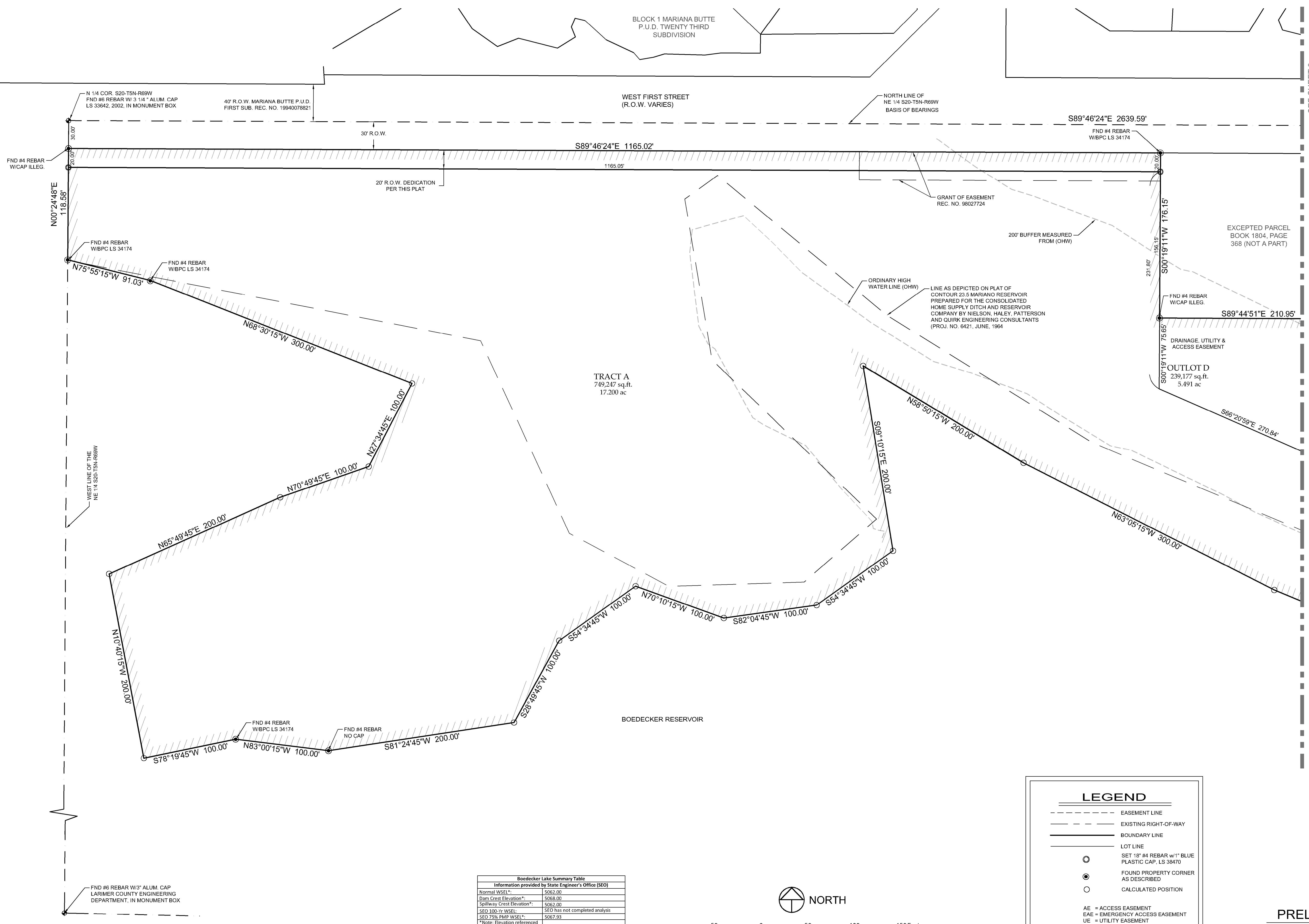
**NORTHERN ENGINEERING**  
PHONE: 970.222.4158  
www.northernengineeringinc.com  
301 North Horsetooth Road Suite 103  
Fort Collins, Colorado 80521

PARKSIDE FIRST SUBDIVISION  
CITY OF LOVELAND  
STATE OF COLORADO

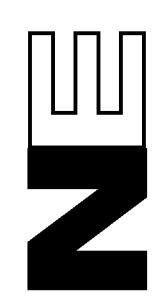
Sheet 1  
Of 4 Sheets

## PARKSIDE FIRST SUBDIVISION

A PARCEL OF LAND LOCATED IN THE NORTHEAST QUARTER OF SECTION 20,  
TOWNSHIP 5 NORTH, RANGE 69 WEST OF THE 6TH PRINCIPAL MERIDIAN, CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO



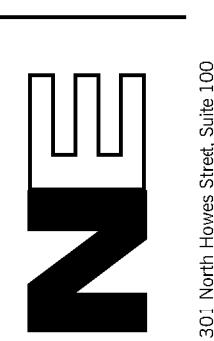
NORTHERN ENGINEERING



PARKSIDE FIRST SUBDIVISION  
CITY OF LOVELAND  
STATE OF COLORADO

SECTION: 20  
TOWNSHIP: 5 N  
RANGE: 69 W of the 6th PM

PHONE: 970.222.4158  
www.northernengineering.com



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Robert C. Tessely  
Registered Professional Land Surveyor  
Colorado Registration No. 38470  
For and on behalf of Northern Engineering Services, Inc.

PARKSIDE FIRST SUBDIVISION  
PC ATTACHMENT



## PARKSIDE FIRST SUBDIVISION

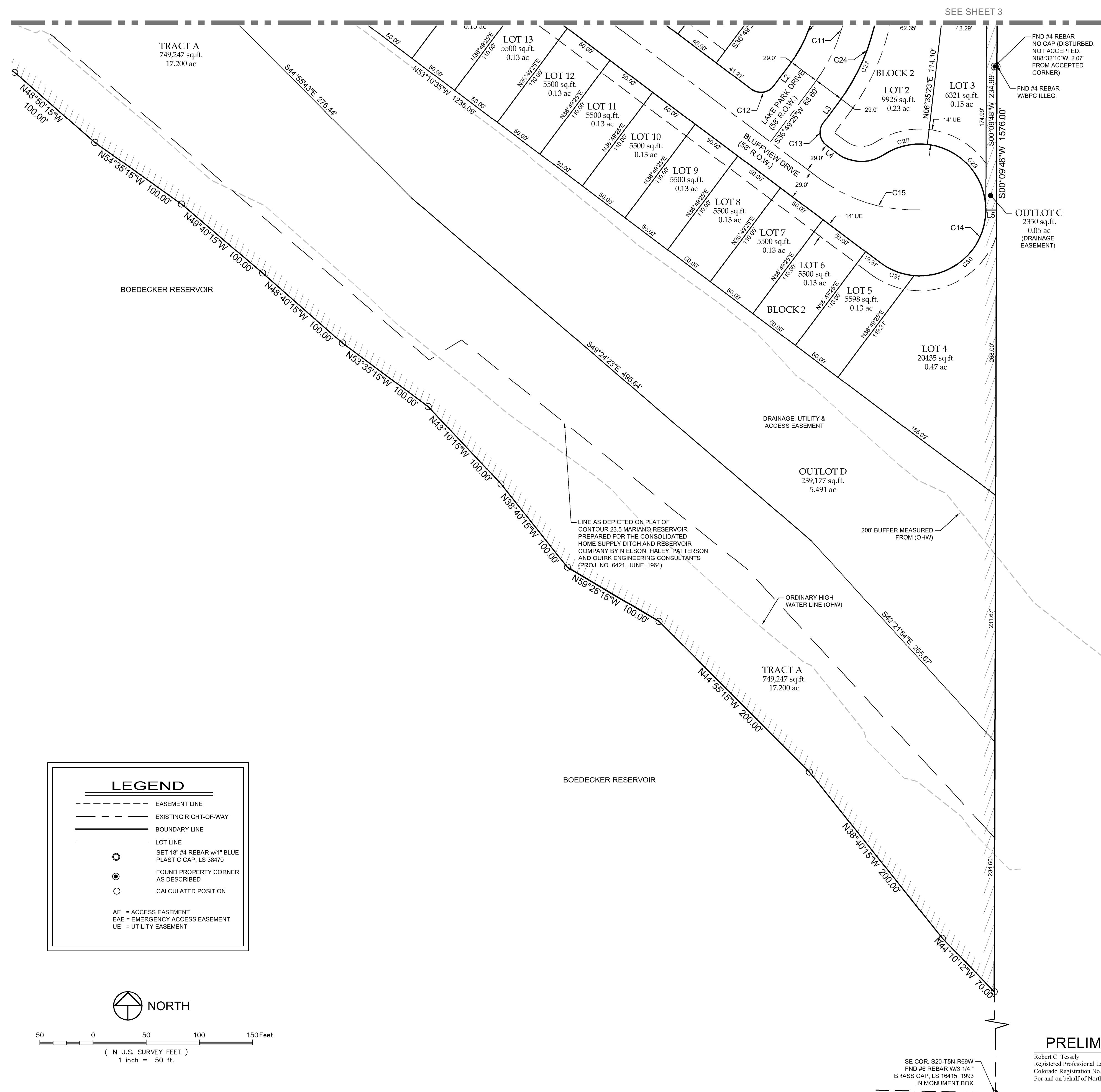
A PARCEL OF LAND LOCATED IN THE NORTHEAST QUARTER OF SECTION 20,  
TOWNSHIP 5 NORTH, RANGE 69 WEST OF THE 6TH PRINCIPAL MERIDIAN, CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO

CURVE TABLE

CURVE	DELTA	RADIUS	LENGTH	BEARING	CHORD
C1	90°00'00"	15.00'	23.56'	N45°13'36"E	21.21'
C2	90°00'00"	15.00'	23.56'	S44°46'24"E	21.21'
C3	90°00'00"	15.00'	23.56'	S45°13'36"W	21.21'
C4	90°00'00"	15.00'	23.56'	N44°46'24"W	21.21'
C5	89°56'12"	15.00'	23.55'	S44°48'18"E	21.20'
C6	89°56'12"	15.00'	58.08'	S44°48'18"E	52.30'
C7	117°51'59"	74.00'	152.23'	S44°48'18"W	126.77'
C8	13°57'54"	209.00'	50.94'	N07°08'45"E	50.81'
C9	90°00'00"	15.00'	23.56'	N44°50'12"W	21.21'
C10	90°00'00"	15.00'	23.56'	N45°09'48"E	21.21'
C11	36°39'37"	165.00'	105.57'	S18°29'37"W	103.78'
C12	90°00'00"	15.00'	23.56'	S81°49'25"W	21.21'
C13	90°00'00"	15.00'	23.56'	N08°10'35"W	21.21'
C14	250°07'23"	62.00'	270.66'	N01°45'44"E	101.50'
C15	38°42'17"	150.13'	101.42'	N72°32'46"W	99.50'
C16	53°24'11"	165.00'	153.79'	N26°28'29"W	148.28'
C17	53°24'11"	136.00'	126.76'	N26°28'29"W	122.22'
C18	53°24'11"	194.00'	180.82'	S26°28'29"E	174.34'
C19	90°00'00"	15.00'	23.56'	N45°13'36"E	21.21'
C20	90°00'00"	15.00'	23.56'	S44°46'24"E	21.21'
C21	38°48'32"	149.99'	101.60'	S70°26'14"E	99.66'
C22	250°18'21"	62.00'	270.86'	S35°11'48"W	101.38'
C23	69°23'27"	38.00'	46.02'	N54°20'45"W	43.26'
C24	36°39'37"	194.00'	124.13'	N18°29'37"E	122.02'
C25	36°39'37"	136.00'	87.02'	S18°29'37"W	85.54'
C26	12°31'39"	194.00'	42.42'	S06°25'38"W	42.33'
C27	24°07'58"	194.00'	81.71'	S24°45'26"W	81.11'
C28	39°53'21"	62.00'	43.16'	N76°38'43"E	42.30'
C29	83°34'25"	62.00'	90.44'	S41°37'24"E	82.63'
C30	94°51'00"	62.00'	102.64'	S47°35'18"W	91.31'
C31	31°48'37"	62.00'	34.42'	N69°04'53"W	33.98'
C32	7°11'34"	194.00'	24.35'	N49°34'48"W	24.34'
C33	15°19'23"	194.00'	51.88'	N38°19'19"W	51.73'
C34	17°19'34"	194.00'	58.66'	N21°59'51"W	58.44'
C35	13°33'40"	194.00'	45.92'	N06°33'14"W	45.81'
C36	36°28'05"	136.00'	86.56'	S18°00'26"E	85.11'
C37	16°56'06"	136.00'	40.20'	S44°42'32"E	40.05'
C38	18°39'55"	15.00'	4.89'	N09°06'21"W	4.86'
C39	71°20'05"	15.00'	18.68'	N54°06'21"W	17.49'
C40	22°44'00"	62.00'	24.60'	S78°35'22"E	24.44'
C41	36°57'51"	62.00'	40.00'	S48°44'23"E	39.31'
C42	36°57'52"	62.00'	40.00'	S11°46'29"E	39.31'
C43	36°57'54"	62.00'	40.00'	S25°11'25"W	39.31'
C44	37°16'19"	62.00'	40.33'	S62°18'32"W	39.62'
C45	18°33'49"	62.00'	20.09'	N89°46'24"W	20.00'
C46	30°25'37"	38.00'	20.18'	S34°51'50"E	19.94'
C47	60°50'28"	62.00'	65.84'	N50°04'16"W	62.79'
C48	38°57'51"	38.00'	25.84'	S69°33'33"E	25.35'
C49	6°22'42"	209.00'	23.27'	N87°02'15"E	23.25'
C50	7°35'11"	209.00'	27.67'	N80°03'18"E	27.65'

LINE TABLE

LINE	LENGTH	BEARING
L1	2.92'	N00°09'48"E
L2	24.60'	S36°49'25"W
L3	24.60'	N36°49'25"E
L4	15.43'	N53°10'35"W
L5	10.00'	N89°50'12"W
L6	25.23'	N00°13'36"E
L7	25.23'	S00°13'36"W
L8	49.88'	S89°46'24"E
L9	36.54'	S07°46'53"E



FND #4 REBAR  
NO CAP (DISTURBED,  
NOT ACCEPTED.  
N88°32'10"W, 2.07'  
FROM ACCEPTED  
CORNER)

FND #4 REBAR  
W/BC ILLEG.

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