



## **LOVELAND PLANNING COMMISSION MEETING AGENDA**

**Monday, June 08, 2015  
500 E. 3<sup>rd</sup> Street – Council Chambers  
Loveland, CO 80537**

THE CITY OF LOVELAND DOES NOT DISCRIMINATE ON THE BASIS OF DISABILITY, RACE, CREED, COLOR, GENDER, SEXUAL ORIENTATION, RELIGION, AGE, NATIONAL ORIGIN OR ANCESTRY IN THE PROVISION OF SERVICES. FOR DISABLED PERSONS NEEDING REASONABLE ACCOMODATIONS TO ATTEND OR PARTICIPATE IN A CITY SERVICE OR PROGRAM, CALL 962-2523 OR TDD 962-2620 AS FAR IN ADVANCE AS POSSIBLE.

### **I. CALL TO ORDER**

### **II. PLEDGE OF ALLEGIANCE**

### **III. REPORTS:**

#### **a. Citizen Reports**

This is time for citizens to address the Commission on matters not on the published agenda.

#### **b. Staff Matters**

#### **c. Committee Reports**

#### **d. Commission Comments**

### **IV. APPROVAL OF MINUTES**

#### **Review and approval of the May 11, 2015 Meeting minutes**

### **V. REGULAR AGENDA:**

#### **1. Plan of Development (DDA)**

The Plan of Development (DDA Plan) for the Loveland Downtown Development Authority (DDA) is required by State law to be reviewed by the Planning Commission and a recommendation provided to the City Council. The DDA Plan is defined as a plan for the development or redevelopment of the DDA District over a thirty to fifty year period. After receipt of the Planning Commission recommendation, the City Council will hold a public hearing on July 7, 2015 and thereafter consider a resolution approving the DDA Plan. The DDA may not undertake any development project until the City Council has approved the DDA Plan. Review of this Plan does not require a public hearing.

## **2. Mariana Butte 25<sup>th</sup> Subdivision Preliminary Plat Extension Request**

This request requires quasi-judicial review by the Planning Commission to consider extension of the Preliminary Plat for Mariana Butte 25<sup>th</sup> Subdivision (Mountain Gate) for an additional two-year period. In February of 2012, the Preliminary Plat and Preliminary Development Plan (PDP) were approved by the City for 51 lots (46 paired single-family units and 5 detached single-family units). The Preliminary Plat and PDP were extended previously for a two year period. The 34-acre property is located at the northwest corner of West. 1<sup>st</sup> Street and Namaqua Avenue. The Planning Commission has final authority on this matter.

## **3. Giuliano 4<sup>th</sup> Subdivision - Preliminary Development Plan, Preliminary Subdivision Plat, and Vacation of Rights-of-Way**

The application requires a public hearing and quasi-judicial approval by the Planning Commission for the project to be developed. The Preliminary Development Plan (PDP) and Preliminary Subdivision Plat (PP) require quasi-judicial review. Vacation of Rights-of-Way is considered a legislative matter. The applications for a PDP and PP propose to develop 36 single-family lots on 29.7 acres. In conjunction, a request to vacate Fife Court and a portion of Cascade Avenue are being proposed because the currently platted rights-of-way do not align with the proposed development. New rights-of-way would be re-established with the subdivision plat as presented. The Planning Commission has final authority over the PDP and PP; the Commission must make a recommendation to the City Council on the right-of-way vacation request.

## **4. Update: Site Plan Review Process for Public Schools**

Current Planning staff has been working with representatives of Thompson Schools to develop an agreed upon process for City review of site plans for new schools and other School District projects. Planning staff and School District representatives are bringing forward a draft proposal for review and direction by the Commission.

# **VI. ADJOURNMENT**

# CITY OF LOVELAND

## PLANNING COMMISSION MINUTES

**May 11, 2015**

---

A meeting of the City of Loveland Planning Commission was held in the City Council Chambers on May 11, 2015 at 6:30 p.m. Members present: Chairman Crescibene; and Commissioners Middleton, Meyers, Molloy, Forrest, Ray, and Jersvig, Members absent: Commissioners Dowding and McFall, City Staff present: Bob Paulsen, Current Planning Manager; Moses Garcia, Assistant City Attorney.

*These minutes are a general summary of the meeting. For more detailed information, audio and videotapes of the meeting are available for review in the Development Services office.*

### **CITIZEN REPORTS**

There were no citizen reports.

### **STAFF MATTERS**

1. **Mr. Paulsen, Current Planning Manager**, informed the commissioners that Beverly Walker had accepted a position with the City Clerk's office and Jenell Cheever will be filling in for the Planning Commission in the interim.
2. **Mr. Paulsen** reminded the commissioners that there is no meeting on May 25<sup>th</sup> in observance of Memorial Day.
3. **Mr. Garcia, Assistant City Attorney**, provided an update on upcoming staff changes within the City Attorney's office. Judy Schmidt will be leaving her position the end of May and Sharon Citino will also be leaving her position June 26th. Mr. Garcia will continue with his assignment to the Planning Commission and to the Current Planning office.
4. **Mr. Paulsen** notified the commissioners that the resolution to City Council to seek direction certifying the Loveland Building Division as a Prequalified Building Department is scheduled for the 6/2/15 City Council meeting. He also noted that the City Council has referred back the Title 18 Code Enforcement Amendment to the Planning Commission for further review due to concerns with the code enforcement provisions.

**Mr. Garcia** noted that **Assistant City Attorney, Vincent Junglas**, will contact the Attorney General's office for interpretation of the Municipal Code and rules of procedure. **Commissioner Meyers** noted his concerns regarding the definition of a Peace Officer and the standards for training. He said that the City of Loveland should have been following the Sunrise Approach to classifying Peace Officers. **Commissioner Meyers** agreed with **Mr. Paulsen** that the issue will not need to go back to the Title 18 Committee, but instead go directly to the Planning Commission.

5. **Commissioner Meyers** noted that at the May 5<sup>th</sup> City Council meeting, some City Councilors received draft minutes and not the approved minutes by the Planning

Commission. **Mr. Paulsen** noted that the updated minutes were distributed to City Councilors; however, some councilors were unaware that they had received updated minutes.

### **COMMITTEE REPORTS**

1. **Commissioners Meyers and Molloy** will attend the Title 18 Committee meeting on May 14<sup>th</sup>. **Mr. George, Development Services Director**, will discuss land use in the restricted zones associated with oil and gas. **Mr. Paulsen** noted that he will forward any notes from the meeting to the Planning Commission.
2. **287 Advisory Committee: Commissioner Forrest** is unable to attend the follow-up meeting/work-session with the Urban Land Institute on May 20<sup>th</sup> and asked if anyone would like to attend in her place. **Commissioner Molloy** volunteered to attend.

### **COMMISSIONER COMMENTS**

1. **Commissioner Ray** had concerns with reclassifying Code Enforcement Officers to Peace Officers as this is not the standard in the surrounding communities. He will need to evaluate if this change is necessary when the issue returns to Planning Commission for review.

**Chairman Crescibene** also noted that the presentation regarding the reclassification of Code Enforcement Officers needed to contain more facts instead of a “personal understanding” of the presenter, **Vincent Junglas**.

2. **Commissioner Ray** felt, in retrospect, that the Transportation office did not provide adequate answers regarding the redirection of traffic associated with the alleyway vacation for the proposed Sprouts; the alley vacation request was heard by the Planning Commission on April 13, 2015. Specifically, the cost of signs and difficulties with traffic enforcement should have been explained in more detail.

**Commissioner Molloy** commented that sometimes issues confronted in the vacation should have been addressed in the rezoning application. These issues, such as traffic, become a clean-up item with the right-of-way vacation request, and should have been analyzed with the earlier application.

**Commissioner Forrest** commented that the impact of vacating the alleyway needs to be considered for future developments, not just specifically in relation to Sprouts. A more thorough consideration of the land use and its function should be considered.

**Commissioner Meyers** agreed that the long term impact of parcel consolidation and rezoning on the neighborhood should be considered and not just the necessity of the vacation in relation to Sprouts.

3. **Commissioner Meyers** referenced a May 9<sup>th</sup> article in the Reporter Herald and asked for clarification of the expansion of Artworks by 10-12 units. **Mr. Paulsen** is not aware of a request for adding 10-12 new units and believes this article pertains to Artworks opening up 10-12 existing units. **Mr. Paulsen** noted that any request to expand the project so significantly cannot be approved administratively and would need to be approved through the Planning Commission. **Mr. Paulsen** will confirm this information and notify the Planning Commission of his findings.



## **APPROVAL OF THE MINUTES**

**Commissioner Molloy** made a motion to approve the April 27, 2015 minutes; upon a second from **Commissioner Forrest** the minutes were approved with six ayes and one abstention.

## **REGULAR AGENDA**

### **1. Development Center Update**

**Project Description:** Staff will provide a brief status update on the City's Development Center which will be located in the Fire Administration Building at 410 East 5<sup>th</sup> Street. Plans for renovating the building to provide integrated permitting services and accommodations for the entire Development Services Department are nearing completion.

**Bob Paulsen, Current Planning Manager**, presented the proposed concept for the new Development Center. The project will be out to bid in July with an anticipated finish date in early 2016. All of the Development Services Department, along with the Transportation Development Review office will move to the new center; reviewers from other departments will be at the new center on a part-time basis.

#### **Commissioner Questions and Comments:**

- **Commissioner Middleton** suggested that insulation be added above the suspended ceilings due to the close proximity of the fire trucks operating within the building.
- **Commissioner Meyers** expressed concerns with adequate conference rooms and meeting spaces and **Commissioner Crescibene** asked what the considerations for growth were. **Mr. Paulsen** stated that space availability within the building will be tight. Although meeting space is limited on the first floor, the Planning office will still be able to use the conference rooms in the Emergency Operation Center (EOC rooms) located on the second floor. Additionally, if expansion was needed, the basement could be renovated or an addition added onto the building at some future time.
- **Mr. Paulsen** stated that the move should have minimal disruptive effects on the daily operations as the new building will be completed prior to moving.

### **2. Economic Development Update**

**Project Description:** **Betsey Hale, Economic Development Director**, will give an update on Council approved economic incentive packages for various business entities throughout the years. Council approves agreements based on five year projections, so projects since 2010 have not yet completed the expected five year performance period.

In analyzing the economic incentives, there are two essential parts: the City support of projects and the economic benefit the City is looking to gain from the incentive.

#### **Commissioner Questions and Comments:**

- **Commissioner Meyers** asked what the average hourly wage for the proposed 1125 jobs and what opportunities there are for graduating students. **Ms. Hale** said she could

research the average wage for primary employment and average wage for retail and provide this information to the commissioners.

- **Commissioner Meyers** asked what the financial impact the opening of the Bass Pro Shop would have on businesses like Jax and Sportsman Warehouse. **Ms. Hale** stated that the effects on sales shifting from shops like these to the Bass Pro Shop are evaluated when considering an incentive. So far, an incentive has not been offered to Bass Pro Shop.
- **Commissioner Molloy** asked if the new Scheels sporting goods store received an incentive in Johnstown. **Ms. Hale** was not sure if they received any assistance but would share any information that she receives with the Planning Commission. She noted that Johnstown cannot use the URA code anymore due to changes in legislation and that may affect future developments.
- **Commissioner Molloy** commented that Centerra has multiple levels of taxes and wondered if these taxes might drive the competition to shop in other cities. **Ms. Hale** said that is possible and sales shifting can affect the city's revenues. Economic Development is researching how sales shifting to other cities can be reduced.
- **Commissioner Molloy** asked what the likelihood that all of the RTA-related projects mentioned in the presentation will actually coming to fruition. **Ms. Hale** said the likelihood is high because prior to applying you must actually show that you can build the project and what the financial gap is. However, ultimately would need to receive the RTA from the state in order for the projects to move forward.
- **Ms. Hale** provided an update on the Brinkman Project. She noted that it has been successful and that the project has been full since its opening. However, due to taxes being paid a year later you won't start seeing the revenue until 2015 or 2016.
- In regards to the HP Campus Redevelopment / RMCIT, **Ms. Hale** explained that there are several tenants considering the space and some smaller companies committed to moving into the space. Recently the city entered into a partnership with Loveland Development Fund and commissioned an evaluation for an Art and Design School that would encompass 200,000 square feet. **Commissioner Meyers** asked how close the site was to filling the projected 5000 jobs and **Ms. Hale** stated there are currently between 30-40 jobs created. Ms. Hale explained that this is partly due to the space being restricted for use only by technology jobs, limiting what businesses can occupy the site. **Commissioner Ray** asked for clarification for why call centers have been rejected as these tend to have an IT basis. **Ms. Hale** clarified that the call centers interested in the space are more outbound call centers focused on sales.
- **Commissioner Ray** asked how many of the 2500 jobs at the proposed Adventure Park are good jobs, defined as jobs that will pay greater than \$70,000. **Ms. Hale** stated the average for all jobs will be between \$30,000 -\$40,000; however, she will need to research how many positions are higher paying jobs versus entry level jobs.

**Chair Crescibene** called for a recess at 9:15 p.m.

**Chair Crescibene** called the meeting to order at 9:27 p.m.

- **Ms. Hale** commented that by creating the Adventure Park it will help make Loveland a destination center that will generate other income through overnight stays, food sales and retail sales. **Ms. Hale** stated that she is willing to come to another meeting with additional information on the economic impact and development in Loveland.

### 3. High Plains School Update

**Project Description:** **Dr. Dan Maas, Chief Operations Officer, Thompson Schools,** will provide a progress update on the K-8 High Plains School and address Commission questions.

**Dr. Maas** provided an update on the school design and site preparation efforts. He noted that there is a Construction Review Committee required by Board of Education policy whenever a school is built. The members are: Julie Piotraschke, Marc Seter, Candie Joshi, Scott Holzclaw, Christine Ricks, Nancy Rumfelt, Tiffany Miller, Lisa Riggensbach, Chastity Stringer, Jennifer Kingston, David Hallet. Ex-Officio members are: Dr. Carl Langer, Denise Montagu, Dr. Dan Maas, Sara Rasmussen, Matt Throop, Skip Armatoski, Noreen Smyth, Kim Perry.

#### **Commissioner Questions and Comments:**

- **Dr. Maas** provided samples of the building materials that would be used for the school. In light of concerns raised at the February 9, 2015, Planning Commission meeting, the Construction Review Committee considered a lighter color palate of materials but felt that this darker palate was actually more pleasing and inviting.
- **Dr. Mass** noted that the High Plains School has a smaller than typical site, but will utilize multi-purpose outside space that is shared with the city Parks Department. Some space will be public park space and other space that will be school property used as playground. **Dr. Maas** explained that a perimeter fence was not part of the design as the Parks Department asked that there not be a perimeter fence due to the dual nature of a playground and a community park. **Commissioner Ray** expressed concerns about not having a fence around the playground.
- **Dr. Maas** addressed the traffic concerns presented by the commissioners. Two separate studies were performed to redesign the bus loop to ensure safety. The new design separates buses from parent pick-up. It is anticipated that the school will be serviced by four to five buses. **Commissioner Meyers** recommended the school have more than one traffic guard.
- **Dr. Maas** discussed proposed boundary areas and the method used to determine the recommended school boundaries. The proposed boundary maps are posted on the Thompson School District website. The School Board will need to approve the school boundaries in August of 2015
- **Dr. Maas** discussed the landscape and said the design is basic due to cost and safety, utilizing mostly low level rock work and a planned learning garden. **Commissioner Malloy** expressed concerns with the extent of landscape plantings and indicated that the

landscape plan may initially be limited and would like to see some areas planned with more extensive landscaping in the future once funds are available.

- In response to **Commission Forrest** question, **Dr. Maas** said the school is seeking a gold level of LEED certification.
- **Commissioner Meyers and Commissioner Middleton** asked for clarification on what limits there might be when a commissioner is in going to a school board meeting to express concerns as a private citizen. **Mr. Garcia** said that the appearance of a conflict of interest needs to be considered when making statements as a private citizen when as a commissioner you are involved in making a recommendation on the issue. **Mr. Garcia** stated that when addressing an outside board or commission, you can communicate that you are a commissioner but you should not state that you are representing the opinion of the Planning Commission.

#### 4. Proposed City Budget Reductions

**Project Description:** On May 19<sup>th</sup> the City Council will be considering an ordinance reducing the 3% sales tax on food for home consumption (groceries). The reduction would be implemented over a 3-year period. In preparation of this public hearing, the Council has requested that the City Manager prepare a proposal for reductions to the City budget that would accommodate the loss in general fund revenues attributable to this tax. This is estimated to be approximately \$6 Million annually if the sales tax is reduced in full (eliminated).

The Planning Commission will be provided with information about the budget reduction proposal developed by City staff, and specifically provided with details about the reductions proposed for the Development Services Department. With this information, the Commission will be invited to provide a written statement in response to the proposed reductions to the City Council in advance of the May 19<sup>th</sup> public hearing.

##### **Commissioner Questions and Comments:**

- **Commissioner Crescibene** emphasized that the Commission is not going to discuss if they like the sales tax cut or not; rather, the discussion will focus on the effects to the city's budget and programs.
- **Mr. Greg George, Development Services Director**, provided an introduction to this matter, explaining that all City departments have developed a plan to accommodate revenue reductions that would occur if the sales tax on groceries is eliminated. **Mr. George** indicated that all boards and commissions were being given the opportunity to respond to the impacts that are proposed to occur to the City department that they are associated with. **Mr. George** focused his presentation on the proposed impacts to the Development Services Department. **Mr. George** stated that the Commissioners can either formulate a statement of their collective position (must be motioned and voted upon) and then submitted, in writing, to City Council by May 13<sup>th</sup> at 5:00pm. The second option is that as individuals each member can submit their position regarding the budget

cuts. All city staff must be totally neutral and therefore cannot provide the Planning Commission any direction on their response.

- **Mr. George** stated that most cuts are line item budget cuts that reduce such items as printing, supplies, etc. However, some cuts include staff positions not currently filled or positions that will be eliminated.
- **Commissioner Meyers** questioned why there was such a drastic cut proposed in 2016 versus a reduction implemented over 3 years. **Mr. George** explained that some of the budget cuts deal with vacant position which results in a larger budget reduction in the first year of the three year phase-in.
- Several Commissioners voiced concerns that being short staffed may negatively affect the turn-around time for processing development applications. **Mr. George** reiterated that it is up to the Planning Commission to decide if the proposed budget cuts are a good idea and what their recommendation to City Council will be.
- **Commissioner Meyers** questioned whether the consultant fees would be reduced. **Mr. Paulsen** said that the consultant budget for current projects would not be reduced and most expenditures will be complete prior to the end of 2015. **Mr. Paulsen** indicated that the proposed reductions are focused on eliminating ongoing expenditures, not one time expenses like specific consultant contracts.
- **Mr. George** stated that Development Services was asked to reduce their budget by almost 4%. **Chair Crescibene** asked where the remaining 96% percent of budget cuts would come from and **Mr. George** responded that a reduction similar to 4% across all other departments would be implemented. Since there are other departments larger than Development Services their 4% budget reduction will have a greater impact on the overall budget reduction package.
- **Mr. Paulsen** clarified that certain programs and services must be provided, including federally or state mandated program that cannot be cut. **Mr. George** stated programs that have a low mandate or a low cost recovery feature were looked at first to cut. In the event City Council does not implement the repeal of the food sales tax, there is no plan to implement these cuts.

**Chair Crescibene** called for a recess at 10:37 p.m.

**Chair Crescibene** called the meeting to order at 10:51 p.m.

Commissioners discussed the proposed budget reductions and formulated a recommendation for City Council.

*Commissioner Meyers, made a motion to approve these recommendations be submitted to the City Manager by the Chair. Upon a second by Commissioner Ray, the motion was unanimously approved.*

## **ADJOURNMENT**

**Chair Crescibene** adjourned the meeting at 11:40 p.m.

Approved by:\_\_\_\_\_

John Crescibene, Planning Commission Chair

\_\_\_\_\_  
Jenell Cheever, Planning Commission Secretary



## CITY OF LOVELAND

### ECONOMIC DEVELOPMENT OFFICE

Civic Center • 500 East Third • Loveland, Colorado 80537  
(970) 962-2304 • FAX (970) 962-2900 • TDD (970) 962-2620

---

**AGENDA ITEM:** REGULAR AGENDA ITEM #1  
**MEETING DATE:** 6/8/2015  
**TO:** City of Loveland Planning Commission  
**FROM:** Betsey Hale, Economic Development Director  
**PRESENTER:** Betsey Hale, Economic Development Director  
Representatives of the Loveland Downtown Development Authority

---

**TITLE:** A motion to recommend the City Council adoption of the Plan of Development for the Downtown Development Authority

**RECOMMENDED COMMISSION ACTION:** Approve the motion

---

#### **SUMMARY:**

The Plan of Development (DDA Plan) for the Loveland Downtown Development Authority (DDA) is required by State law to be reviewed by the Planning Commission and a recommendation thereon to be made to the City Council. The DDA Plan is defined as a plan for the development or redevelopment of the DDA District over a thirty to fifty year period. After receipt of the Planning Commission recommendation, the City Council will hold a public hearing on July 7, 2015 and immediately thereafter consider a resolution approving the DDA Plan. The DDA may not undertake any development project until the City Council has approved the DDA Plan.

---

#### **BACKGROUND:**

In January 2014 the Loveland City Council directed staff to work with the private sector to develop an organization that would lead downtown redevelopment efforts. In July 2014 the Loveland Downtown Partnership (LDP) was formed to act as in this capacity. The City has made a commitment to fund the Loveland Downtown Partnership for a period of 10 years. An election to form the DDA was held in February of 2015 and the voters approved the formation of the DDA.

The LDP (and its precursor, the “Downtown Working Group”), working with a variety of interested downtown individuals and groups, drafted and approved “A Strategic Plan for Revitalizing Downtown Loveland” (Strategic Plan) which has now been approved by the City Council as an official City downtown plan. The Strategic Plan, along with other applicable City downtown plans and policies (listed on page 9 of the DDA Plan) formed the basis of the DDA Plan, which was drafted by the LDP with assistance from City Staff. City Council reviewed and commented on the draft DDA Plan at a public meeting in December of 2104.

With regard to the planning components of the DDA Plan, it includes the DDA's objectives and purposes, a list of potential development and redevelopment projects, and contains the legal description and map of the DDA District.

The DDA Plan has been recommended for approval by the LDP Board and has been adopted by the DDA Board, subject to City Council approval.

---

**LIST OF ATTACHMENTS:**

1. Plan of Development

Links to documents referenced in the Plan of Development:

- Appendix III (link): [Downtown Vision Book \(2010\)](#)
- Appendix IV (link): [Destination Downtown: HIP Streets Master Plan \(2010\)](#)
- Appendix V (link): [Downtown Strategic Plan, Amendment to City's Comprehensive Plan \(2009\)](#)

2. A Strategic Plan for Revitalizing Downtown Loveland

3. Map of DDA District

Documents referenced in Appendix III, IV and V are available for review in the Economic Development office located at 500 E. Third Street, Suite 300, Loveland, CO 80537.



replace with  
**LOGO**

# LOVELAND DOWNTOWN DEVELOPMENT AUTHORITY

## Plan of Development



# Table of Contents

I.	Foreword .....	2
II.	The District .....	3
	District Map	
	Boundaries of the DDA	
III.	Objective and Purposes .....	5
IV.	Plan of Development Projects .....	6
V.	Strategic Downtown Plan .....	9
VI.	Methods of Financing Projects.....	10
VII.	Appendices .....	11

## Foreword

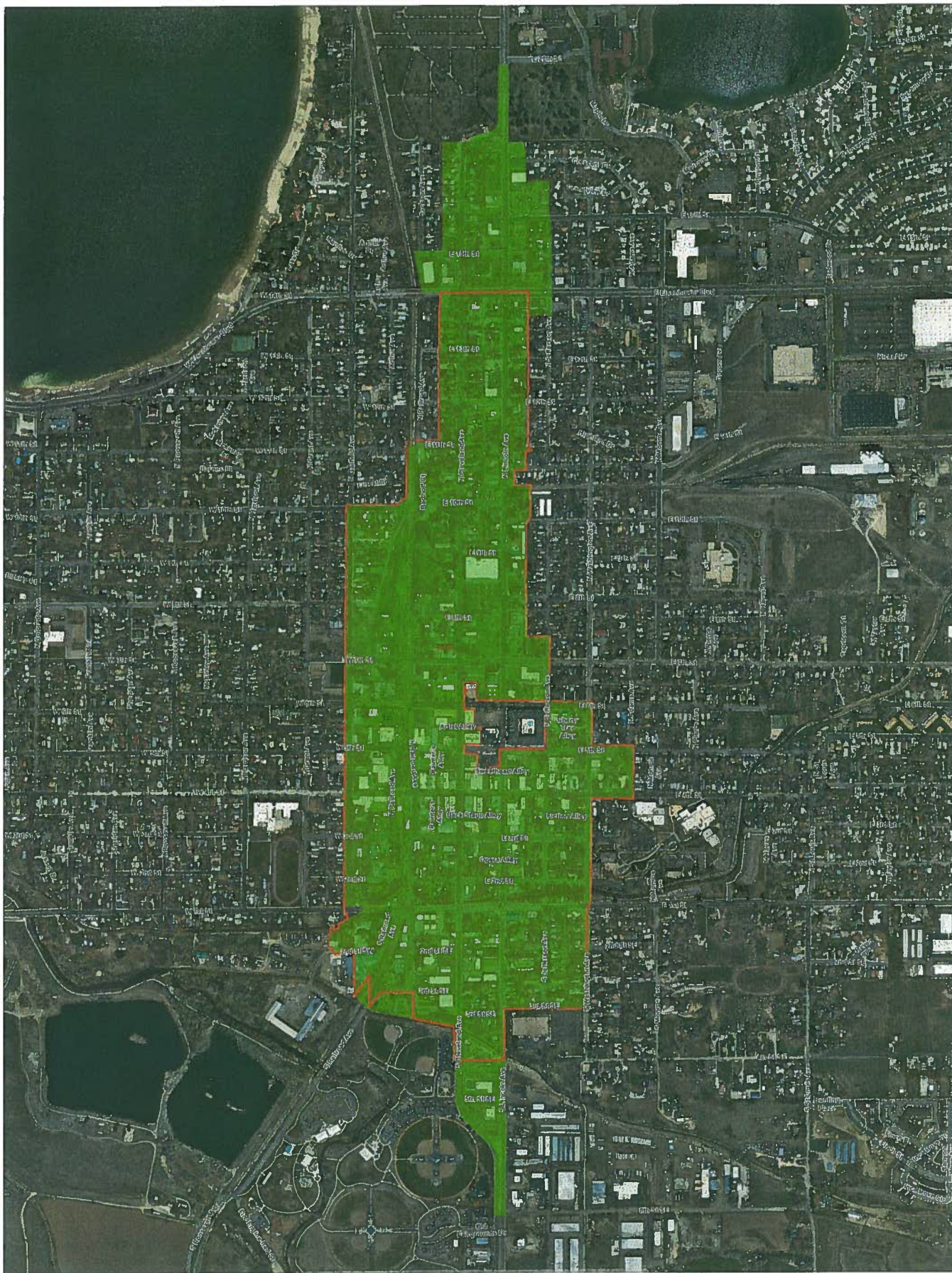
---

The downtown (“Downtown”) of the City of Loveland (“Loveland”) serves as the heart of a city that from its beginnings in 1877 has defined the best of Colorado’s entrepreneurial spirit and sense of civic pride, with an emphasis on arts and cultural activities. Downtown Loveland business and property owners believe that a strong economic foundation is critical to sustaining a vibrant community respectful of its history, committed to the full inclusion of all its citizens, and strategically positioned to thrive in a globally competitive marketplace.

This Plan of Development (the “Plan”) is an essential first legal step in creating a vibrant Downtown that provides a safe, dynamic environment to gather, live, educate, shop, work and play. The needs of the Downtown have been recognized over the past years in vision documents, comprehensive and strategic plans and master planning efforts. All of these have identified the need to have a strong Downtown for the economic health and future of Loveland.

The emphasis of this Plan is on needs of the Downtown over a thirty (30) to fifty (50) year period and the type of projects and programs that are required to satisfy those needs, rather than dictating the physical location, dimensions and design which can only evolve through continual planning efforts.





Loveland Downtown Development Authority  
**District Map**

- DDA approved by voters on February 10, 2015
- Downtown URA



NOTE: All calculations were made using best available data and should be considered approximate. Methodology statement available upon request.





## Boundaries of the DDA

The boundary of the Loveland Downtown Development Authority (the “DDA”), as shown on the preceding map, outlines the area in which the DDA will exercise its statutory powers (the “District”). The District was established on the basis of the best information available at the time. It is intended that the boundaries will change given changing times and circumstances. Property owners adjoining the District are encouraged to petition for inclusion if and when the uses and purposes of their properties become compatible with the purposes of the District.

The District is generally bounded by northbound Lincoln Avenue and southbound Cleveland Avenue; the eastern boundary of the District goes from the tip of the southern gateway, following Lincoln Avenue to 3<sup>rd</sup> Street SE, east 2 blocks to Washington Avenue, 6 blocks north to 4<sup>th</sup> Street, east one block to Adams Avenue, north to 5<sup>th</sup> Street, and then back west to Washington Avenue; then from the intersection of 5<sup>th</sup> Street and Washington Avenue, north one block and west one block to Jefferson Avenue, north 1½ blocks and west another ½ block, then continuing northward, including the properties that front on Lincoln Avenue, toward Eisenhower Boulevard, to 1/2 block south of Eisenhower Boulevard, then east to Jefferson Avenue, north to the alley one half block past 16<sup>th</sup> Street, west 190 feet, north to the boundary with Lakeside Cemetery, west to Lincoln Avenue, north to the end of the one way system, and from the tip of the northern gateway, the western boundary includes the properties on the west side of Cleveland Avenue heading south to 11<sup>th</sup> Street, then west to just past the railroad tracks, south on Railroad Avenue for one block, and again west on 10<sup>th</sup> Street to Garfield Avenue, then south 11 blocks to past 2<sup>nd</sup> Street SW to the intersection of Garfield Avenue and Railroad Avenue, then following the irrigation ditch southeast back to Cleveland Avenue and then south to the end of the one way system.

The legal description of the District is attached as Appendix I to this Plan.

# Objective and Purposes

The primary objectives of the DDA are to promote the safety, prosperity, security and general welfare of the District and its inhabitants, to prevent deterioration of property values and structures within the District, to prevent the growth of blighted areas within the District, to assist Loveland in the development, redevelopment and planning of the economic and physical restoration and growth of the District, to improve the overall appearance, condition and function of the District, to encourage a variety of uses compatible with the artistic and cultural community, to sustain and improve the economic vitality of the District, to promote the historic, artistic and cultural elements of the District, and to encourage pedestrian traffic and security in the District. To achieve these objectives, the specific goals of the DDA include the following and any other activities, plans, and development and redevelopment authorized by law.

The Plan recognizes that this is a long term revitalization strategy focused on implementing an entrepreneurial environment in which District products and services meet local demands and attract new residents and businesses to the area.

To achieve these objectives, the specific goals of the DDA include, but are not limited to the following:

1. Work with private entities, developers and property owners to promote positive investment in the District.
2. Work with business owners, and business entrepreneurs to promote retail growth, new job growth and other uses in the District.
3. Identify and help form collaborative public/private partnerships that promote economic growth in ways that honor and sustain strong community values.
4. Implement key elements of Loveland's approved infrastructure plan.
5. Increase residential and employment density as catalysts for enriching life for residents and visitors alike.
6. Assist emerging and existing businesses in navigating various local, county, and state regulations and taxing policies.
7. Identify and establish a communications process with current business and property owners within the District.
8. Establish multiple communication forums with emphasis on email, social media, and newspaper.
9. Work with Loveland in evaluating and potentially implementing a "One Stop" approach to Downtown development including identifying a potential organizational structure therefor.
10. Improve the visual attractiveness of the District including but not limited to façade renovations, public streets, alleys, curbs, gutters, sidewalks, lighting along with street furniture and landscaping.
11. Underground the utility systems.
12. Promote a diversity of activities in the District.
13. Promote and encourage the renovation and reuse of vacant and deteriorated structures within the District.
14. Encourage the creation and continuation of public events within the District
15. Promote and market the District.
16. Promote Loveland's unique identity as a destination for arts and culture.

## Plan of Development Projects

- A. Plan projects may include public facilities and other improvements to public or private property of all kinds consistent with the priorities of the DDA by all means permitted by federal, state and local laws and regulations, including but not limited to, land assemblage, demolition, removal, site preparation, construction, renovation, repair, remodeling, reconstruction purchase of property interests, rehabilitating, equipping, selling and leasing in connection with such public and private improvements.
- B. Descriptions of specific development projects that have been conceptually identified as potential key downtown redevelopment projects including, but not limited to, the following:

Item	City/DDA Funding	Other Public Funding	Private	Notes
Redevelopment Projects				
South Catalyst Project: 3 <sup>rd</sup> Street Site	\$15,000,000	\$15,000,000	\$50,000,000	Amount includes the estimate on land plus the cost of the redevelopment with a parking structure
4th Street/Rialto Square	\$2,500,000	\$0	\$7,500,000	Includes the cost of the land plus redevelopment cost
Arcadia (4th and Cleveland)	\$400,000	\$75,000	\$1,800,000	Based on preliminary review of proposed plan.
4th and Lincoln/Redevelopment	\$2,000,000	\$0	\$6,250,000	Potential project/timeline unknown
Elks Lodge	\$250,000	\$200,000	\$2,000,000	Estimates are for rehab at \$100/square foot
Loveland	\$250,000	\$200,000	\$2,000,000	Estimates are for rehab at \$100/square foot
VFW Hall	\$500,000	\$0	\$4,000,000	
Feed and Grain	\$0	\$2,300,000	\$1,000,000	
Pulliam Building	\$4,600,000	\$200,000	\$1,200,000	Assumes the City receives a grant from the State Historic Fund and Historic Tax Credits
House of Neighborly Service Building	\$500,000	\$0	\$5,000,000	Assumes a redevelopment of a 20,000 sq/foot building
Larimer County Building/6th and Cleveland	\$500,000	\$0	\$3,750,000	Assumes the City takes possession of the building as part of the South Catalyst
Safeway site	\$5,000,000	\$0	\$30,000,000	Requires further investigation
Railroad site	\$2,500,000	\$0	\$15,000,000	Land at 7th and Garfield



Other private	\$1,500,000		\$10,000,000	Includes other projects not contemplated plus façade grants and fire safety grants
<b>SUBTOTAL REDEVELOPMENT</b>	<b>\$35,500,000</b>	<b>\$17,975,000</b>	<b>\$139,500,000</b>	

- C. Descriptions of specific potential public facilities and improvements that have been conceptually identified to complement private developments including, but not limited to, the following:

Infrastructure				
4th Street/Phase I - 3 blocks	\$5,860,000			4th Street from Railroad to Jefferson
4th Street/Additional 2 blocks	\$2,500,000			4th Street to Garfield and Washington
3rd Street	\$2,250,000			3rd Street west of Cleveland to Feed and Grain
5th Street	\$3,010,000			5th Street from Lincoln to Railroad
5th Street Plaza	\$2,187,413			Museum plaza proposal in the parking lot at 5th and Lincoln
Power	\$5,000,000			Estimates are for \$300,000 per block to underground the power
Railroad Avenue 1st to 5th	\$4,000,000			May include connectivity with the trail system.
Cost Escalation	\$3,161,483			Estimates were completed in 2009, the number is 20 percent of the cost of the streetscape improvements
<b>SUBTOTAL INFRASTRUCTURE</b>	<b>\$27,968,896</b>	<b>\$0</b>	<b>\$0</b>	

- D. Other specific development projects and public facilities currently contemplated are as follows:

Other Capital Cost				
Trail Expansion	\$1,000,000			
Quiet Zones	\$1,000,000	\$2,000,000		Includes five rail crossings from Railroad through 22nd Street



<b>SUBTOTAL OTHER</b>	<b>\$2,000,000</b>	<b>\$2,000,000</b>	<b>\$0</b>	

E. The DDA also may seek to support other projects not directly identified above including, but not limited to, the following:

- a. Beautification programs;
- b. Pedestrian facilities and circulation improvements;
- c. Parking that is not otherwise included within specific projects (IE 3<sup>rd</sup> Street Catalyst); and
- d. Downtown hotel or other convention facilities built in conjunction with a private development.

# Strategic Downtown Plan

The DDA, acting in coordination with the Loveland Downtown Partnership and Loveland, will need to establish short and long term priorities based on adopted strategic plans and identified development projects as such plans and projects evolve. The current plans, which are referenced below and attached as Appendices II through V to this Plan, are as follows:

## **Appendix II:**     *A Strategic Plan for Revitalizing Downtown Loveland (2014)*

The plan, adopted by the Loveland City Council and the Loveland Downtown Partnership, provides the comprehensive outline for short and long term success in Downtown Loveland.

A Strategic Plan for Revitalizing Downtown Loveland is driven by the following principles:

1. We are committed to a process driven by community stakeholders and supported by the City of Loveland.
2. We are committed to a long term revitalization strategy (20 yrs.) that combines immediate action to improve communications and marketing with an ongoing responsibility to maintain and improve the downtown infrastructure.
3. We are committed to shaping policies and procedures that provide adequate flexibility for the organization to respond quickly and effectively to changing conditions at the local, state, national, and/or international levels.
4. We are committed to implementing an entrepreneurial environment in which Downtown products and services meet local demands and attract new residents and businesses to the area.
5. We are committed to shaping collaborative public/private partnerships that promote economic growth in ways that honor and sustain strong community values.

## **Appendix III:**     *Downtown Vision Book (2010)*

The purpose of the Downtown Vision Book is to highlight catalyst projects, and describe the context, character and the opportunity for revitalization. In addition, the Downtown Vision Book identifies ideas, opportunities and strategies to further benefit Downtown Loveland. The Private-Public projects are designed to capture not only the value of public participation, but to be a catalyst for private investment, enhanced connections and enrichment of the community experience for residents, businesses and visitors alike.

## **Appendix IV:**     *Destination Downtown: HIP Streets Master Plan (Infrastructure Plan) (2010)*

The Infrastructure Plan was completed in 2009 highlights the streetscape, utility and other public infrastructure improvements in Downtown Loveland.

## **Appendix V:**     *Downtown Strategic Plan – Amendment to the City’s Comprehensive Plan (2009)*

The plan, adopted by the Loveland City Council as an amendment to the Comprehensive Plan, was the basis for the effort by Loveland and the Loveland Downtown Team to revitalize the Downtown.

# Methods of Financing Projects

In order to finance the projects and purposes of the DDA, the following financial sources are authorized to be utilized:

- A. Proceeds of bonds of, loans or advances to, or indebtedness incurred by Loveland secured by the pledge of the following tax revenues for the maximum period of time authorized by C.R.S. § 31-25-807(3):
  - a. **Property Tax Increment:** All of that portion of property taxes in excess of such taxes which are produced by the levy at the rate fixed each year by or for any public body upon the valuation for assessment of taxable property within the boundaries of the District last certified prior to the effective date of approval by the City Council of Loveland of this Plan or, as to an area later added to the boundaries of the District, the effective date of the modification of this Plan.
  - b. **Municipal Sales Tax Increment:** All of that portion of municipal sales tax in excess of such taxes collected within the boundaries of the District for the twelve month period ending on the last day of the month prior to the effective date of approval by the City Council of Loveland of this Plan. For purposes of calculating the amount of municipal sales tax, "municipal sales tax" shall be as defined in Section 3.16.010 and Section 3.16.020A of the Loveland Municipal Code, provided that such definition shall specifically not include sales tax on the sale of food for home consumption as defined in C.R.S. § 39-26-102(4.5)(a), with the exception of candy and soda as defined in C.R.S. § 39-26-707(1.5) which shall be included in the definition of municipal sales tax.
  - c. **Other sources:** Such other sources of revenue for repayment of bonds, loans, advances or other indebtedness of Loveland as may be authorized by law.

*All such taxes described in this paragraph A shall be adjusted, collected, allocated and used as set forth in C.R.S. § 31-25-807(3), as amended from time to time.*

- B. Membership fees
- C. Private contributions
- D. Proceeds of loans to the DDA
- E. Fees and other charges imposed in connection with projects undertaken by the DDA
- F. Grants and other funds made available by public agencies and other entities
- G. All types of bond issues, including industrial development revenue and special assessment bonds, and
- H. All such other sources and methods as may be authorized by law from time to time, including but not limited to, C.R.S. § 31-25-801, et seq.

## Appendix I

### Legal Description of the District

Beginning at the point of intersection of the south right-of-way (ROW) line of E. 4<sup>th</sup> Street and the east ROW line of N. Washington Avenue; thence southerly along said east ROW line to its point of intersection with the north ROW line of E. 3<sup>rd</sup> Street; thence continuing southerly to the point of intersection of the south ROW line of E. 3<sup>rd</sup> Street and the east ROW line of N. Washington Avenue; thence continuing southerly along said east ROW line to its point of intersection with the north ROW line of E. 1<sup>st</sup> Street; thence southwesterly to the point of intersection of the south ROW line of E. 1<sup>st</sup> Street and the east ROW line of S. Washington Avenue; thence southerly along said east ROW line its point of intersection with the north ROW line of the alley between E. 1<sup>st</sup> Street and 2<sup>nd</sup> Street S.E.; thence continuing southerly to the point of intersection of the south ROW line of said alley and the east ROW line of S. Washington Avenue; thence continuing southerly along said east ROW line its point of intersection with the north ROW line of 2<sup>nd</sup> Street S.E.; thence continuing southerly to the point of intersection of the south ROW line of 2<sup>nd</sup> Street S.E. and the east ROW line of S. Washington Avenue; thence continuing southerly along said east ROW line to its point of intersection with the south ROW line extended of 3<sup>rd</sup> Street S.E.; thence westerly along said extended line to the point of intersection of the west ROW line of S. Washington Avenue and the south ROW line of 3<sup>rd</sup> Street S.E.; thence continuing westerly along said south ROW line to its point of intersection with the east ROW line of S. Lincoln Avenue; thence southerly along said east ROW line to its point of intersection with the north line extended of the 5<sup>th</sup> Street S.E. ROW; thence continuing southerly along said east ROW line to its point of intersection with the south line of the 5<sup>th</sup> Street S.E. ROW line; thence southerly along said east ROW line to its point of intersection with the north line of the 8<sup>th</sup> Street S.E. ROW; thence continuing southerly along said east ROW line to its point of intersection with the south line of the 8<sup>th</sup> Street S.E. ROW; thence westerly along the south line extended of the 8<sup>th</sup> Street S.E. ROW to the west line of the S. Lincoln Avenue ROW; thence northerly along the west ROW line of S. Lincoln Avenue to its point of intersection with the southwest line of the S. Cleveland Avenue ROW; thence continuing northwesterly along said southwest ROW line to its point of intersection with the south line of the 5<sup>th</sup> Street S.E. ROW; thence northerly along the west line of the S. Cleveland Avenue ROW to its point of intersection with the north line of the 5<sup>th</sup> Street S.E. ROW; thence continuing northerly along said west ROW line of S. Cleveland Avenue to its intersection with the north bank of the Farmer's Ditch; thence northwesterly along said bank to its point of intersection with the west ROW line of the Burlington Northern/Santa Fe Railroad and the south line of Henrikson Addition; thence continuing northwesterly along said south line to the southwest corner of Henrikson Addition; thence northerly along the west line of said Henrikson Addition to its point of intersection with the south ROW line of 2<sup>nd</sup> Street S.W.; thence westerly along said south ROW line to the NW corner of Mill First Addition; thence northerly perpendicular to said ROW line to a point on the south line of Mill Second Addition; thence westerly along said south line to the SW corner of Mill Second Addition; thence northerly and easterly along the west line of said Mill Second Addition to the NW corner thereof; thence easterly and southerly along the north line of Mill Second Addition to the NE corner thereof; thence northwesterly to the SE corner of Riverside Addition; thence northerly along the east line of Riverside Addition to its point of intersection with the south ROW line of W. 1<sup>st</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of W. 1<sup>st</sup> Street and the west ROW line of the N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of W. 2<sup>nd</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of W. 2<sup>nd</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of W. 3<sup>rd</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of W. 3<sup>rd</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly to the point of intersection of the south ROW line of the alley between W. 3<sup>rd</sup> Street and W. 4<sup>th</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly to the point of intersection of the north ROW line of said alley and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of W. 4<sup>th</sup> Street; thence continuing northerly to the point



of intersection of the north ROW line of W. 4<sup>th</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of the alley between W. 4<sup>th</sup> Street and W. 5<sup>th</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly to the point of intersection of the north ROW line of said alley and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of W. 5<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of W. 5<sup>th</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of W. 6<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of W. 6<sup>th</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of W. 7<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of W. 7<sup>th</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of the alley between W. 7<sup>th</sup> Street and W. 8<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of said alley and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of W. 8<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of W. 8<sup>th</sup> Street and the west ROW line of N. Garfield Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of W. 10<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of W. 10<sup>th</sup> Street and the west ROW line of N. Garfield Avenue; thence easterly to the point of intersection of the east ROW line of N. Garfield Avenue and the north ROW line of W. 10<sup>th</sup> Street; thence easterly and northeasterly along said north ROW line to its point of intersection with the west ROW line of the Burlington Northern/Santa Fe Railroad; thence northerly along said west ROW to its point of intersection with the south ROW line of the alley between W. 10<sup>th</sup> Street and W. 11<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of said alley and the west ROW line of the Burlington Northern/Santa Fe Railroad; thence continuing northerly along said west ROW line to its point of intersection with the south line of Little Barnes Ditch; thence continuing northerly to the point of intersection of the north line of said Ditch and the west ROW line of the Burlington Northern/Santa Fe Railroad; thence northerly along said west ROW line to its point of intersection with the south ROW line of E. 11<sup>th</sup> Street; thence continuing northerly to the point of intersection of the west ROW line of said Railroad and the north ROW line of W. 11<sup>th</sup> Street; thence northeasterly to the point of intersection of the east ROW line of N. Railroad Avenue and the north ROW line of E. 11<sup>th</sup> Street; thence easterly along said north ROW line to its point of intersection with the west ROW line of the alley between N. Railroad Avenue and N. Cleveland Avenue; thence northerly along said west ROW line to its point of intersection with the south ROW line of the alley between E. 11<sup>th</sup> Street and E. 12<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of the alley between E. 11<sup>th</sup> Street and E. 12<sup>th</sup> Street and the west ROW line of the alley between N. Railroad Avenue and N. Cleveland Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of E. 12<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of E. 12<sup>th</sup> Street and the west ROW line of the alley between N. Railroad Avenue and N. Cleveland Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of the alley between E. 12<sup>th</sup> Street and E. 13<sup>th</sup> Street; thence continuing northerly to the point of intersection of the north ROW line of the alley between E. 12<sup>th</sup> Street and E. 13<sup>th</sup> Street and the west ROW line of the alley between N. Railroad Avenue and N. Cleveland Avenue; thence continuing northerly along said west ROW line to its point of intersection with the south ROW line of the alley between E. 13<sup>th</sup> Street and E. Eisenhower Boulevard; thence continuing northerly to the point of intersection of the north ROW line of said alley between E. 13<sup>th</sup> Street and E. Eisenhower Boulevard and the west ROW line of the alley between N. Railroad Avenue and N. Cleveland Avenue; thence continuing northerly along said west ROW line to the point of intersection of the east line of Lot 21, Block 4, Loveland Heights Addition and the south line of the vacated alley ROW; thence easterly along said south line to the centerline of the vacated alley ROW; thence northerly along said centerline to its point of

intersection with the south ROW line of E. Eisenhower Boulevard; thence continuing northerly along the west line extended of said Lots to its point of intersection with the centerline of E. Eisenhower Avenue; thence westerly along said centerline, to its point of intersection with the west ROW line of the Burlington Northern/Santa Fe Railroad; thence northwesterly along said west ROW line to its point of intersection with the north line extended of the E. 15<sup>th</sup> Street ROW; thence easterly along said north line extended to its point of intersection with the west ROW line of Jackson Avenue; thence easterly along said north line extended of the East 15<sup>th</sup> Street ROW to its point of intersection with the east ROW line of Jackson Avenue; thence continuing easterly along the north ROW of E. 15<sup>th</sup> Street to its point of intersection with the east ROW line of the alley between Jackson Avenue and N. Lincoln Avenue; thence northerly along said east ROW of the alley to its point of intersection with the south ROW line of E. 16<sup>th</sup> Street; thence northerly along said east ROW of the alley to its point of intersection with the north ROW line of E. 16<sup>th</sup> Street; thence continuing northerly along said east ROW of the alley to its point of intersection with the southern property line of the Loveland Burial Park Cemetery; thence easterly along said southern property line to its point of intersection with the west ROW line of N. Cleveland Avenue; thence northeasterly along the northwestern ROW line of N. Cleveland Avenue to its point of intersection with the west ROW line of N. Lincoln Avenue; thence northerly along said west ROW line to its point of intersection with the south line extended of the E. 20<sup>th</sup> Street ROW; thence easterly along said south line extended to its intersection with the east ROW line of N. Lincoln Avenue; thence southerly along said east ROW line to its point of intersection with the south boundary line of the Stephenson 1<sup>st</sup> Subdivision; thence easterly along said south boundary line to its point of intersection with the west boundary of the Conger Subdivision of the North End Addition; thence southerly along said west boundary line to its intersection with the south boundary of the Conger Subdivision of the North End Addition; thence easterly along said south boundary to its intersection with the west boundary line of the Grandview Subdivision of North End Addition; thence southerly along said west boundary line to its intersection with the north ROW line of E. 16<sup>th</sup> Street; thence southeasterly across E. 16<sup>th</sup> Street to the point of intersection of the south ROW line of E. 16<sup>th</sup> Street and the east ROW line of N. Jefferson Avenue; thence southerly along said east ROW line to its point of intersection with the north ROW line of E. Eisenhower Blvd.; thence southerly along said east ROW line to its point of intersection with the south ROW line of E. Eisenhower Blvd; thence continuing southerly along said east ROW line to its point of intersection with the south boundary line extended of the WARNOCK ADD AMD L1-4 35-39 & POR L40 & VACATED ALLEY Subdivision; thence westerly along said south boundary line extended to its intersection with the west ROW line of N. Jefferson Avenue; thence westerly along said south boundary line to its point of intersection with the west ROW line extended of the alley between N. Lincoln Avenue and N. Jefferson Avenue; thence southerly along said west alley line to its point of intersection with the north ROW line of E. 13<sup>th</sup> Street; thence continuing southerly to the point of intersection of said west alley line and the south ROW line of E. 13<sup>th</sup> Street; thence continuing southerly along said west alley line to its points of intersection with the north ROW line of E. 12<sup>th</sup> Street; thence continuing southerly to the point of intersection of said west alley line with the south ROW line of E. 12<sup>th</sup> Street; thence continuing southerly along said west alley line to its point of intersection with the north line of Little Barnes Ditch; thence continuing southerly to the point of intersection of said west alley line and the south line of said Ditch; thence continuing southerly along said west alley line to its point of intersection with the centerline of the alley ROW vacated via Ordinance 3317 and recorded at Reception Number 86051452 adjoining Block 2, Lincoln Place Addition; thence easterly along the centerline of said vacated alley to its point of intersection with the east line of Lot 10, Block 2, Lincoln Place Addition; thence southerly along said east line 20 feet to a point; thence westerly perpendicular to said east line to a point on the east line of Lot 11, Block 2, Lincoln Place Addition; thence southerly along the east line of said Lot 11 to its point of intersection with the north ROW line of E. 11<sup>th</sup> Street; thence continuing southerly to the point of intersection of the east line of Lot 11, Block 3, Lincoln Place Addition and the south ROW line of E. 11<sup>th</sup> Street; thence westerly along said south ROW line to its point of intersection with the east line of Lot 13, Block 3, Lincoln Place Addition; thence southerly along said east line to its point of intersection with the north ROW line of the Great Western/Omni Railroad; thence easterly along said north ROW line to its point of intersection with the east line of Lot 10, Block 3, Lincoln Place Addition; thence southerly to the point of intersection of the east line of Lot 2, Block 5, Orchard Park Addition and the south ROW line of said Railroad; thence continuing southerly along the east line of said Lot 2 to the NE corner of Lot 1, Block 5, Orchard Park; thence continuing south



along the east line of said Lot 1 to its point of intersection with the north ROW line of E. 10<sup>th</sup> Street; thence southwesterly to the point of intersection of the south ROW line of E. 10<sup>th</sup> Street and the east ROW line of the alley between N. Jefferson Avenue and N. Lincoln Avenue; thence southerly along said east ROW line to its point of intersection with the north ROW line of E. 8<sup>th</sup> Street; thence continuing southerly to the point of intersection of the south ROW line of E. 8<sup>th</sup> Street and the east ROW line of the alley between N. Jefferson Avenue and N. Lincoln Avenue; thence southerly along said east ROW line to its point of intersection with the north ROW line of the alley between E. 8<sup>th</sup> Street and E. 7<sup>th</sup> Street; thence easterly along said north ROW line to its point of intersection with the west ROW line of N. Jefferson Avenue; thence continuing easterly to the point of intersection of said north ROW line and the east ROW line of N. Jefferson Avenue; thence southerly to the point of intersection of the south ROW line of said alley and the east ROW line of N. Jefferson Avenue; thence continuing southerly along said east line to its point of intersection with the north ROW line of E. 7<sup>th</sup> Street; thence continuing southerly to the point of intersection of the south ROW line of E. 7<sup>th</sup> Street and the east ROW line of E. Jefferson Avenue; thence continuing southerly along said east line to its point of intersection with the north ROW line of the alley between E. 7<sup>th</sup> Street and E. 6<sup>th</sup> Street; thence continuing southerly to the point of intersection of the south ROW line of said alley and the east ROW line of N. Jefferson Avenue; thence continuing southerly along said east line to its point intersection with the north ROW line of E. 6<sup>th</sup> Avenue; thence easterly along said north line to its point intersection with the west ROW line of N. Washington Avenue; thence continuing easterly to the point intersection of the north ROW line of E. 6<sup>th</sup> Avenue and the east ROW line of N. Washington Avenue; thence southerly to the point of intersection of the south ROW line of E. 6<sup>th</sup> Avenue and the east ROW line of N. Washington Avenue; thence southerly along said east ROW line to its point of intersection with the north ROW line of the alley between E. 6<sup>th</sup> Street and E. 5<sup>th</sup> Street; thence continuing southerly to the point of intersection of the south ROW line of said alley and the east ROW line of N. Washington Avenue; thence continuing southerly along said east line to its point of intersection with the north ROW line of E. 5<sup>th</sup> Street; thence easterly along said north ROW line to its point of intersection with the west ROW line of N. Adams Avenue; thence continuing easterly to the point of intersection of the north ROW line of E. 5<sup>th</sup> Street and the east ROW line of N. Adams Avenue; thence southerly to the point of intersection of the south ROW line of E. 5<sup>th</sup> Street and the east ROW line of N. Adams Avenue; thence southerly along said east ROW line to its point of intersection with the north ROW line of E. 4<sup>th</sup> Street; thence continuing southerly to the point of intersection of the east ROW line of N. Adams Avenue and the south ROW line of E. 4<sup>th</sup> Street; thence westerly to the point of intersection of the west ROW line of N. Adams Avenue and the south ROW line of E. 4<sup>th</sup> Street; thence continuing westerly along said south ROW line to the Point of Beginning;

And,

Less [County building parcel] LOTS 13 THRU 16, BLK 7, City of Loveland, County of Larimer, State of Colorado; ALSO POR VACATED ALLEY PER BK 1712 PG 733; and [Former Home State Bank parcel] LOTS 1 THRU 8, BLK 12, City of Loveland, County of Larimer, State of Colorado; and [Museum parcel] LOTS 19-24, BLK 12, City of Loveland, County of Larimer, State of Colorado; and [Vacant Parking Lot parcel] LOTS 1-7, LESS S 25 FT LOTS 1-3 AND LESS S 25 FT OF E 5 FT LOT 4, BLK 13, City of Loveland, County of Larimer, State of Colorado; and [Lincoln Place parcel] The subdivision LINCOLN PLACE COMMUNITY, City of Loveland, County of Larimer, State of Colorado (20100069697) in its entirety (formerly known as Block 41 of Finley's Addition, City of Loveland, County of Larimer, State of Colorado), and [Street & Alley ROW] The full right-of-way of East 6th Street east of the easterly boundary line of the N. Cleveland Avenue right-of-way and west of the centerline of the N. Jefferson Avenue right-of-way, City of Loveland, County of Larimer, State of Colorado; and All public alley right-of-way within BLK 12, City of Loveland, County of Larimer, State of Colorado; and The full right-of-way of East 5th Street east of the easterly boundary line of the N. Cleveland Avenue right-of-way and west of the westerly boundary line of the N. Lincoln Avenue right-of-way, City of Loveland, County of Larimer, State of Colorado; and The north half of the street right-of-way of the intersection of East 5th Street and N. Lincoln Avenue, north of the centerline of East 5th Street, City of Loveland, County of Larimer, State of Colorado; and The north half of the right-of-way of East 5th Street north of the centerline of East 5th Street, east of the easterly boundary of the N. Lincoln Avenue right-of-way, and west of the

centerline of the N. Jefferson Avenue right-of-way, City of Loveland, County of Larimer, State of Colorado; and The west half of the street right-of-way of N. Jefferson Avenue south of the southerly boundary of East 6th Street, and north of the northerly boundary of E 5th Street, City of Loveland, County of Larimer, State of Colorado.

DRAFT





# A Strategic Plan for Revitalizing Downtown Loveland

## Introduction

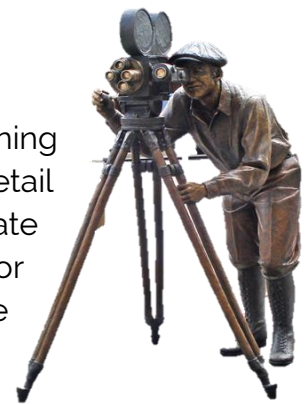
Downtown Loveland serves as the heart of a City that from its beginnings in 1877 has defined the best of Colorado's entrepreneurial spirit and sense of civic pride. From its agricultural roots in sugar beets and cherry orchards to today's thriving arts community, Downtown Loveland continues to offer diverse day time activities and great nightlife within a family-friendly environment. Today's Downtown offers a wide array of businesses, retail shops, galleries, cafes, breweries and restaurants within walking distance of our historic Rialto Theatre Center.



Downtown business and property owners believe that a strong economic foundation is critical to sustaining a vibrant community respectful of its history, committed to the full inclusion of all its citizens, and strategically positioned to thrive in a globally competitive marketplace. With this background in mind, we have developed the following strategic plan for revitalizing the Downtown as the heart of creative excellence in Northern Colorado.

## Our Vision

Our residents enjoy the convenience of shopping, working, and learning in a pedestrian-friendly city-center. Visitors find restaurants, retail shops, and entertainment easily accessible by public transit or private vehicle. The presence of local residents at all hours of the day or night demonstrates the Downtown's commitment to friendly service in a clean, safe environment.



## Our Mission

To create a vibrant Downtown that provides a safe, dynamic environment to gather, live, educate, shop, work, and play.

## Guiding Principles

This Downtown Plan for Revitalization is driven by the following principles:

1. We are committed to a process driven by community stakeholders and supported by the City of Loveland.
2. We are committed to a long term revitalization strategy (20 yrs.) that combines immediate action to improve communications and marketing with an ongoing responsibility to maintain and improve the downtown infrastructure.
3. We are committed to shaping policies and procedures that provide adequate flexibility for the organization to respond quickly and effectively to changing conditions at the local, state, national, and/or international levels.
4. We are committed to implementing an entrepreneurial environment in which Downtown products and services meet local demands and attract new residents and businesses to the area.
5. We are committed to shaping collaborative public/private partnerships that promote economic growth in ways that honor and sustain strong community values.



## Goals

1. To create a sustainable Downtown Development Authority (DDA) within twelve (12) months and determine the feasibility of a Business Improvement District within two (2) years.
2. To create an event and marketing plan in cooperation with the Community Marketing Commission, Loveland Chamber of Commerce and other entities that promotes a unique Downtown brand.
3. To implement key elements of the approved (City of Loveland) infrastructure plan by commencing construction over the next twenty-four (24) months and completing within five years, in ways that complement and preserve the Downtown's historic and cultural heritage.
4. To increase residential and employment density as catalysts for enriching life for residents and visitors alike.
5. To assist emerging and existing businesses in navigating various local, county, and state regulations and taxing policies.



## Action Plan



The Downtown Working Group will take the following actions in order to meet the strategic goals listed above.

**1. Create a sustainable Downtown Development Authority (DDA) within twelve (12) months and determine the feasibility of a Business Improvement District within two (2) years.**

1.1 Establish geographically and economically appropriate boundaries for the Downtown that allow for future residential and employment growth.

- 1.2 Identify and establish a communications process with current business and property owners within the defined geographic boundaries.
- 1.3 Establish multiple communication forums with emphasis on email, social media, and newspaper.
- 1.4 Prepare an organizational structure for a "One Stop" approach to Downtown development.
- 1.5 Based on this strategic plan, develop and present a DDA plan to the Loveland City Council in order to seek an election to create a Downtown Development Authority.

**2. Create an event and marketing plan in cooperation with the Community Marketing Commission, Loveland Chamber of Commerce and other entities to promote a unique Downtown brand.**



- 2.1 Consolidate existing community business and economic development groups where appropriate to alleviate overlaps and inefficiencies.
- 2.2 Cross-load membership/representation among service and advisory groups to improve communications and effectiveness.
- 2.3 Convene groups of local community and business people to identify existing cultural and economic strengths.
- 2.4 Coordinate events around a common theme/brand that sets the Downtown apart from other local communities.
- 2.5 Build local interest in the Downtown as a catalyst to attracting new residents, businesses, and visitors.



- 3. Implement key elements of the approved (City of Loveland) infrastructure plan by commencing construction over the next twenty-four (24) months and completing within five years, in ways that complement and preserve the Downtown's historic and cultural heritage.**



- 3.1 Create a user-friendly summary with completed and planned infrastructure projects.
- 3.2 Hold small group meetings with residents, business owners, and other interested groups to discuss the implications of each project as well as the links between and among those projects.
- 3.3 Gather community input regarding the balance between economic development and preserving local history and culture.
- 3.4 Create timely, ongoing progress reports via multiple media sources.
- 3.5 Design an operating budget that balances immediate marketing and communication needs with a sustainable infrastructure.

**4. Increase residential and employment density as catalysts for enriching life for residents and visitors alike.**

- 4.1 Partner with the City of Loveland economic development department to keep abreast of demographic trends and emerging economic opportunities.



4.2 Establish a working relationship with area brokers and developers to highlight business and residential opportunities in the Downtown.

4.3 Establish a working relationship with funding sources and development groups that demonstrate an understanding of and a commitment to sustainable economic growth.

4.4 Create opportunities for a wide variety and type of business and residential uses with an emphasis on encouraging growth of local businesses.

**5. Assist emerging and existing businesses in navigating local, county, and state regulations and taxing policies.**

- 5.1 Create a proactive partnership with the Loveland Center for Business Development to assist with business development.
- 5.2 Work with city, county, and state agencies to provide ongoing workshops and seminars related to business regulations and tax policies/procedures.
- 5.3 Provide "just-in-time" support/advocacy for business and property owners who need immediate answers or directions.
- 5.4 Provide tips through blogs and other media related to common problems experienced/reported by local business and property owners.
- 5.5 Provide recognition of those individuals or groups who make significant contributions to the success of the Downtown.



**Special Thanks  
to the  
Downtown Working Group  
for their dedication to the creation of the  
Strategic Plan**

**Clay Caldwell  
Jim Cox  
Debbie Davis  
Harry Devereaux  
Justin Erion  
Barry Floyd  
Dan Johnson  
Bryan Jones  
Heather Lelchhook  
Mike Lynch  
Mindy McCloughan  
Gene Pielin  
Troy Peterson  
Tim Reeser  
Doug Rutledge  
Carla Schlosser  
Nicole Yost**

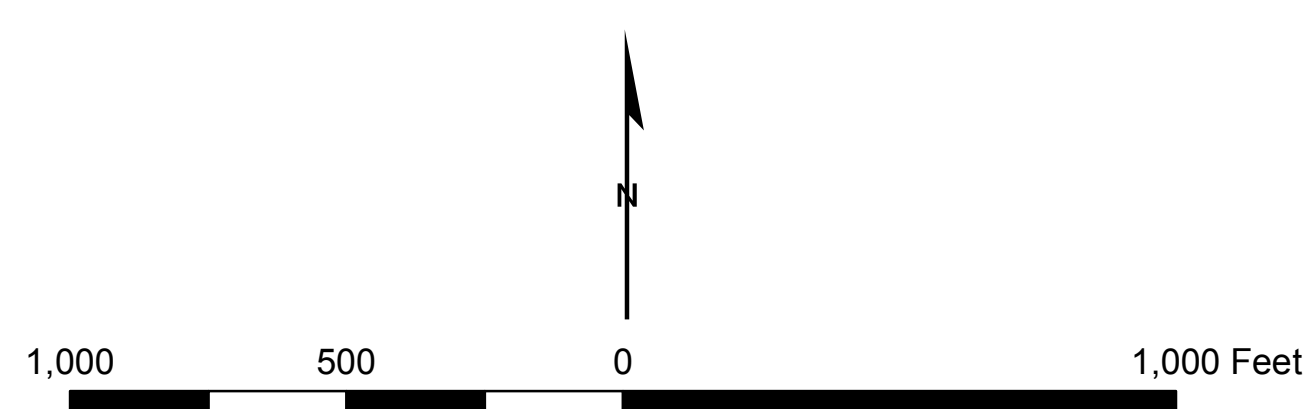




**Loveland Downtown Development Authority**

DDA approved by voters on February 10, 2015

 Downtown URA



NOTE: All calculations were made using best available data and should be considered approximate. Methodology statement available upon request.







## MEMORANDUM

June 8, 2015 – Regular Agenda 2

To: Loveland Planning Commission

From: Troy Bliss, Senior Planner

Subject: **Preliminary Plat Extension Request of Mariana Butte 25<sup>th</sup> Subdivision (Mountain Gate) (PZ #10-00120)**

**This is a quasi-judicial action on an application to extend the Preliminary Plat for Mariana Butte 25<sup>th</sup> Subdivision (Mountain Gate).**

Mr. Thomas Hartley, owner of the subject property, has submitted a written request for a two-year extension of the Preliminary Plat and Preliminary Development Plan for the Mariana Butte 25<sup>th</sup> Subdivision (Mountain Gate). Mr. Hartley is the owner and potential developer of the 34-acre property generally located at the northwest corner of W. 1<sup>st</sup> Street and Namaqua Avenue. In February of 2012, the Preliminary Plat and Preliminary Development Plan (PDP) (**Attachments 3 and 4**) were approved by the City for 51 lots (46 paired single-family units and 5 detached single-family units).

Section 16.20.020 of the subdivision code indicates that Preliminary Plats shall be valid for one year. A Final Plat must be submitted to the City within this one year time period or the application is deemed withdrawn, unless an extension to the Preliminary Plat is granted by the Planning Commission for good cause shown. A previous two-year extension was granted by the Planning Commission in July of 2013 – extending approval to February 7, 2015. The extension was granted to Jess Rodriguez, potential developer and applicant at the time.

Mr. Hartley's request for another two-year extension came into the Current Planning office prior to the expiration date. Since Mr. Hartley has been out of the state for a number of months, the request is being presented to Planning Commission after the deadline. The City's policy with these extension requests has been to allow the Planning Commission hearing after the expiration date if a request is made prior to the expiration and if there are extenuating circumstances that prevent the owner from being available. However, any consideration for approving another extension would be back-dated, resulting in the expiration to occur on February 7, 2017, for example.

With PUD zoning, a PDP is processed at the same time as the Preliminary Plat. The PDP is also valid for one year following approval. The PDP can be extended by the Current Planning Manager. It is customary for the Current Planning Manager to extend a PDP for the same length of time that the Planning Commission extends a Preliminary Plat. When a Preliminary Plat or PDP expires without an extension, a new application must be filed and all application fees paid.

Mr. Hartley as well as Stephen/Kari Robinson (agent/messenger) cites economic factors including the real estate market versus the cost of infrastructure improvements and raw water that are complicating the financial feasibility of the project (see **Attachments 1 and 2**). On January 15, 2015, Mr. Hartley was in contact with staff regarding the extension (**see Attachment 1**). Upon requesting additional information, staff received a supplementary email on February 2, 2015 (**see Attachment 2**) further elaborating on the request.

In preparation for bringing this forward to the Planning Commission, staff verifies with all City Departments that the project is still in compliance with current development standards. The Mariana Butte 25<sup>th</sup> Subdivision Preliminary Plat complies with all current City standards and the associated PDP still maintains its vested right. Additionally, the neighborhood supported this project and the donation of a portion of land to the Loveland Historical Society for the Medina Family Cemetery. However, if a further extension is not granted by the Planning Commission, this action would have no impact to the Medina Family Cemetery. The land has already been deeded over to the Loveland Historical Society.

**Planning staff has no objection to this request.** Staff believes that it will serve no practical purpose to require a re-submittal of the approved applications. If the Planning Commission approves an extension request for the Preliminary Plat, staff would look favorably on extension of the Preliminary Development Plan for the same time period. If an extension request is denied by the Planning Commission, staff will not entertain an extension of the Preliminary Development Plan.

If an extension is approved, the applicant will be required to submit a Final Plat and Final Development Plan within the specified extension period or a re-application of the Preliminary Plat and Preliminary Development Plan will be necessary. These final documents must be accompanied by Public Improvement Construction Plans (infrastructure plans) that are consistent with the City's current standards at the time of submittal.

## **RECOMMENDED MOTION**

Move to make a finding that the applicant has shown good cause due to recent economic conditions, including the real estate market versus the cost of infrastructure improvements and raw water, and based on that finding, approve

the request for a two-year extension of the Preliminary Plat of the Mariana Butte 25<sup>th</sup> Subdivision to February 17, 2017.

**ATTACHMENTS:**

1. January 15, 2015, Letter of Request from Thomas Hartley
2. February 2, 2015, email supplementing extension request by Thomas Hartley forwarded by Stephen/Kari Robinson
3. Preliminary Plat for Mariana Butte 25<sup>th</sup> Subdivision (approved February 7, 2012)
4. Portions of the Preliminary Development Plan for Mariana Butte 25<sup>th</sup> Subdivision (Mountain Gate)

**January 15, 2015**

**City of Loveland  
Current Planning Division  
500 East Third Street, Suite 310 • Loveland, CO 80537  
Troy Bliss  
Senior Planner  
970-962-2579  
Troy.Bliss@cityofloveland.org**

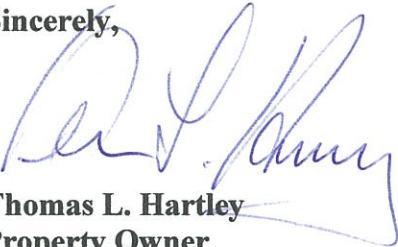
**RE: Mariana Butte 25<sup>th</sup> -  
Request for Extension of Approval  
Preliminary Development Plan and Preliminary Plat**

**This is a request for an extension of approval for the Mariana Butte 25th Subdivision-  
Preliminary Development Plan and Preliminary Plat for a two year time extension.**

**Through our real estate broker, we have been diligently following leads to find a developer  
to follow through with a final application on this land and hope it will happen soon. Should  
you desire to contact this broker, I can supply his contact information.**

**Thank you for your consideration of this request.**

**Sincerely,**



**Thomas L. Hartley  
Property Owner  
13900 W. 44th Ave., Suite A  
Golden, CO 80403  
303-877-8569  
eatonshartley@gmail.com**

## Troy Bliss

---

**From:** Kari Robinson <stephenco@gmail.com>  
**Sent:** Monday, March 09, 2015 1:23 PM  
**To:** Troy Bliss  
**Cc:** Tom Hartley; Robinson  
**Subject:** Re: University Hills/ Mariana Butte 25th Subdivision

Kindly Confirm Receipt of this email.

Hello Troy,

Acting as agent and/or messenger for Tom Hartley we would like to have a meeting in the month of June or July on the Mariana Butte extensions. Tom will be back at the end of May and that will give us time to meet with the engineers and planners and make a proposal to the City of Loveland.

Thank you,  
Stephen Robinson/klr

Kari Robinson  
A. Stephen and Company, Inc.  
970-282-8505 office  
970-239-4391 fax  
970-222-7522 cell

CONFIDENTIALITY STATEMENT: Please be advised that this e-mail communication and any attachments hereto are confidential documents intended only for the use of the individual(s) or entity(ies) named above and may be subject to attorney/client and/or work-product privileges. If you have received this communication in error, please notify us by telephone and delete and destroy all electronic and hard copies of this communication. If you are not the proper recipient, you are hereby notified that any disclosure, dissemination, distribution or copying of this communication or the information contained therein is strictly prohibited. Thank you.

On Wed, Mar 4, 2015 at 3:41 PM, Troy Bliss <[Troy.Bliss@cityofloveland.org](mailto:Troy.Bliss@cityofloveland.org)> wrote:

Kari, thank you for sending this to me. I am not quite sure why I did not receive this on February 2, 2015. The only thing I can think of is that it ended up in my spam and I didn't look there in time.

Tom, if you feel that this captures your justification fully, I will proceed with including this in my communication to the Planning Commission. As mentioned in previous conversations, the Planning Commission has been (in general) reluctant to grant further extensions to projects. Having a strong justification and presenting it to the Planning Commission will be important in obtaining approval. The next Planning Commission meeting will be on **March 23, 2015**. I can get this request scheduled for that meeting, if you would like. However, as we discussed over the phone

yesterday, you will need to have someone at the meeting representing you. They will need to be prepared to provide a presentation (based upon your justification) as to the request for extending approval. This individual will also need to be able to respond to questions from the Planning Commission and/or make decisions relative to potential conditions on your behalf. If you need more time to coordinate this, I can look to schedule something in April or possibly when you return from South Carolina. Please let me know how you would like to proceed and who would be attending the Planning Commission meeting in place of you. Thank you.

Troy Bliss

Senior Planner

Current Planning

Development Services

City of Loveland

[\(970\) 962-2579](tel:9709622579)

[Troy.Bliss@cityofloveland.org](mailto:Troy.Bliss@cityofloveland.org)

**From:** Kari Robinson [mailto:[stephenco@gmail.com](mailto:stephenco@gmail.com)]

**Sent:** Wednesday, March 04, 2015 11:51 AM

**To:** Troy Bliss

**Cc:** Tom Hartley; Robinson

**Subject:** Re: University Hills/ Mariana Butte 25th Subdivision

Troy,

I am resending this email that was originally sent to you on February 2.

Please advise.

Thank you,

Kari

Kari Robinson  
A. Stephen and Company, Inc.  
[970-282-8505](tel:970-282-8505) office  
[970-239-4391](tel:970-239-4391) fax  
[970-222-7522](tel:970-222-7522) cell

CONFIDENTIALITY STATEMENT: Please be advised that this e-mail communication and any attachments hereto are confidential documents intended only for the use of the individual(s) or entity(ies) named above and may be subject to attorney/client and/or work-product privileges. If you have received this communication in error, please notify us by telephone and delete and destroy all electronic and hard copies of this communication. If you are not the proper recipient, you are hereby notified that any disclosure, dissemination, distribution or copying of this communication or the information contained therein is strictly prohibited. Thank you.

On Mon, Feb 2, 2015 at 10:00 AM, Kari Robinson <[stephenco@gmail.com](mailto:stephenco@gmail.com)> wrote:

**Good Morning Troy,**

**Below is the request for the extension from the Owner-Tom Hartley.**

**If you could please acknowledge receipt of this email for me, it is much appreciated.**

**Thank you,**

**Kari**

Kari Robinson  
A. Stephen and Company, Inc.  
[970-282-8505](tel:970-282-8505) office  
[970-239-4391](tel:970-239-4391) fax  
[970-222-7522](tel:970-222-7522) cell

CONFIDENTIALITY STATEMENT: Please be advised that this e-mail communication and any attachments hereto are confidential documents intended only for the use of the individual(s) or entity(ies) named above and may be subject to attorney/client and/or work-product privileges. If you have received this communication in error, please notify us by telephone and delete and destroy all electronic and hard copies of this communication. If you are not the proper recipient, you are hereby notified that any disclosure, dissemination, distribution or copying of this communication or the information contained therein is strictly prohibited. Thank you.

----- Forwarded message -----

From: **Tom Hartley** <[eatonshartley@gmail.com](mailto:eatonshartley@gmail.com)>



Date: Fri, Jan 30, 2015 at 8:44 AM  
Subject: University Hills  
To: Kari Robinson <[stephenco@gmail.com](mailto:stephenco@gmail.com)>

Troy Bliss

Planning Department

City of Loveland

This is a request to extend our entitlements for another two years on our project at

First and Nemaquwa. The reasons for this request are the issues that surround raw water in Loveland, with the cost going up over 300% since we started the project and the fact that Loveland has a policy of having approved structure plans prior to the recording of the final plat. As I am not a builder but a land developer I need to have someone under contract to build the vertical to be able to comply with your requirements. The development market has been somewhat stagnant because of financing issues coming out of our recent real estate recession.

Thank you

Tom Hartley

Owner



PRELIMINARY PLAT FOR

MARIANA BUTTE TWENTY FIFTH SUBDIVISION

BEING A SUBDIVISION OF LOT 1 BLOCK 12 MARIANA BUTTE SEVENTH SUBDIVISION AND A PORTION OF TRACT A MARIANA BUTTE PUD FIRST SUBDIVISION, ALL SITUATE IN THE SOUTHWEST QUARTER OF SECTION 16, TOWNSHIP 5 NORTH, RANGE 69 WEST OF THE 6TH P.M., TO THE CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO.


SHEET 1 OF 3

PROPERTY DESCRIPTION

MARIANA BUTTE TWENTY FIFTH SUBDIVISION:  
Parcel 1  
Lot 1, Block 12, Mariana Butte Seventh Subdivision, in the City of Loveland, County of Larmer, State of Colorado.

Parcel 2  
A parcel of land being a portion of Tract A, Mariana Butte P.U.D., First Subdivision to the City of Loveland, County of Larmer, State of Colorado, according to the Plat on file in the office of the Clerk and Recorder said County, being more particularly described as follows:  
The said parcel of land is situated in the Southwest Quarter of Section 16, Township 5 North, Range 69 West of the 6th P.M., in the City of Loveland, County of Larmer, State of Colorado, and is subject to the City of Loveland, according to the Plat on file in the office of the Clerk and Recorder said County, thence along the Southerly and Easterly lines of Tract O, said Mariana Butte Seventh Subdivision, the following two courses and distances: South 89°52'30" East 20.00 feet; thence North 00°07'30" East 280.59 feet to a point on the South line of Tract W, said Mariana Butte P.U.D., First Subdivision; thence along said South line South 89°55'31" East 1,137.17 feet to a point on the Easterly line of Tract A, thence along the Easterly and Southerly lines of Tract A, the following courses and distances: South 04°22'54" West 254.36 feet, thence North 89°45'22" West 289.28 feet, thence South 00°15'20" West 229.94 feet, thence North 89°48'36" West 848.49 feet to a point on the East line of Tract T, said Mariana Butte Seventh Subdivision, thence along said East line and along the East line of said Lot 1 Block 12, North 00°07'30" East 200.00 feet to the Point of Beginning.  
Containing 33.52 Acres, more or less, and being subject to all existing easements and/or rights-of-way of record.

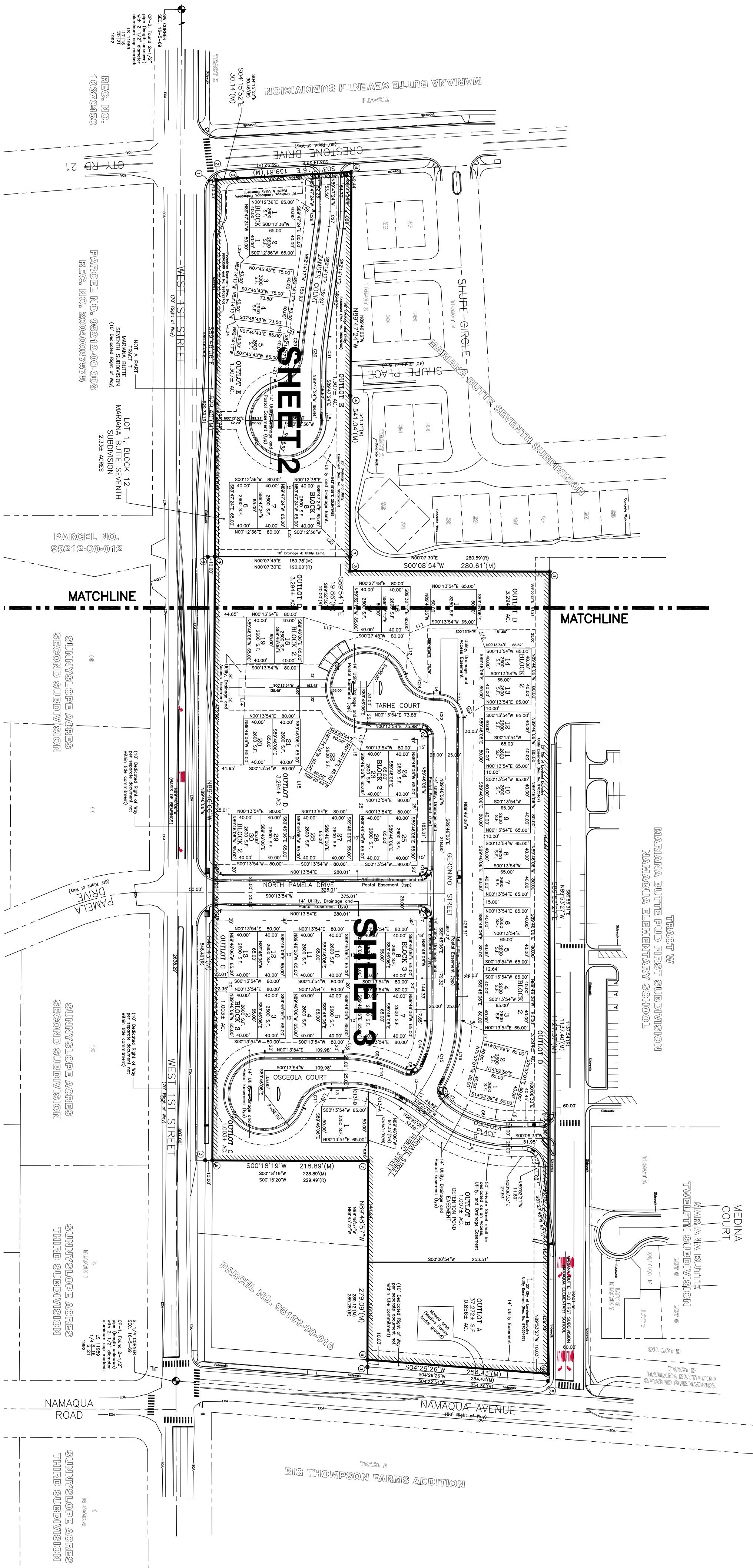
GENERAL NOTES:

1. Outlots A, B, C, D and E are not building sites.
2. This project is subject to an Ametaxation Agreement which has been recorded in the Real Property records in the Office of the Larmer County Clerk and Recorder.
3. This project, at final plat stage may be subject to a Development Agreement which shall be recorded in the Real Property records in the Office of the Larmer County Clerk and Recorder.
4. All easements adjacent to Street right of ways, as shown on this plat, are to be dedicated as public 14' Access, Utility, Drainage and Postal Easements.
5. All on-site drainage improvements, including swales and detention ponds, shall be owned and maintained by the Mariana Butte Twenty Fifth Subdivision Homeowners Association.
6. Maintenance and upkeep of stormwater detention ponds, storm sewer systems, swales, and permanent stormwater quality improvements are required by the City of Loveland and are a continuing obligation of the Mariana Butte Twenty Fifth Subdivision Homeowners Association. The Mariana Butte Twenty Fifth Subdivision Homeowners Association shall provide ongoing maintenance of the private stormwater improvements as needed to maintain compliance with the approved construction plans and reports.
7. The property lies within Flood Zone "X" (no hazards) per the Flood Insurance Rate Map (FIRM) as prepared by the Federal Emergency Management Agency (FEMA), Panel No. 08069C168F, having an effective date of December 19, 2006.
8. Developer-Installed landscaping improvements shall be owned and maintained by the Morano Butte Twenty Fifth Subdivision Homeowners Association (HOA).
9. Publicly dedicated rights of way are to be maintained by the City of Loveland. Landscaping Improvements within publicly dedicated rights of way are to be maintained by Morano Butte Twenty Fifth Subdivision Homeowners Association.
10. Outlots B, C, D and E are hereby dedicated as emergency access, access, public utility, drainage and landscape easements. All portions of outlots that fall within and/or overlap on existing or proposed exclusive easement, except for the Osceola Place crossing, shall be excluded from the above referenced dedication as emergency access, access, public utility, drainage and landscape easements.
11. Basis of Bearings for this Survey: The South line of the of Southwest Quarter of Section 16, Township 5 North, Range 69 West of the 6th P.M., City of Loveland, County of Larmer, State of Colorado, is assumed to bear South 89°48'06" East.
12. Benchmark: City of Loveland benchmark 95.62 Aluminum disk on concrete irrigation headwall at the Southwest corner of the intersection of 1st Street and Namaqua Road. Elevation=5033.40
13. (M) = Field measured bearing and/or distance per legal monumentation.  
(R) = Record bearing and/or distance per legal description and recorded subdivision plats.
14. All building pods as shown and labeled hereon contain 2600.0 square feet unless otherwise noted.
15. Structures/items that shall be contained within the lot/building envelope include roof eaves, foundation footings, window wells, building extrusions such as bay windows, porches, decks and patios.
16.  indicates Project Boundary

SETBACK NOTES:

Front Setback: Zero (0') feet to the property line, sixteen (16') feet minimum from the back of the sidewalk (or back of curb if no sidewalk exists) to the face of the structure, and twenty (20') feet minimum to the face of the garage.  
Side Setback: Zero (0') feet to the property line, and ten (10') feet minimum between adjacent structures.  
Rear Setback: Zero (0') feet to the property line, and ten (10') feet minimum between adjacent structures.

TRACT	ACRE(S)	% SITE	USE	WATER	OWN/MAINTAIN
Outlot A	0.856	2.56%	Point of Interest/Loveland Historical Society	N/A	Historical Society
Outlot B	1.007	3.02%	Utilities/Landscape/Open Drainage/Access	N/A	Homeowners Assoc.
Outlot C	1.003	3.02%	Utilities/Landscape/Open Drainage/Access	Public	Homeowners Assoc.
Outlot D	3.294	9.85%	Utilities/Landscape/Open Drainage/Access	Public	Homeowners Assoc.
Outlot E	1.307	3.90%	Utilities/Landscape/Open Drainage/Access	Public	Homeowners Assoc.
Totals	7.467	22.28%			





PRELIMINARY PLAT FOR

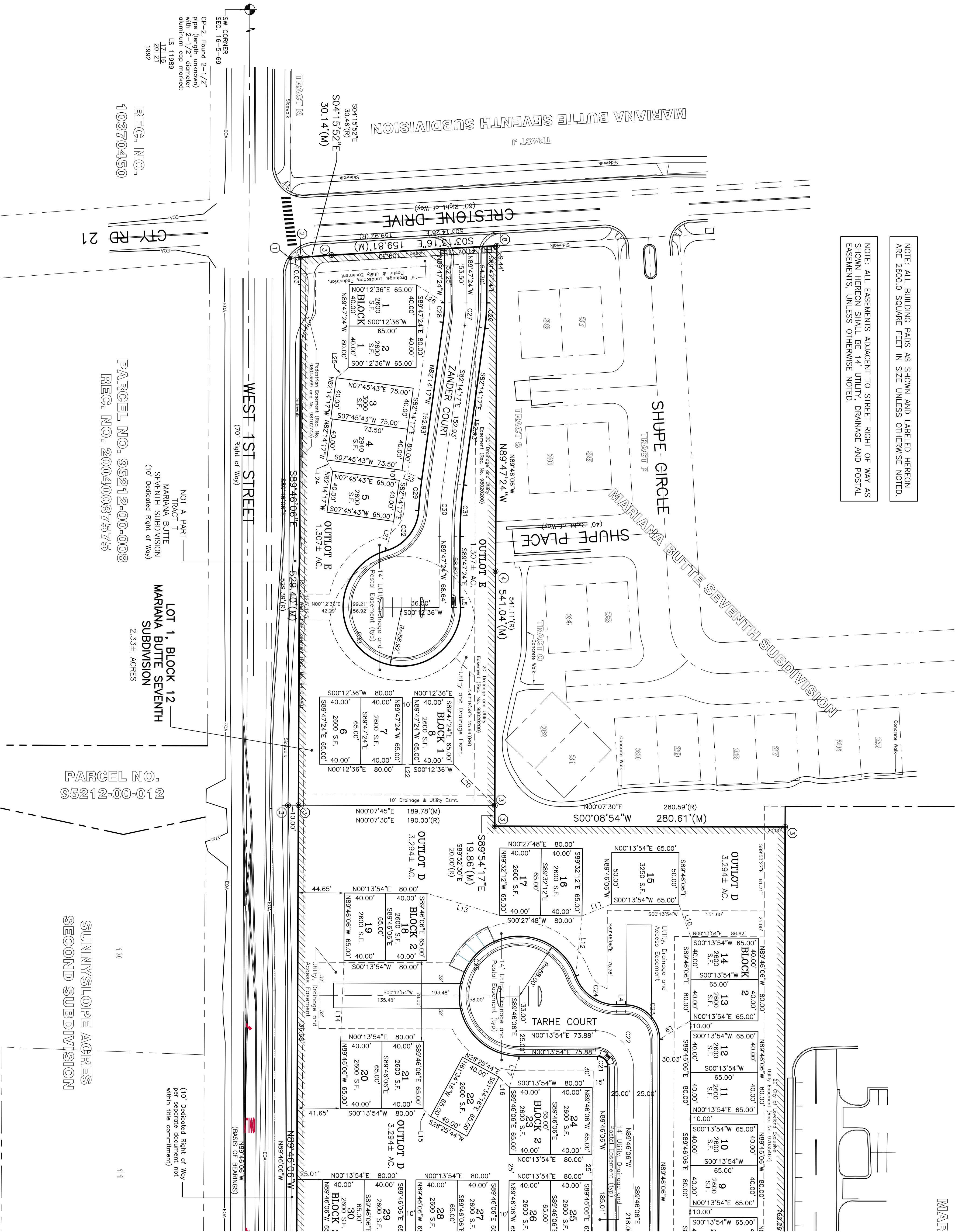
MARIANA BUTTE TWENTY FIFTH SUBDIVISION

BEING A SUBDIVISION OF LOT 1 BLOCK 12 MARIANA BUTTE SEVENTH SUBDIVISION AND A PORTION OF TRACT A MARIANA BUTTE PUD FIRST SUBDIVISION, ALL SITUATE IN THE SOUTHWEST QUARTER OF SECTION 16, TOWNSHIP 5 NORTH, RANGE 69 WEST OF THE 6TH P.M., TO THE CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO.

SHEET 2 OF 3

NOTE: ALL BUILDING PADS AS SHOWN AND LABELED HEREON ARE 2600.0 SQUARE FEET IN SIZE UNLESS OTHERWISE NOTED.

NOTE: ALL EASEMENTS ADJACENT TO STREET RIGHT OF WAY AS SHOWN HEREON SHALL BE 14' UTILITY, DRAINAGE AND POSTAL EASEMENTS, UNLESS OTHERWISE NOTED.



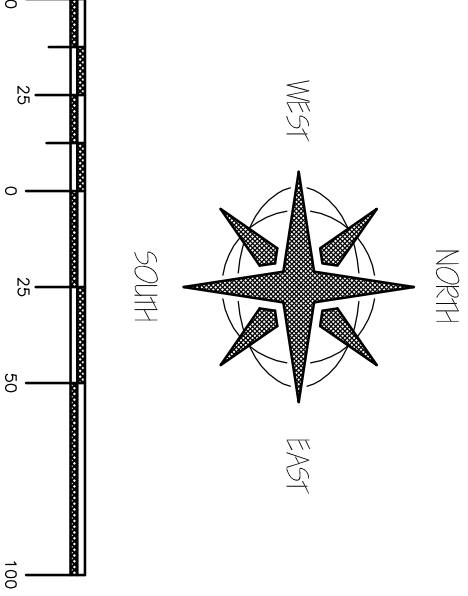
SEE SHEET 3 OF 3

LINE	BEARING	DISTANCE
L1	S45°39.55'E	11.83'
L2	N36°20.05'E	7.61'
L3	N00°13°54'E	15.93'
L4	N00°13°54'E	8.84'
L5	N89°24.36'E	10.04'
L6	S78°56.55'E	19.95'
L7	N82°40.47'E	32.66'
L8	N50°41.44'W	35.01'
L9	S89°49.05'W	34.49'
L10	N21°51.58'W	30.73'
L11	N84°00'15'W	28.33'
L12	S16°53.15'W	72.95'
L13	S46°22.32'W	10.51'
L14	S44°21.39'W	14.08'
L15	S72°51.32'E	31.36'
L16	S81°08.22'E	29.34'
L17	S89°26.13'E	15.67'
L18	S82°05.53'W	92.12'
L19	S82°05.53'W	92.12'

CURVE	DELTA	RADIUS	LENGTH	BEARING	CHORD
C1	90°00'00"	11.50'	18.05'	N44°53.22'W	16.26'
C2	07°43.51"	108.00'	14.57'	N86°15.44'E	14.56'
C3	90°01'07"	10.00'	15.71'	S45°07'06"W	14.14'
C4	36°13'32"	180.00'	120.13'	N18°13.19'E	118.14'
C5	36°13'32"	180.00'	120.13'	N18°13.19'E	118.14'
C6	36°13'32"	140.00'	88.82'	N18°13.19'E	87.05'
C7	83°20'51"	10.00'	14.55'	N78°00.50'E	13.30'
C8	90°56.54"	10.00'	15.87'	N17°30.21'W	14.26'
C9	27°44.12"	180.00'	91.98'	N14°06'00'W	91.08'
C10	36°06.11"	185.00'	103.97'	S18°16.59'W	102.26'
C11	20°07'23"	48.00'	31.40'	N35°10.13'E	48.25'
C12	20°07'23"	48.00'	31.40'	N35°10.13'E	48.25'
C13	36°06.11"	140.00'	88.82'	S18°16.59'W	86.76'
C13-A	21°14.15"	140.00'	51.89'	S28°42.57'W	51.60'
C13-B	14°51.55"	140.00'	36.92'	S07°39.52'W	36.22'
C14	26°48.46"	140.00'	65.52'	N7°23.11'W	64.92'
C15	36°07.39"	185.00'	104.04'	N17°43.46'W	102.32'
C16	28°28'00"	180.00'	91.98'	S18°16.59'W	102.32'
C17	28°28'00"	180.00'	91.98'	S18°16.59'W	102.32'
C18	90°00'00"	10.00'	15.71'	N44°46'06'W	14.14'
C19	90°00'00"	10.00'	15.71'	N44°46'06'W	14.14'
C20	90°00'00"	10.00'	15.71'	N44°46'06'W	14.14'
C21	90°00'00"	10.00'	15.71'	N44°46'06'W	14.14'
C22	90°00'00"	10.00'	15.71'	N44°46'06'W	14.14'
C23	90°00'00"	10.00'	15.71'	N44°46'06'W	14.14'
C24	70°07'23"	42.00'	51.40'	N35°17.56'E	48.25'
C25	250°07'23"	58.00'	253.20'	S54°42.24'E	94.95'
C26	07°33'08"	185.00'	21.40'	N86°00'50'W	21.33'
C27	07°33'08"	185.00'	21.40'	N86°00'50'W	21.33'
C28	07°33'08"	185.00'	21.40'	N86°00'50'W	21.33'
C29	04°08.34"	420.00'	30.44'	S84°18.34'E	30.43'
C30	04°08.34"	420.00'	30.44'	S84°18.34'E	30.43'
C31	07°33'08"	339.92'	50.08'	S86°00'50'E	50.04'
C32	70°49'12"	43.95'	53.25'	N50°59.15'W	49.92'
C33	254°15'09"	58.92'	252.58'	N37°18.47'E	90.77'

MONUMENTATION LEGEND

- 1 Did not find or Set Property Pin
- 2 Found Pin NAIL with 1" Diameter Shaver Steel US 30462
- 3 Found 1/2" Rebar (length unknown) with 1" Diameter Plastic Cap (LS 32829)
- 4 Found 1/2" Rebar (length unknown) with no Cap or Markings (used for line)
- 5 Found 1/2" Rebar (length unknown) with 1" Diameter Plastic Cap (LS 16415)
- 6 Found 1/2" Rebar (length unknown) with 1" Diameter Red Plastic Cap (LS 30452)
- 7 Found 1/2" Rebar (length unknown) with 1" Diameter Plastic Cap (LS 15278)
- 8 Found Bent 3/4" Pipe (length unknown) with 1" Diameter Red Plastic Cap (LS 32829) and not Accept Monumentation



STATEMENT OF LINEAR UNITS USED:

Linear Units Used for this survey - U.S. Survey Feet

According to Colorado law you must commence any legal action based upon any defect in this survey within ten years from the date of the certification shown hereon.

REVISIONS:

- 1) City of Loveland DRT comments dated 11-12-2010
- 2) Revise street names

BY:

DATE:

2-8-2011  
11-8-2011

JESS RODRIGUEZ  
5534 Stone Church Court  
Loveland, CO 80537

INTERMILL LAND SURVEYING, INC.

1301 NORTH CLEVELAND AVENUE LOVELAND, COLORADO 80537 BUS. (970)-669-0516 / FAX (970)-635-9775

PRELIMINARY PLAT FOR  
MARIAN BUTTE TWENTY FIFTH SUBDIVISION  
PART OF THE SOUTHWEST QUARTER OF SECTION 16-5-69, LOVELAND, COLORADO

DRAWN BY: SJS  
CHECKED BY: SJS  
APPROVED BY: SJS

DATE: 7-14-2010  
SCALE: 1" = 50'

PROJECT NO.: P-09-6733

SHEET 2 OF 3



PRELIMINARY PLAT FOR

MARIANA BUTTE TWENTY FIFTH SUBDIVISION

BEING A SUBDIVISION OF LOT 1 BLOCK 12 MARIANA BUTTE SEVENTH SUBDIVISION AND A PORTION OF TRACT A MARIANA BUTTE PUD FIRST SUBDIVISION, ALL SITUATE IN THE SOUTHWEST QUARTER OF SECTION 16, TOWNSHIP 5 NORTH, RANGE 69 WEST OF THE 6TH P.M., TO THE CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO.

SHEET 3 OF 3

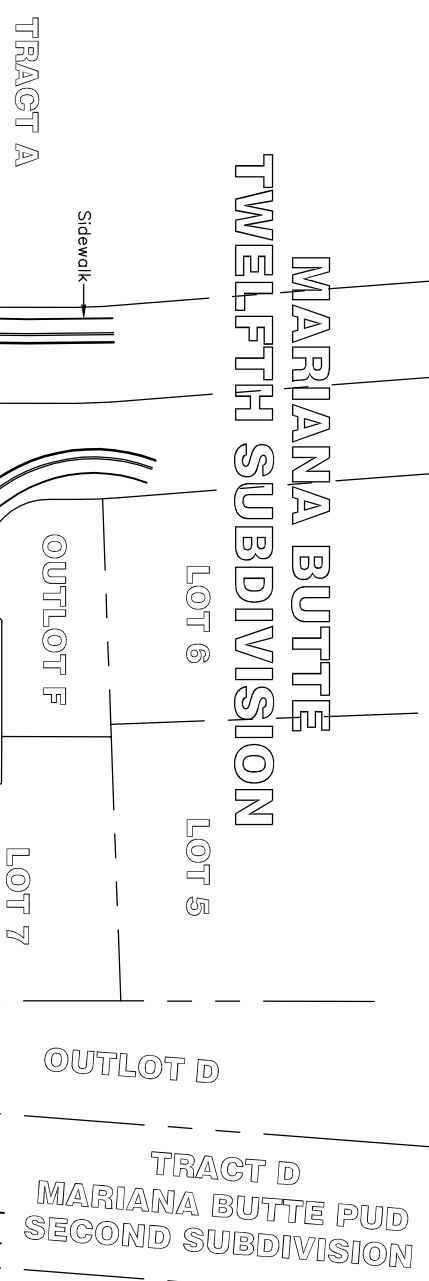
NOTE: ALL BUILDING PADS AS SHOWN AND LABELED HEREON ARE 2600.0 SQUARE FEET IN SIZE UNLESS OTHERWISE NOTED.

NOTE: ALL EASEMENTS ADJACENT TO STREET RIGHT OF WAY AS SHOWN HEREON SHALL BE 14' UTILITY, DRAINAGE AND POSTAL EASEMENTS, UNLESS OTHERWISE NOTED.

TRACT W  
MARIANA BUTTE PUD FIRST SUBDIVISION  
NAMAQUA ELEMENTARY SCHOOL

MEDINA COURT

MARIANA BUTTE  
TWELFTH SUBDIVISION

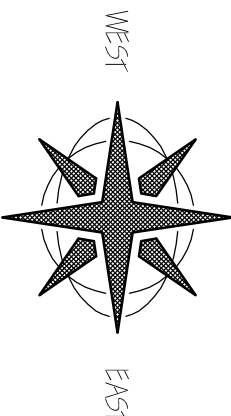


LINE TABLE		
LINE	BEARING	DISTANCE
L1	S53.39.55"E	11.83'
L2	N86.20.05"E	7.61'
L3	N00.13.54"E	15.93'
L4	N89.24.36"E	10.04'
L5	N89.24.36"E	10.04'
L6	S79.56.39"E	19.59'
L7	S89.50.17"E	14.21'
L8	N24.10.47"E	32.86'
L9	N36.41.44"W	35.01'
L10	S89.48.05"W	34.49'
L11	N89.48.05"W	34.49'
L12	N84.00.15"W	58.33'
L13	S16.53.45"W	72.95'
L14	N87.33.57"W	78.06'
L15	S46.22.32"W	10.51'
L16	S44.21.36"W	14.08'
L17	S7.25.13"E	51.58'
L18	N89.48.05"W	34.49'
L19	S46.16.10"W	28.34'
L20	S46.16.10"W	55.93'
L21	N72.21.12"W	29.25'
L22	S00.12.36"W	10.00'
L23	S24.22.26"W	20.86'
L24	S24.22.26"W	20.86'
L25	S24.22.26"W	20.86'
L26	S22.30.53"W	32.12'
L27	N36.20.05"E	11.23'

CURVE TABLE			
CURVE	DATA	RADIUS	LENGTH
C1	90.00.00"	11.30'	18.06'
C2	90.00.00"	11.30'	18.06'
C3	90.01.07"	10.00'	15.71'
C4	36.13.32"	190.00'	120.13'
C5	36.13.32"	165.00'	104.32'
C6	36.13.32"	140.00'	88.52'
C7	83.20.51"	10.00'	14.55'
C8	90.05.34"	10.00'	15.82'
C9	36.06.11"	42.00'	51.40'
C10	36.06.11"	165.00'	104.04'
C11	26.07.23"	58.00'	25.20'
C12	36.06.11"	140.00'	88.52'
C13	36.06.11"	140.00'	88.52'
C14	36.06.11"	140.00'	88.52'
C15	36.07.39"	165.00'	104.04'
C16	28.38.30"	190.00'	120.13'
C17	90.00.00"	10.00'	15.71'
C18	90.00.00"	10.00'	15.71'
C19	90.00.00"	10.00'	15.71'
C20	90.00.00"	10.00'	15.71'
C21	90.00.00"	10.00'	15.71'
C22	90.00.00"	10.00'	15.71'
C23	90.00.00"	10.00'	15.71'
C24	70.07.23"	42.00'	51.40'
C25	26.07.23"	58.00'	25.20'
C26	07.23.08"	188.08'	24.40'
C27	07.23.08"	188.08'	24.40'
C28	07.23.08"	188.08'	24.40'
C29	04.08.34"	420.92'	30.44'
C30	07.23.08"	400.00'	56.72'
C31	07.23.08"	379.92'	50.08'
C32	70.49.12"	43.08'	53.26'
C33	25.41.09"	35.92'	29.58'

TRACT A  
BIG THOMPSON FARMS ADDITION

- MONUMENTATION LEGEND**
- 1 Did not find or Set Property Pin
  - 2 Found PK Nail with 1" Diameter Shiner Stamped "LS 30462"
  - 3 Found 1/2" Rebar (length unknown) with 1" Diameter Plastic Cap (LS 32829)
  - 4 Found 1/2" Rebar (length unknown) with no cap or Markings (used for line)
  - 5 Found 1/2" Rebar (length unknown) with 1" Diameter Plastic Cap (LS 30462)
  - 6 Found 1/2" Rebar (length unknown) with 1" Diameter Red Plastic Cap (LS 30462)
  - 7 Found 1/2" Rebar (length unknown) with 1" Diameter Plastic Cap (LS 15278)
  - 8 Found Rebar 3/4" Pipe (length unknown) with 1" Diameter Plastic Cap (liegible). Did not Accept Monument Position.
  - 9 Found 1/2" Rebar (length unknown) with 1" Diameter Plastic Cap (LS 32829)
  - 10 Found 1/2" Rebar (length unknown) with 1" Diameter Plastic Cap (LS 32829) 0.34' North of Accepted Corner.



Horizontal Scale: 1" = 50'

Vertical Scale: 1" = 50'

Linear Units Used for this Survey - U.S. Survey Feet

According to Colorado law you must commence any legal action based upon any defect in this survey within three years after you first discover such defect in no event, may any action based upon any defect in this survey be commenced more than ten years from the date of the certification shown hereon.

**INTERMILL LAND SURVEYING, INC.**  
1301 NORTH CLEVELAND AVENUE  
LOVELAND, COLORADO 80537  
BUS. (970)-669-0516 / FAX (970)-635-9775

PRELIMINARY PLAT FOR  
**MARIAN BUTTE TWENTY FIFTH SUBDIVISION**  
PART OF THE SOUTHWEST QUARTER OF SECTION 16-5-69, LOVELAND, COLORADO

CLIENT: **JESS RODRIGUEZ**  
5534 Stone Church Court  
Loveland, CO 80537

REVISIONS:

1) City of Loveland DRT comments dated 11-12-2010	DATE: 2-8-2011
2) Revise street names	DATE: 11-8-2011

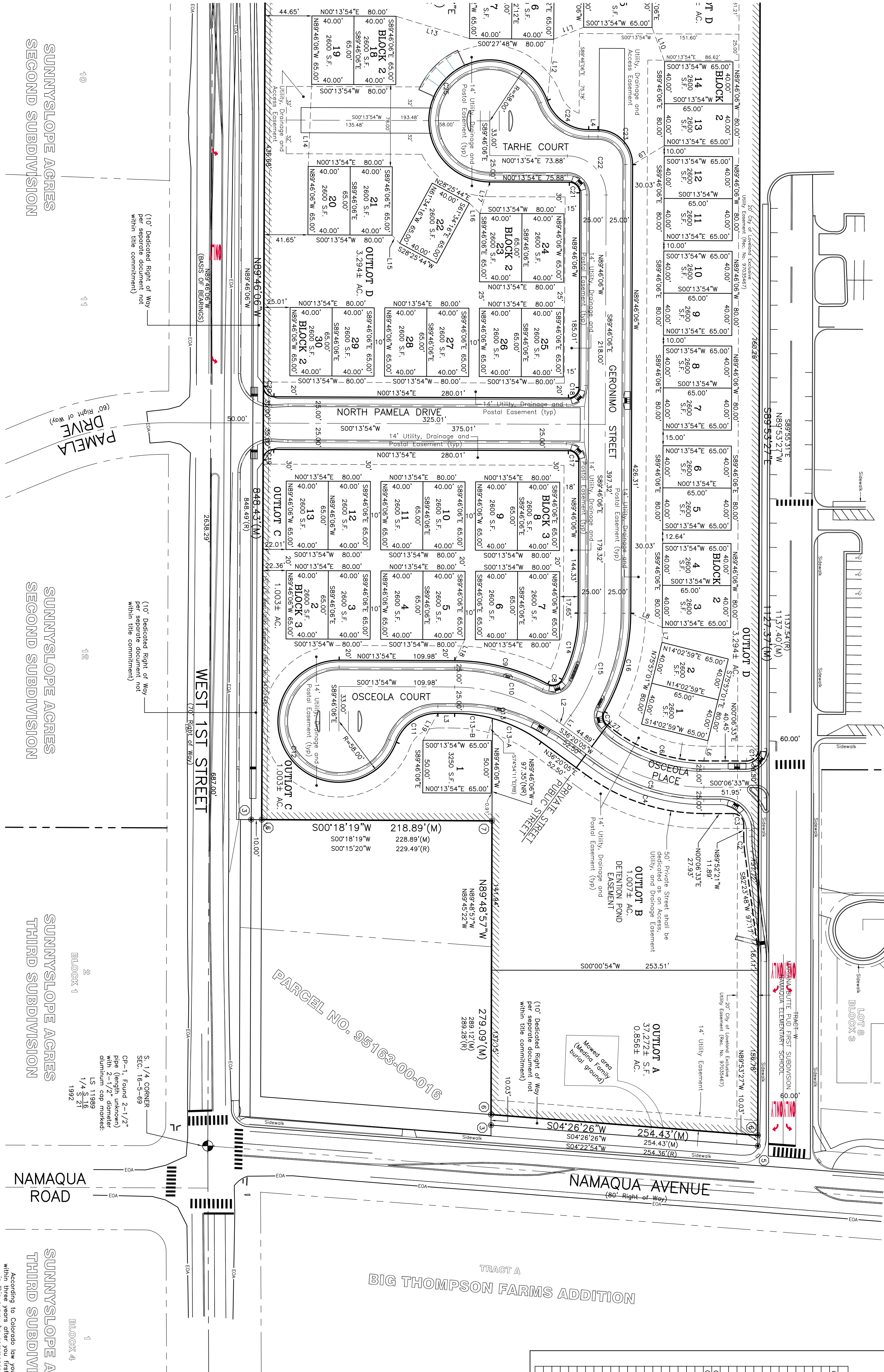
BY: \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_



SUNNYSLOPE ACRES  
SECOND SUBDIVISION

SUNNYSLOPE ACRES  
SECOND SUBDIVISION

SUNNYSLOPE ACRES  
THIRD SUBDIVISION

SUNNYSLOPE ACRES  
THIRD SUBDIVISION



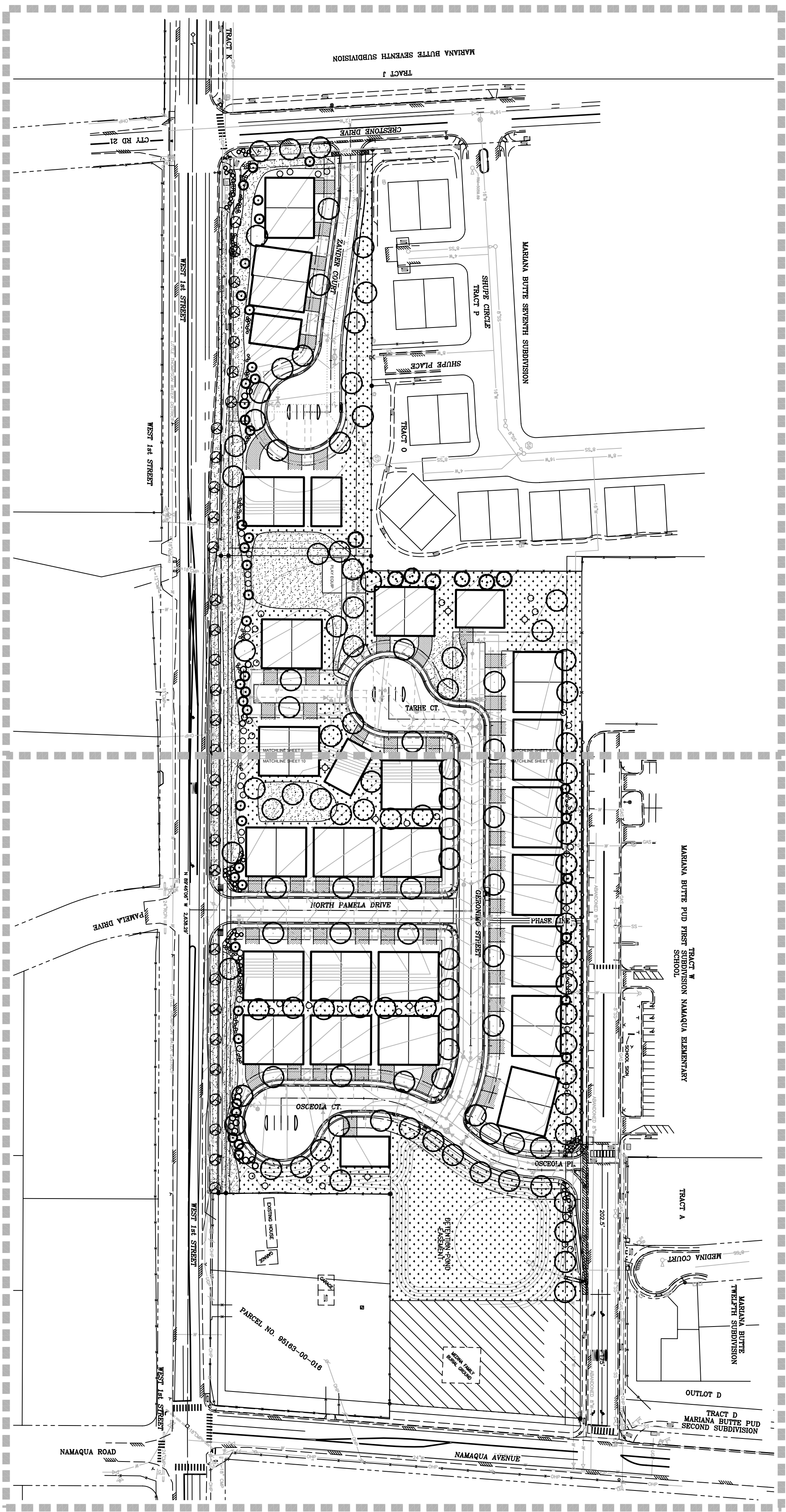
# MARIANA BUTTE TWENTY FIFTH SUBDIVISION

## MOUNTAIN GATE PRELIMINARY DEVELOPMENT PLAN

### PRELIMINARY LANDSCAPE PLAN

SHEET 9

SHEET 10



#### LANDSCAPE CONCEPT

THE MOUNTAIN GATE LANDSCAPE IS INTENDED TO MEET SEVERAL IMPORTANT REQUIREMENTS. FIRST, THE LANDSCAPE IS MEANT TO MEET THE LOVELAND SITE DEVELOPMENT GUIDELINES AND SPECIFICATIONS IN ADDITION TO THE AMENDED MARIANA BUTTE GENERAL DEVELOPMENT PLAN. SECONDLY, THE LANDSCAPE IS DESIGNED TO PROVIDE AN ATTRACTIVE SETTING FOR THE DEVELOPMENT WHICH DOES NOT REQUIRE EXTENSIVE WATER AND MAINTENANCE TO FULFILL ITS FUNCTIONS. IN ADDITION, THE MOUNTAIN GATE LANDSCAPE IS INTENDED TO BE PART OF THE MARIANA BUTTE PUD VISUALLY, WHILE PROVIDING A LINK TO THE NEARBY RURAL CHARACTER THROUGH THE USE OF NATIVE GRASSES AND PLANTINGS WITH INCREASED DROUGHT TOLERANCE.

#### LANDSCAPE NOTES

1. THIS PRELIMINARY LANDSCAPE PLAN ILLUSTRATES THE PROPOSED LANDSCAPE TREATMENTS WHICH SHALL BE THE RESPONSIBILITY OF THE DEVELOPER. THE LANDSCAPING WITHIN EACH OF THE PROPOSED BUILDING SITES WILL BE THE RESPONSIBILITY OF THE DEVELOPER OF EACH BUILDING. ORNAMENTAL TREES ARE USED INSTEAD OF REQUIRED CANOPY TREES ALONG 1st STREET DUE TO OVERHEAD POWER LINES. THESE TREES MUST NOT BE FRUITBEARING AND THE BRANCHES MUST BE PRUNED TO PRESERVE PEDESTRIAN ACCESS AND AVOID SIGHT TRIANGLE CONFLICTS.
2. ALL LANDSCAPING SHALL BE IN COMPLIANCE WITH THE CITY OF LOVELAND SITE DEVELOPMENT PERFORMANCE STANDARDS AND GUIDELINES, AND THE AMENDED MARIANA BUTTE GENERAL DEVELOPMENT PLAN.
3. ALL LANDSCAPED AREAS SHALL PROVIDE 75% COVERAGE OF LIVE MATERIALS WITHIN 3 YEARS OF INSTALLATION, AS STATED IN THE LOVELAND SITE DEVELOPMENT PERFORMANCE STANDARDS AND GUIDELINES.
4. THE AREA BETWEEN EACH BUILDING SITE AND THE BACK OF SIDEWALK SHALL BE LANDSCAPED AND MAINTAINED BY THE OWNER OF THE BUILDING.
5. NO FENCING SHALL BE INSTALLED WITHIN OR AROUND INDIVIDUAL BUILDING LOTS.
6. COORDINATE STREET TREE LAYOUT WITH METER PIT LAYOUT TO MAINTAIN MINIMUM 10' SEPARATION.

#### IRRIGATION NOTES

1. ALL LANDSCAPE PLANTINGS IN COMMON AREAS ON THE SITE, EXCEPT NATIVE SEEDING, SHALL BE PROVIDED WITH PERMANENT AUTOMATIC IRRIGATION SYSTEM. NATIVE SEEDING SHALL BE PROVIDED WITH TEMPORARY IRRIGATION UNTIL THE SEED IS ESTABLISHED.
2. ALL IRRIGATION SHALL BE IN COMPLIANCE WITH THE LOVELAND SITE DEVELOPMENT PERFORMANCE STANDARDS AND GUIDELINES, AND ANY APPLICABLE CODES.

#### PROPOSED BUFFERYARD PLANTINGS

BUFFERYARD 1 - CRESTONE DRIVE - BUFFERYARD TYPE B  
WIDTH - 15', LENGTH - 190'

	REQUIRED	PROPOSED
CANOPY TREES	4	4
FLOWERING TREES / LARGE SHRUBS	4	4
SHRUBS	10	10
EVERGREEN TREES	2	2

BUFFERYARD 2 - FIRST STREET - BUFFERYARD TYPE C  
WIDTH - 40', LENGTH - 980'+358'=1338'

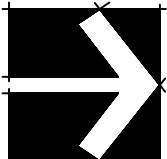
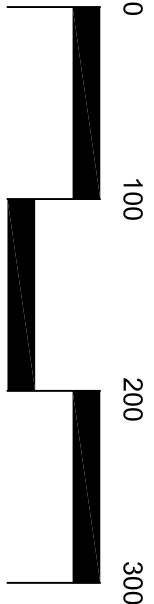
ORNAMENTAL TREES	21	31
FLOWERING TREES / LARGE SHRUBS	14	15
SHRUBS	101	136
EVERGREEN TREES	21	30

BUFFERYARD 3 - ELEMENTARY SCHOOL - BUFFERYARD TYPE B  
WIDTH - 25', LENGTH - 515'+135'=650'

CANOPY TREES	12	17
FLOWERING TREES / LARGE SHRUBS	12	12
SHRUBS	30	33
EVERGREEN TREES	6	6

#### NOTES:

1. THE REQUIRED CANOPY STREET TREES ALONG FIRST STREET ARE ORNAMENTAL TREES, SINCE THEY ARE LOCATED BENEATH OVERHEAD POWER LINES.
2. BUFFERYARD PLANTINGS ARE NOT PROPOSED ALONG NAMAQUA AVENUE, THAT PORTION OF THE PROPERTY SHALL BE DEDICATED TO THE LOVELAND HISTORICAL SOCIETY.
3. THE CALCULATIONS FOR THE BUFFERYARD ALONG THIRD STREET DO NOT INCLUDE THE PORTION OF THE PROPERTY TO BE DEDICATED TO THE LOVELAND HISTORICAL SOCIETY.



MARIANA BUTTE TWENTY FIFTH SUBDIVISION  
MOUNTAIN GATE PRELIMINARY DEVELOPMENT PLAN



970-217-9169 • www.sitedesign-llc.com • P.O. Box 51 Loveland, CO 80539

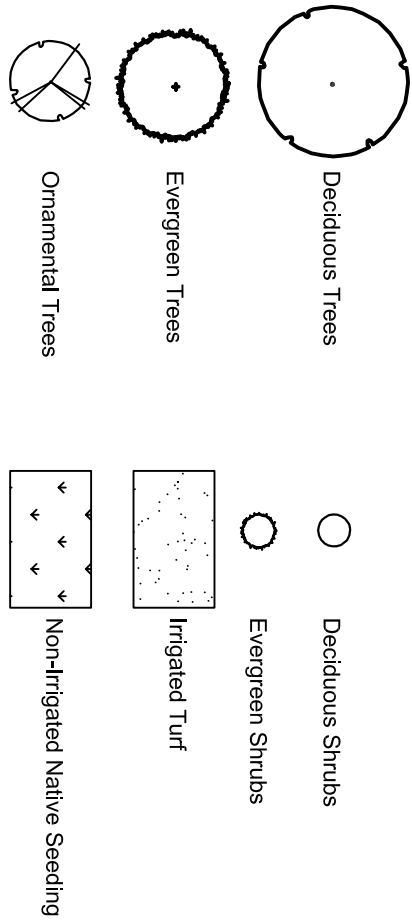
Sheet:

8

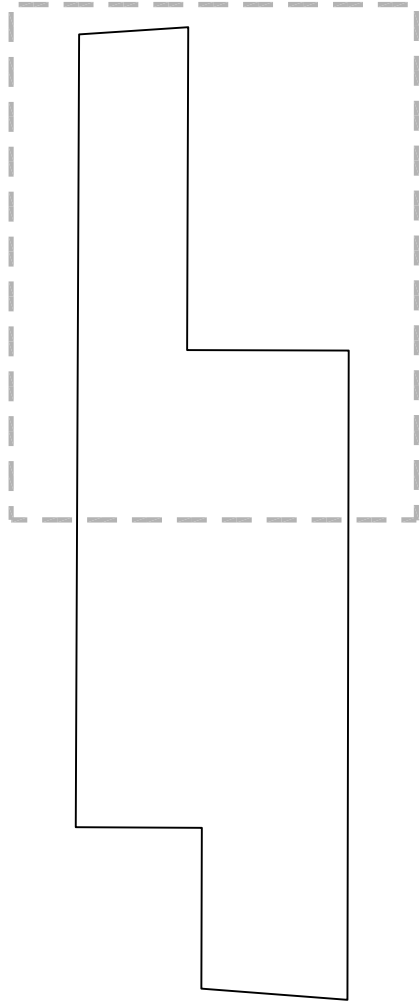
Date: 11-22-11  
Revised:  
Revised:  
Drawn By: TRB  
Scale: 1" = 100'

Preliminary  
Landscape  
Plan - 1

SYMBOLS



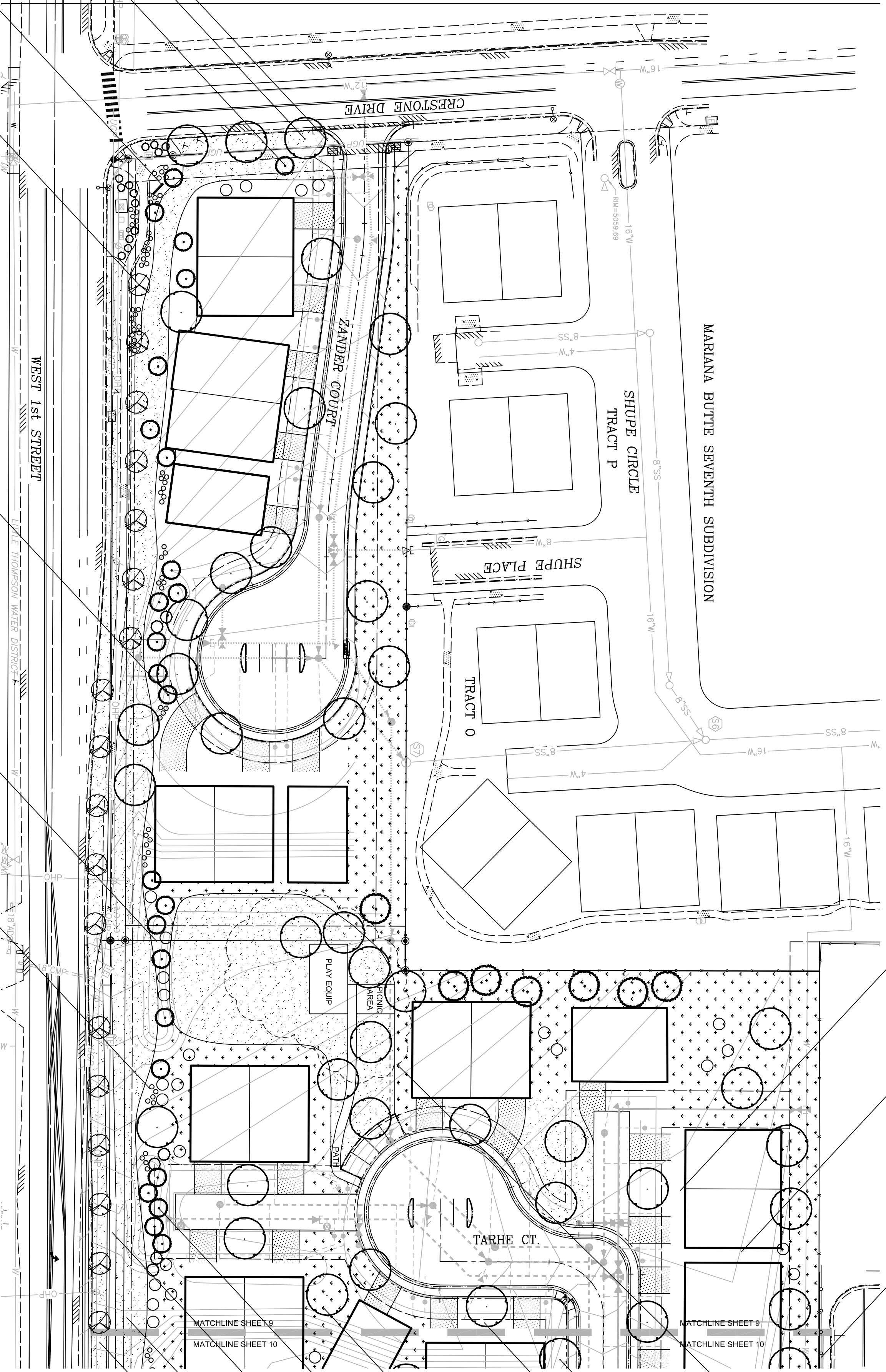
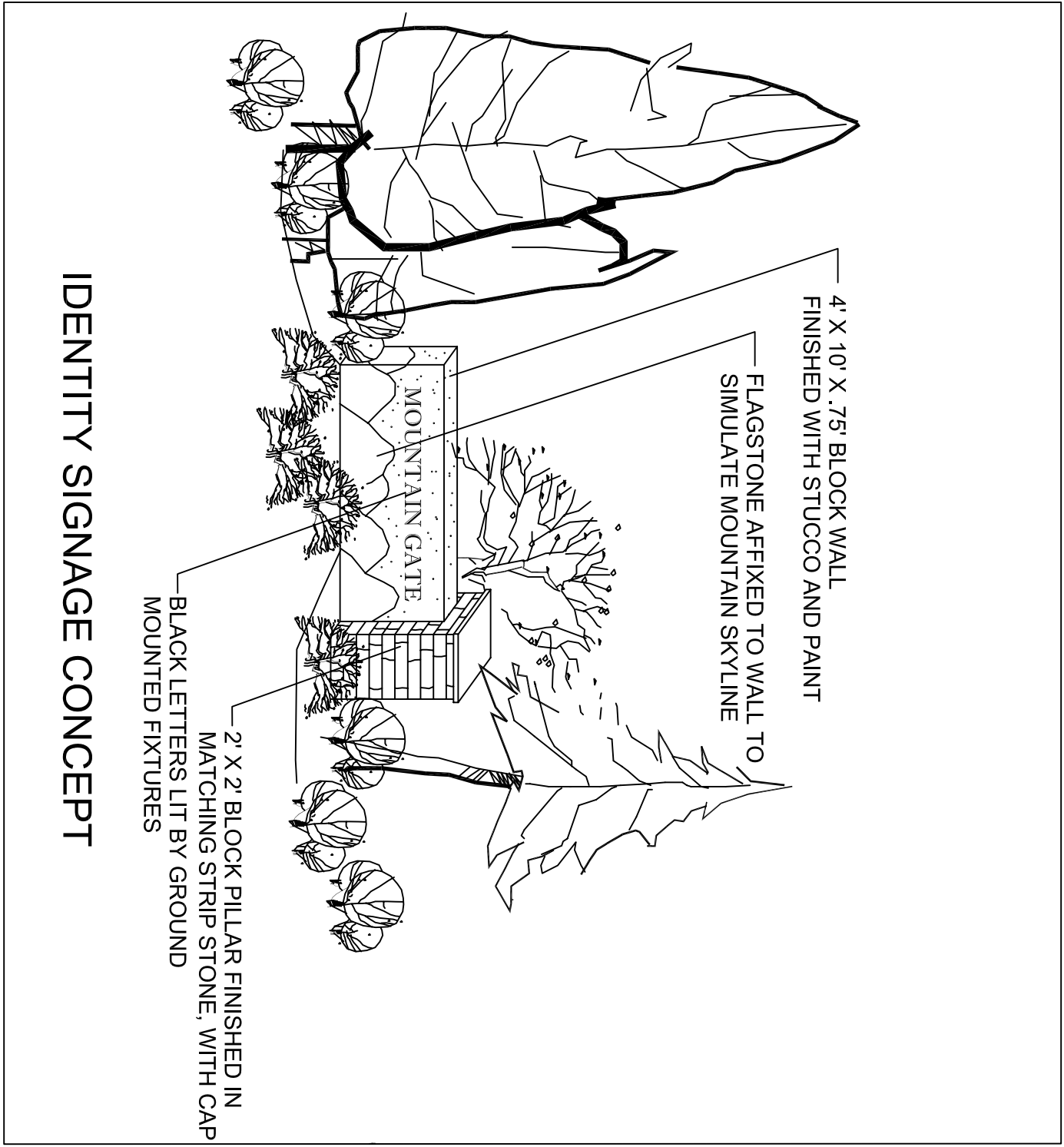
KEY



6' VINYL FENCE (SEE DETAIL SHEET 10)  
FENCE TO BE INSTALLED 1' FROM SIDEWALK  
EDGE, WITHIN 2' STRIP OF 2" ROCK

LANDSCAPE AREAS BETWEEN  
BUILDING SITES TO BE ROCK FINISHED  
SHRUB BEDS OR NATIVE SEEDING WITH  
GROUPS OF XERIC TREES AND SHRUBS

LANDSCAPING WITHIN THE  
BUILDING SITES TO BE PROVIDED  
BY THE DEVELOPER OF EACH BUILDING



RECREATIONAL AMENITIES:  
IRRIGATED TURF, PICNIC AREA  
AND PLAY EQUIPMENT

REQUIRED STREET TREES

LANDSCAPE AREAS BETWEEN EACH  
BUILDING SITE AND BACK OF SIDEWALK  
TO BE LANDSCAPED AND MAINTAINED  
BY THE OWNER OF THE BUILDING

NATIVE SEEDING WITH GROUPS  
OF XERIC TREES AND SHRUBS, TYPICAL

BERMING AND EVERGREEN TREES

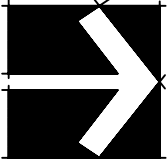
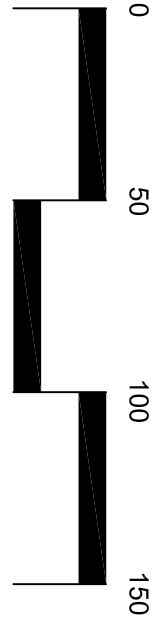
6' DETACHED SIDEWALK

IRRIGATED TURF

IRRIGATED TURF

IRRIGATED TURF  
REQUIRED STREET TREES  
BUFFERYARD 1 PLANTINGS  
SEE DESCRIPTION ON SHEET 8  
PROPOSED SIGNAGE LOCATION  
ORNAMENTAL TREES AS STREET TREES  
DUE TO OVERHEAD POWER LINES

BERMING AND EVERGREEN TREES  
BED AREAS TO BE DEFINED BY STEEL EDGING  
AND FINISHED WITH ROCK MULCH  
BUFFERYARD 2 PLANTINGS  
SEE DESCRIPTION ON SHEET 8



MARIANA BUTTE TWENTY FIFTH SUBDIVISION  
MOUNTAIN GATE PRELIMINARY DEVELOPMENT PLAN



**SITE DESIGN**  
landscape architecture and consulting

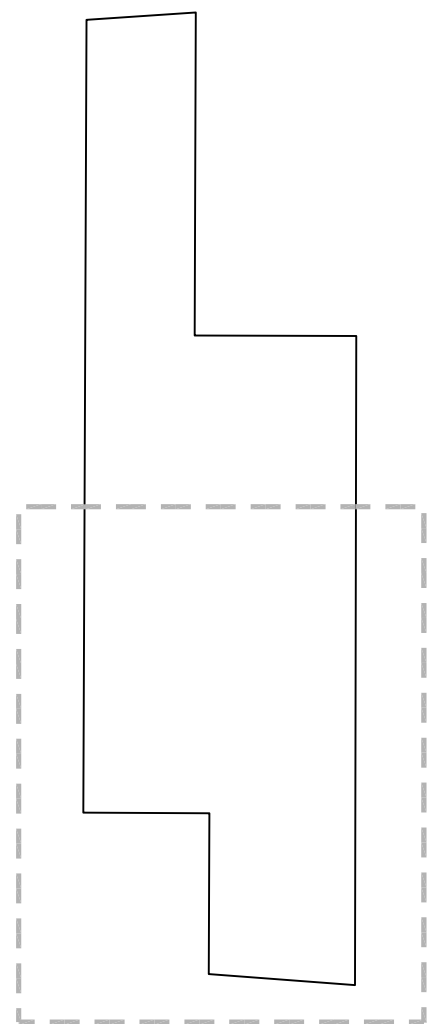
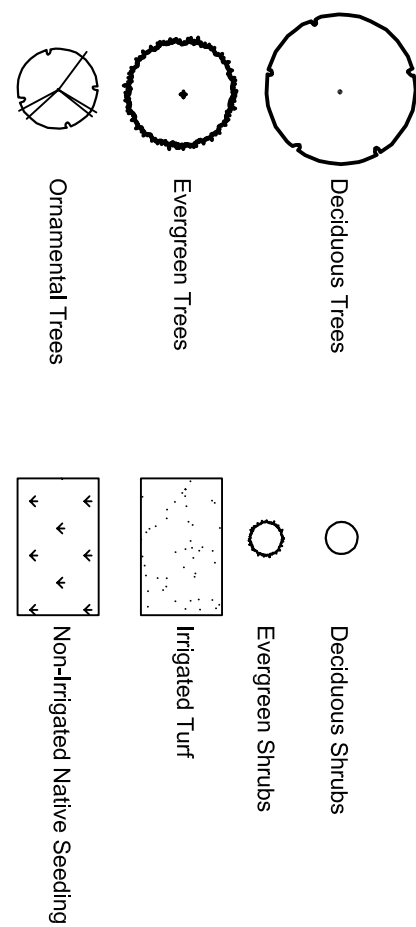
970-217-9169 • www.sitedesign-llc.com • P.O. Box 51 Loveland, CO 80539

Date: 11-22-11  
Revised:  
Revised:  
Drawn By: TRB  
Scale: 1" = 50'

Sheet:

9





6" VINYL FENCE (SEE DETAIL ON THIS SHEET). FENCE TO BE INSTALLED 1' FROM SIDEWALK EDGE, WITHIN 2" STRIP OF 2" ROCK

LANDSCAPING WITHIN THE BUILDING SITES TO BE PROVIDED BY THE DEVELOPER OF EACH BUILDING

PROPOSED SIGNAGE LOCATION UTILITIES SHALL BE POT-HOLED BEFORE CONSTRUCTION

BUFFERYARD 3 PLANTINGS SEE DESCRIPTION ON SHEET 8

NATIVE SEEDING IN DETENTION POND AREA

6' ATTACHED SIDEWALK

THIS AREA TO BE DEDICATED TO LOVELAND HISTORICAL SOCIETY

REQUIRED STREET TREES

LANDSCAPE AREAS BETWEEN BUILDING SITES TO BE ROCK FINISHED SHRUB BEDS OR NATIVE SEEDING WITH GROUPS OF XERIC TREES AND SHRUBS

LANDSCAPE AREAS BETWEEN EACH BUILDING SITE AND BACK OF SIDEWALK TO BE LANDSCAPED AND MAINTAINED BY THE OWNER OF THE BUILDING

REQUIRED STREET TREES

NATIVE SEEDING WITH GROUPS OF XERIC TREES AND SHRUBS, TYPICAL

ORNAMENTAL TREES AS STREET TREES DUE TO OVERHEAD POWER LINES

6' DETACHED SIDEWALK

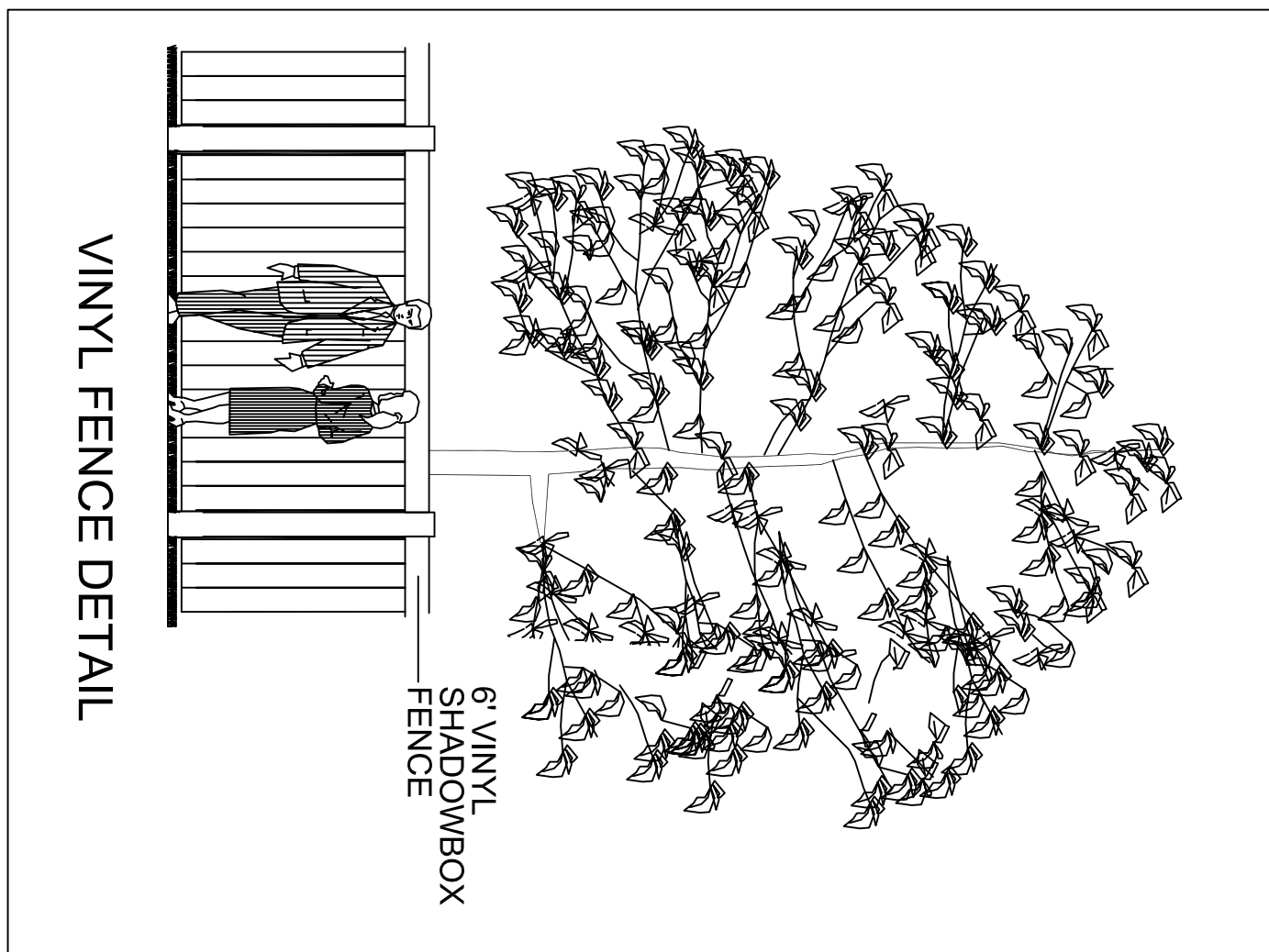
PROPOSED SIGNAGE LOCATIONS UTILITIES SHALL BE POT-HOLED BEFORE CONSTRUCTION

PROPOSED IRRIGATION METER

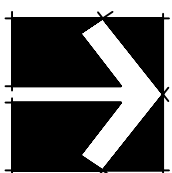
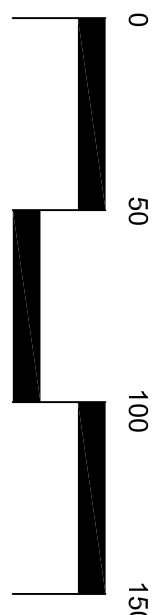
IRRIGATED TURF

EVERGREEN TREES, BERM NOT PROVIDED DUE TO GRADING CONSTRAINTS

BED AREAS TO BE DEFINED BY STEEL EDGING AND FINISHED WITH ROCK MULCH



VINYL FENCE DETAIL



MARIANA BUTTE TWENTY FIFTH SUBDIVISION  
MOUNTAIN GATE PRELIMINARY DEVELOPMENT PLAN



## Development Services Current Planning

500 East Third Street, Suite 310 • Loveland, CO 80537  
(970) 962-2523 • Fax (970) 962-2945 • TDD (970) 962-2620  
www.cityofloveland.org

### Planning Commission Staff Report

June 8, 2015

**Agenda #:** Regular Agenda - 3

**Title:** Giuliano 4<sup>th</sup> Subdivision - Preliminary Development Plan, Preliminary Subdivision Plat and Vacation of Rights-of-Way (PZ 14-174, 175, and 176)

**Applicant:** John Giuliano, Giuliano & Father Construction (represented by Landon Hoover, Hartford Homes)

**Request:** **Preliminary Development Plan, Preliminary Subdivision Plat, and Vacation of Rights-of-Way**

**Location:** Northeast corner of Cascade Avenue (to be built) and W. 43<sup>rd</sup> Street

**Existing Zoning:** P-58: Giuliano Addition Planned Unit Development (PUD)

**Staff Planner:** Troy Bliss

#### **Staff Recommendation**

**APPROVAL** of the Preliminary Development Plan, Preliminary Plat and Vacation of Rights-of-Way.

#### **Recommended Motions:**

1. *Move to make the findings listed in Section VIII of the Planning Commission staff report dated June 8, 2015, and based on those findings approve the Giuliano 4<sup>th</sup> Subdivision Preliminary Development Plan, subject to the conditions listed in Section IX, as amended on the record.*

#### **And**

2. *Move to make the findings listed in Section VIII of the Planning Commission staff report dated June 8, 2015, and based on those findings approve the Giuliano 4<sup>th</sup> Subdivision Preliminary Plat, as amended on the record.*

#### **And**

3. *Move to make the findings listed in Section VIII of the Planning Commission staff report dated June 8, 2015, and based on those findings, recommend that City Council approve the Giuliano 4<sup>th</sup> Subdivision Vacation of Rights-of-Way, subject to the conditions listed in Section IX, by adoption of an ordinance in the form attached hereto, as amended on the record.*

#### **Summary of Analysis**

Hartford Homes has applied to develop the western most 29.7 acres of the Giuliano Addition for 36 single-family lots. The application requires a public hearing and approval by the Loveland Planning Commission for the project to be developed. This report provides the Planning Commission with information and analysis pertinent to city standards, policies, and code provisions that are applicable to the application. City staff supports the project subject to recommended conditions.

The Preliminary Development Plan (PDP) and Preliminary Subdivision Plat (PP) require quasi-judicial. Vacation of Rights-of-Way is considered a legislative matter. Upon the review of all pertinent information, including testimony at the public hearing at 6:30 pm on Monday, June 8<sup>th</sup>, the Planning Commission must render a decision on the application based on findings specified in the Municipal Code. If a majority of Commissioners vote to approve the project, the application can proceed through the city's administrative review process and be developed. Conversely, if a majority of Commissioners vote to deny the project, the project is halted. Regardless of the Commission's decision, an appeal of the Commission's decision on the PDP and PP may be filed, resulting in a new public hearing on the application by the City Council.



## **I. SUMMARY**

The following abbreviations will be used throughout this report and are being provided to help clarify what each represents:

- PUD: Planned Unit Development is a type of zoning that allows for the creation of zoning and design standards that are tailored to a site with this designation. This zoning is unique to a particular site or area, typically described in some form of development plan. The primary purpose of this zoning is to encourage a mixture of land use opportunities that are well integrated in creating an efficient use of land.
- GDP: General Development Plan establishes the zoning, density and design standards for a PUD-zoned property. The plan itself is primarily conceptual in nature, meant to provide guidance with respect to locations for different land uses within a PUD. This plan is usually prepared in conjunction with the annexation of a property as its official zoning document. This plan must be approved by City Council.
- PDP: Preliminary Development Plan is typically the initial step, detailing a specific development proposal within a PUD. Its contents are reviewed against the zoning requirements of the applicable GDP. This plan must be approved by Planning Commission, subject to appeal to City Council.
- FDP: Final Development Plan is the last and subsequent step in the PUD process. Its contents are reviewed against the PDP with final details of all components associated with the project. This plan is approved administratively.
- PP: Preliminary Subdivision Plat is the initial subdivision of a property into more than 4 additional lots. It establishes all necessary conveyances (i.e. dedication of public rights-of-way and easements) for public and private use. This document must be approved by Planning Commission, subject to appeal to City Council.
- TIS: Traffic Impact Study is a study prepared by a traffic engineer to evaluate traffic impacts on a specific development proposal.
- ACF: Adequate Community Facilities is a program adopted by the City of Loveland to ensure that community facilities needed to support new development meet or exceed defined levels of service. This includes fire protection, transportation, water, wastewater, stormwater, and power.
- LMC: Loveland Municipal Code is the collection of adopted City relations, including the zoning and subdivision codes.

The applications for a PDP and PP (see **Attachments 7 and 8**) propose to develop 36 single-family lots on 29.7 acres. The project is being presented by Hartford Homes, the builder of a number of the single-family homes in the Giuliano Addition. The area of focus for these applications is referred to as SF-1 on the GDP (see **Attachment 5**). It currently is platted for 18 single-family lots along with vesting for development of 18 single-family homes – in a less dense, estate type arrangement when compared to the current applications (see **Attachment 6**). Through these applications, Hartford Homes is seeking to double the number of lots/units. The change is being made in response to market demands and overall feasibility. In conjunction, a request to vacate Fife Court and a portion of Cascade Avenue are being proposed because the currently platted rights-of-way do not align with the proposed development. New rights-of-way would be re-established with the subdivision plat as presented.

A general narrative has been provided by the applicant and incorporated into the PDP package (see **Attachment 1**). The following is a review of the various components of the development proposal,

analyzing each with respect to applicable City standards and the Giuliano Addition General Development Plan (GDP):

**Density:** The gross density of the proposed development is 1.21 units per acre. The proposed density is in keeping with the requirements of the GDP between 0.8 to 1.5 units per acre.

**Number of Units:** The proposal consists of development for 36 single-family units. This is the maximum number of units allowed within this area of the GDP.

**Traffic:** The additional traffic generated by this development proposal has been analyzed in a Traffic Impact Study (TIS) (see **Attachment 4**). General conclusions indicate that the proposed development can be accommodated by the existing street system, without the need for additional street improvements. The City's Transportation Development Review office is in agreement with this conclusion. The TIS has also concluded that the City's Adequate Community Facilities (ACF) criteria will be satisfied in order to maintain acceptable levels of service.

**Fire Access and Circulation:** The Fire Protection Division has reviewed the submitted plans and has indicated that fire protection will be served with fire trucks entering the site from both W. 43<sup>rd</sup> Street and Cascade Avenue onto the proposed reconfigured Fife Court (local street) and new John Michael Avenue (local street). The proposed internal local street network provides adequate emergency service from two points of access (i.e. W. 43<sup>rd</sup> Street and Cascade Avenue).

**Setbacks and Building Separation:** The PDP proposes staggered front setbacks ranging from 20, 22, and 24 feet from the back of sidewalk (typical of other areas in the Giuliano Addition). Other building setbacks and separations include a minimum of 5 feet along all side yards (10+ feet between structures), unless on a corner lot where the corner side would be 15 feet. And a minimum of 25 feet to all rear property lines.

**Site Access and Parking:** The PDP proposes two separate access points from W. 43<sup>th</sup> Street and Cascade Avenue linking an internal local street network. Many opportunities are proposed for pedestrian access including a detached public sidewalk along W. 43<sup>th</sup> Street and Cascade Avenue, sidewalks along all local streets, and an internal private walkway at the north end of the site connecting with other open spaces in the Giuliano Addition.

Off-street parking is provided for each dwelling unit within individual garages and driveways. Parking would also be permitted along areas of the internal local streets (i.e. Fife Court and John Michael Avenue). The provided off-street parking arrangement meets GDP requirements.

**Architecture:** The PDP proposes a combination of both single story and two story homes. It is anticipated that a majority of the homes will be single story. While building design is preliminary at this point, minimum design standards are established with the PDP such as all exteriors will be constructed of painted wood/composite siding, masonry has been included as a front wainscot on the façade and associated porch columns, fiberglass shingles would be used as the roofing material, minimum 5/12 roof pitch on all primary roof planes would be established, a minimum of 3 windows for single-story/4 windows for two-story homes is provided (using window treatments

on doors and garage doors), and incorporating earth tone colors for all homes. Design requirements contained in the PDP will be further detailed and incorporated into any subsequent Final Development Plan (FDP), prior to construction.

**Landscaping and Open Space:** A majority of the proposed development site would be devoted to landscaping and open space areas (approximately 66%). This is primarily a result of the eastern third of the site being a limestone ridge identified as being environmentally sensitive and to be preserved. Other landscaping and open space areas include detention, pedestrian trail greenbelt, and internal pocket parks for recreational use. No defined playground areas with equipment are proposed considering that this development will be marketed toward empty nesters or individuals seeking minimal maintenance responsibilities.

**Stormwater Design:** Surface run-off would be directed to two detention ponds located at both the north and south ends of the site. These ponds are designed to provide water quality measures and detain runoff before releasing to the existing drainage system east of the site.

## **II. KEY ISSUES**

From a City staff perspective, there are no key issues, except that the sidewalk placement along internal local streets is of concern. Throughout the review of these applications, City staff has taken the position that detached sidewalks along all public streets should be incorporated with this development. While there is no requirement for detached sidewalks as part of the GDP or LMC, the objectives of any PUD are (among others), “to encourage innovation in residential development...” and “encourage the building of new developments incorporating the best features of modern design”. From a City staff perspective, detached sidewalks were seen as a design element speaking to these objectives. Through further considerations and negotiations with the applicant, City staff modified the recommendation – suggesting that in addition to the proposed detached sidewalk along the east side of Fife Court, this design be carried over along all internal local streets that front open space areas (excluding other individual lots where space is limited).

This project is in keeping with the overall GDP for the Giuliano Addition. All applicable City standards and requirements will be met. The City will be able to adequately serve this proposed development in terms of street connections, utilities, and emergency access. Agencies outside of the City including the School District and utility providers have expressed no concerns over the proposal. The increase in number of units from 18 to 36 has no negative impact to the School District’s enrollment projections or school capacity. Referrals were also sent to utility providers such as Xcel Energy, Comcast, and CenturyLink with no comments provided.

## **III. ATTACHMENTS**

Supporting Documentation

1. Project Narrative (Provided by Applicant)
2. Giuliano 4<sup>th</sup> Subdivision Preliminary Development Plan Resolution

3. Giuliano 4<sup>th</sup> Subdivision – Draft Vacation of Rights-of-Way Ordinance
4. Traffic Memorandum (excluding appendices)

Plan Documents

5. Giuliano Addition General Development Plan (GDP) (Approved in 2001)
6. Giuliano First Subdivision – Final Development Plan (FDP) (Approved in 2003)
7. Giuliano 4<sup>th</sup> Subdivision - Preliminary Development Plan (Proposed)
8. Giuliano 4<sup>th</sup> Subdivision - Preliminary Subdivision Plat (Proposed)

**IV. VICINITY MAP**



**V. SITE DATA**

ACREAGE OF PUD SITE GROSS .....	29.7 AC
NUMBER OF PLATTED LOTS .....	36 (PROPOSED)
MASTER PLAN DESIGNATION .....	LDR - LOW DENSITY RESIDENTIAL
EXISTING ZONING .....	P-58: GIULIANO ADDITION PUD
EXISTING USE .....	VACANT/UNDEVELOPED
EXIST ADJ ZONING & USE - NORTH .....	P-103: PONDEROSA RIDGE ADDITION PUD –
.....	UNDEVELOPED/VACANT PARCELS

EXIST ADJ ZONING & USE - SOUTH.....	P-91: LEE FARM ADDITION PUD – UNDEVELOPED/VACANT PARCEL (PLANNED SINGLE FAMILY)
EXIST ADJ ZONING & USE - WEST.....	P-103: PONDEROSA RIDGE ADDITION PUD – UNDEVELOPED/VACANT PARCELS (PLANNED SINGLE FAMILY)
EXIST ADJ ZONING & USE - EAST.....	P-58: GIULIANO ADDITION PUD – SINGLE FAMILY RESIDENCES
UTILITY SERVICE .....	CITY OF LOVELAND

## **VI. BACKGROUND**

The site lies on the north side of W. 43<sup>rd</sup> Street at the far west end of the Giuliano Addition. This 29.7 acre property is one of three vacant/undeveloped properties in the Giuliano Addition (the other two include a multi-family component at the northeast end of the addition and a commercial component along the entire eastern edge fronting N. Wilson Avenue). The entire Giuliano Addition is located on the north side of W. 43<sup>rd</sup> Street between N. Wilson Avenue and Cascade Avenue (planned major collector along west edge). Inclusive of internal public local streets, the Giuliano Addition is approximately 159.6 acres in size. It was annexed in 2001 and has been developed primarily of single-family homes but does include the Ponderosa Elementary School. The Giuliano Addition is a planned community, having a zoning designation of Planned Unit Development (PUD). The zoning was established through a General Development Plan (GDP), approved by the City in 2001. The GDP sets forth specific land use standards and requirements including but not limited to allowable uses, density, number of units, architectural design, building heights, building setbacks, landscaping, and open space. Because the GDP is the official zoning document for the Giuliano Addition, it is the basis for which development proposals are reviewed by the City for conformance.

The initial developer and builder of the Giuliano Addition has built a majority of the single-family lots but is seeking to sell the property to Hartford Homes, who has also built a number of homes in the area. Through the GDP, the subject property is zoned for low density single-family use. As presented, the development proposal conforms to all aspects of the Giuliano Addition GDP.

## **VII. STAFF, APPLICANT, AND NEIGHBORHOOD INTERACTION**

- A. Notification:** An affidavit was received from Landon Hoover, Hartford Homes, certifying that written notice was mailed to all property owners within 900 feet of the property on May 13, 2015, and notices were posted in prominent locations on the perimeter of the site at least 15 days prior to the date of the Planning Commission hearing. In addition, a notice was published in the Reporter Herald on May 23, 2015. These notices satisfy City requirements.
- B. Neighborhood Response:** A neighborhood meeting was held at 5:30 p.m. on March 19, 2015, in the Gertrude Scott Meeting Room at the Loveland Public Library. The meeting was attended by approximately 15 neighbors, along with City staff and the applicant. Generally, neighbors were supportive of the development proposal noting that the proposed homes would be compatible existing



development in the Giuliano Addition. Neighbors were also supportive of the specific approach to market the development, proposing an age-targeted approach with a majority of the homes to be single story. Additionally, neighbors also felt that the connection of the trail system at the north end of the site and continued preservation of the limestone ridge were important elements with this project.

## **VIII. FINDINGS AND ANALYSIS**

In this section of the report, the applicable findings contained in the Municipal Code and the Comprehensive Master Plan are specified in italic print followed by the staff analysis as to whether the findings are met by the submitted applications. **The Planning Commission decision to approve or deny the PDP and PP applications must be based on a majority vote on whether or not the findings can be met.** The Planning Commission decision regarding the Vacation of Rights-of-Way is recommendation that would be forwarded to City Council.

### **Preliminary Development Plan and Preliminary Plat**

#### **A. City Utilities and Services**

##### **1. Loveland Municipal Code**

###### **a. Section 18.41.050.E.2:**

(i) *Development permitted by the PDP will not have negative impacts on City utilities. If such impacts exist, Section 18.41.050.D.4(b) of the Loveland Municipal Code requires City staff to recommend either disapproval of the PDP or reasonable conditions designed to mitigate the negative impacts.*

(ii) *Whether development permitted by the PDP will be complementary to and in harmony with existing development and future development plans for the area in which the PDP is located by incorporating public facilities or infrastructure, or cash-in-lieu, that are reasonably related to the proposed development so that the proposed development will not negatively impact the levels of service of the City's services and facilities.*

**b. Chapter 16.41:** *A positive determination of adequacy, or a positive determination of adequacy with conditions, has been made in accordance with Section 16.41.100 for fire protection and emergency rescue services, Section 16.41.120 for water facilities and services, Section 16.41.130 for wastewater facilities and services, Section 16.41.140 for storm drainage facilities, and Section 16.41.150 for power.*

#### **Fire:**

Staff believes that this finding can be met, due to the following:

- The proposed development will comply with the requirements in the ACF Ordinance for response distance requirements from the first due Engine Company.
- The Giuliano 4<sup>th</sup> Subdivision will not negatively impact fire protection for the subject development or surrounding properties.

#### **Water/Wastewater:**

The subject development is situated within the City's current service area for both water and wastewater and is also accommodated in the City's Water and Wastewater Master Plans.

Regarding water, the development lies entirely within the City's boosted P2 Pressure zone. Typically, finished floor elevations situated above finished floor elevations of 5170 and above will lie within this zone. At this time there is a small water booster station known as Buck Pump Station that serves the portions of Buck and Giuliano Subdivisions within this boosted P2 pressure zone. The subject area was initially included in the sizing of the Buck Pump Station for 20 lots. The proposed subdivision will increase the originally platted 18 lots to 34 lots. The City has made a determination that existing Buck Pump Station has the capacity for these additional 18 lots.

The current version of the City's water Master Plan requires the extension of a 12" water main through (or adjacent to) the development extended to the north property line of the development.

Regarding wastewater, the development will connect to an existing 8" wastewater main from the Giuliano 1st Subdivision extended to the site.

The Department finds that the Development will be compliant to ACF for the following reasons:

- The proposed development will not negatively impact City water and wastewater facilities. The proposed development is in harmony with existing and future development and incorporates public infrastructure designed so that the proposed development will not negatively impact the levels of service of the City utilities adjacent to the development.
- The proposed facilities shown on the Preliminary PICPs have been designed pursuant to the City's Development Standards.
- The proposed facilities have been designed to minimize flood damage and infiltration.

**PW-Stormwater:**

Staff believes that this finding can be met, due to the following:

- When final designed and built, the development will not negatively impact City storm drainage utilities and will comply with the Adequate Community Services ordinance outlined in the Loveland Municipal Code, Section 16.41.140.
- No irrigation ditches traverse the site.
- No natural drainage courses/open channels traverse the site.

**Power:**

Staff believes that this finding can be met, due to the following:

- The proposed development will have no negative impact on the City electric utilities. The proposed development meets the criteria for level of service as outlined in the ACF ordinance, Section 16.41.150. There is a 200 amp 3-phase vault located on the northeast corner of W. 43<sup>rd</sup> Street and Stump Avenue. The power from this vault can be extended to the proposed project at the developer's cost. There also are 2-4" conduit extended to the

west under the intersection that can be used for extending power to the proposed project.

## **B. Transportation**

### **1. Section 18.41.050.E.2:**

- a. *Development permitted by the PDP will not have negative impacts on traffic in the area. If such impacts exist, Section 18.41.050.D.4(b) of the Loveland Municipal Code requires City staff to recommend either disapproval of the PDP or reasonable conditions designed to mitigate the negative impacts.*
- b. *Whether development permitted by the PDP will be complementary to and in harmony with existing development and future development plans for the area in which the PDP is located by incorporating public facilities or infrastructure, or cash-in-lieu, that are reasonably related to the proposed development so that the proposed development will not negatively impact the levels of service of the City's services and facilities.*

### **2. Section 16.41.110:** *A positive determination of adequacy, or a positive determination of adequacy with conditions, has been made for transportation facilities in accordance with Chapter 16.41 of the Loveland Municipal Code.*

## **Transportation Engineering:**

Staff believes that this finding can be met, due to the following:

- A Traffic Impact Study (TIS) has been submitted with the Giuliano 4th Subdivision Preliminary Development Plan (PDP) and Preliminary Plat applications which demonstrates that the transportation system, incorporating the required roadway improvements, can adequately serve the land uses proposed.
- Access to the site will be from two proposed full movement access points. One proposed access to 43rd Street and the access onto Cascade Avenue.
- The applicant's traffic engineer, Joseph Delich, P.E., has submitted a Traffic Impact Study (TIS) that indicates that the traffic associated with the proposed development will meet the City's standards. The proposed Giuliano 4th Subdivision is estimated to generate approximately 410 daily trips, 35 weekday AM peak hour trips, and 42 weekday PM peak hour trips.

In conclusion, the development of the subject property pursuant to any of the uses permitted by right under the zoning district will not adversely impact any existing City infrastructure when typical roadway improvements are constructed. A positive determination of adequacy for transportation facilities for the proposed PDP has been made under the provisions of paragraph ii, above.

All required infrastructure improvements and the extension of public utility services will be at the cost of the developer for installation. This is an important distinction because it allows the City to make the findings as stated above in Sections A and B.

## **C. Land Use**

### **1. Loveland Comprehensive Master Plan**

## **Section 4.2 -Growth Management Plan – Residential Land Use**

*Objectives for residential land uses are provided below. Descriptions of residential land use categories are provided in Section III of this Plan.*

**RES1:** *Orderly development which is phased and coordinated with the community's fiscal and service capacity is encouraged.*

*1.A A consistent & balanced relationship between the Land Use pattern & capacity of streets, utilities, and community services should be met so that those systems are not overburdened.*

*1.B Urban development proposals should be contiguous to existing development within the city limits.*

**RES2:** *Development should only be permitted where provision of facilities and services (i.e., police, fire, water, sewer, parks, schools, roads, communications systems, etc.) will be made available in a timely manner.*

**RES3:** *The development of a full range of housing types to meet the needs of all age and socio-economic groups is encouraged.*

**RES4:** *A mix of housing densities throughout the City is encouraged.*

**RES5:** *Quality design and compatible land use relationships with all proposed and existing developments is encouraged.*

**RES6:** *Residential development in areas which have been officially designated as floodplain areas is discouraged.*

**RES7:** *Pedestrian and bicycle friendly development is encouraged by considering among other things:*

*7A. Walking or biking distance to an existing or planned neighborhood park and within easy access to a community park;*

*7B. Easy access to major employment centers;*

*7C. Walking or biking distance and safe accessibility to an existing or planned elementary school;*

*7D. Easy access to existing or planned neighborhood shopping centers.*

**RES8:** *Energy-conscious land use and site planning practices are encouraged.*

**RES9:** *Applicable elements of the Open Lands Plan and Parks and Recreation Master Plan should be considered when evaluating in residential development proposals.*

*9A. High value habitat that allows wildlife movement should be protected.*

*9B. Mitigation measures, such as buffer standards, may need to be taken in such areas as the Big Thompson River Corridor, designated wetlands, and identified natural areas to offset or accommodate the impacts of development.*

**RES10:** *Residential development proposals are encouraged where appropriate to incorporate the “clustering” of units to promote open space.*

**RES11:** *Motor vehicle access to low density lots should be from local streets (not collectors).*

**RES12:** *The developer of a residential project should consider assembling available land parcels and prepare a master plan design for the larger area, rather than submit separate individual proposals.*

**RES13:** *Private property rights should be considered when evaluating in residential development proposals.*

**RES14:** *Businesses and home occupations should be allowed in residential areas that are unobtrusive and compatible with residential uses.*

### **2. Section 18.41.050.E.2:**

*The PDP conforms to the intent and objectives of Title 18 with regard to Planned Unit Developments and any applicable area plan.*

**Current Planning:** Staff believes that this finding can be met based on the following facts:

- The PDP is supporting infill development opportunities. While everything around the site has not been developed, all undeveloped areas do have development entitlements/vesting and capable of having public services extended.
  - The recommended land use and density for this area of the City is LDR. This category recommends an overall residential density of 2-4 dwelling units per acre. The gross density of the PDP is 1.21 dwelling units per acre, keeping with the LDR land use classification.
  - The type of units being proposed (i.e. patio homes) is geared towards providing another type of housing opportunity.
  - With the inclusion of the recommended condition regarding detached sidewalks, City staff believes that the project would be creating a more pedestrian neighborhood. The proposed greenbelt with trail at the north site, links to other open space areas in the Giuliano Addition as well as to the Ponderosa Elementary School. Streets built with this development will also include designated bike lanes.
  - A clustering design is being incorporated due to the amount and nature of open space areas. For example, the preservation of the limestone ridge is a key natural physical feature associated with this site.
3. **Section 18.41.050.E.2:** *Development permitted by the PDP will not have detrimental impacts on property that is in sufficient proximity to the PDP to be affected by it. If such impacts exist, Section 18.41.050.D.4(b) of the Loveland Municipal Code requires City staff recommend either disapproval of the PDP or reasonable conditions designed to mitigate the negative impacts.*
4. **Section 16.20.030:** *The subdivision does not create, or mitigates to the extent possible, negative impacts on the surrounding property.*

**Current Planning:** Staff believes that this finding can be met due to the following:

Section 16.41.110, the Adequate Community Facilities (ACF) ordinance, requires a proposed development to comply with one of the five standards in order for a positive determination of adequacy to be made: (i) all transportation facilities are currently in place or will be in place prior to issuance of a building permit for the development; **OR** (ii) provision of transportation facilities are a condition of the development approval and are guaranteed to be provided at or before the approval of a final plat or issuance of the first building permit for the proposed development; **OR** (iii) transportation facilities are under construction and will be available at the time that the impacts of the proposed development will occur; **OR** (iv) provision of transportation facilities needed to achieve the Adopted Level of Service are guaranteed by an executed and enforceable development agreement which ensures that such facilities will be in place at the time that the impacts of the proposed development will occur; **OR** (v) transportation facilities needed to achieve the adopted level of services are included in the Capital Improvement Program (CIP) and the CIP contains a financially feasible funding system from available revenue sources to fund the needed improvements and the transportation facilities are likely to be constructed and available at the time that the impacts of the proposed development will occur, or at the time the City extends the transportation facilities to provide a logical link to the project.



Two access points onto W. 43<sup>rd</sup> Street and Cascade Avenue (to be built with this project along west edge) are proposed for this development with each access providing ingress/egress to an internal local street system. The developer's traffic engineer has prepared a Transportation Impact Study (TIS) to analyze the traffic impacts as well as the local road network that service the site (see **Attachment 4**). The TIS concluded that the proposed development can be accommodated by the existing street system, without the need for additional improvements. Additionally, the City's Adequate Community Facilities (ACF) criteria will be satisfied in order to maintain acceptable levels of service.

5. **Section 18.41.050.E.2:** *Development permitted by the PDP will be complementary to and in harmony with existing development and future development plans for the area in which the PDP is located by:*
  - a. *Incorporating natural physical features into the PDP design and providing sufficient open spaces considering the type and intensity of proposed land uses.*
  - b. *Incorporating site planning techniques that will foster the implementation of the Loveland Comprehensive Master Plan.*
  - c. *Incorporating physical design features that will provide a transition between the project and adjacent land uses through the provisions of an attractive entryway, edges along public streets, architectural design, and appropriate height and bulk restrictions on structures.*
  - d. *Incorporating an overall plan for the design of the streetscape within the project, including landscaping, auto parking, bicycle and pedestrian circulation, architecture, placement of buildings and street furniture.*
6. **Section 16.20.030:** *The subdivision provides desirable settings for buildings, protects views, and affords privacy, protect from noise and traffic, and uses resources such as energy and water in keeping with responsible resource stewardship.*

**Current Planning Division:** Staff believes that this finding can be met due to the following:

- There is one substantial natural physical feature on the site – a limestone ridge along the eastern boundary which is to be preserved.
  - The allowance for somewhat higher density is generally recognized as a positive factor for encouraging efficient land use and a balanced transportation system. The location of the site along major collectors (i.e. W. 43<sup>rd</sup> Street and Cascade Avenue) will provide immediate connection to the City's primary transportation system, without the need to travel through existing neighborhoods to reach a collector or minor arterial streets.
  - The landscape buffers included in the landscape plan provide appropriate transitions to the development – giving separation from all property lines to existing development, future development and natural physical features. The type and density of plantings within the buffer yards will also help provide a sense of privacy and add to the overall landscape of the subdivision.
  - Considering the context of a single-family development, the scale and architectural character of the proposed buildings, lends itself to a similarity with adjacent and nearby single-family development. The homes will be designed in one and two story fashions, on individual lots, using similar architectural detailing as prescribed in the GDP.
7. **Section 18.41.050.E.2:** *The PDP complies with applicable land use and development regulations in effect as of the date that the GDP was approved and any land use and development*

*regulations adopted by the City after that date if the Planning Division and Planning Commission expressly find that compliance with such regulations is necessary to protect public health, safety, and welfare.*

**Current Planning Division:** Staff believes that this finding is not applicable due to the following:

- The zoning regulations for the Giuliano Addition are predominately specified in a General Development Plan (GDP) that was created at the time the 159.6 acres was annexed into the City. City staff has reviewed the proposed development against all standards and requirements of the GDP – finding that it is in full compliance.
8. **Section 16.20.030:** *The lots and tracts are laid out to allow efficient use of the property to be platted.*
9. **Section 16.24.050:** *All lots comply with the standards set forth in the GDP and, to the extent practical, lot lines are at right angles to the street line or at right angles to the tangent of the curve of the street line.*

**Current Planning Division:** Staff believes that this finding can be met due to the following:

- Lots and tracts are laid out to allow an efficient use of the property and maximize open space.
10. **Section 16.24.120:**
- a. *Landscaping complies with the requirements set forth in the GDP and buffer yards required pursuant to the GDP are within separate tracts of land, separate from individual residential lots.*
  - b. *Street trees are located in compliance with the City's Site Development Performance Standards and guidelines, unless waived by the Director.*
  - c. *The subdivision plat includes open space fields in compliance with the requirements set forth in Section 16.24.150, unless waived by the Planning Commission. The open space play fields are designed with respect to size, dimension, topography, and general character to be suitable for outdoor play activities*

**Current Planning Division:** Staff believes that this finding can be met due to the following:

- Landscaping will comply with the requirements set forth in the City's Site Development Performance Standards by including landscape buffer yards around the perimeter of the site.
- Open space primarily consists of internal private common areas. Per Section 16.24.150 of the Loveland Municipal Code, common open spaces also will include play fields/areas required for subdivision's containing more than fifty (50) single family lots. Since this proposed development is not containing more than 50 single-family lots, designated play fields/areas are not provided. However, internal pocket park areas are being provided as well as open space areas surrounding the homes, including a greenbelt with trail at the north end of the site.

#### **D. Environmental Impacts:**

1. **Section 18.41.050.E.2:** *The PDP incorporates environmentally sensitive areas, including but not limited to wetlands and wildlife corridors, into the project design. "Environmentally sensitive areas" are defined in Section 18.41.110 as: slopes in excess of 20%; floodplain; soils*

*classified as having high water table; soils classified as highly erodible, subject to erosion or highly acidic; land incapable of meeting percolation requirements, land formerly used for land fill operations or hazardous industrial use; fault areas; stream corridors; estuaries; mature stands of vegetation; aquifer recharge and discharge areas; habitat for wildlife; and other areas possessing environmental characteristics similar to those listed above.*

**Current Planning:** Staff believes that this finding can be met based on the following facts:

This section indicates that the City will identify, preserve, protect and enhance natural areas and other significant lands; and, that innovative techniques will be used which accomplished the objectives while respecting the rights of private property owners. There is a significant natural physical feature on the site (a limestone ridge) that would be preserved in conjunction with this proposed development.

**Vacation of Public Rights-of-Way**

Chapter 16.36, Section 16.36.010.B

1. *That no land adjoining any right-of-way to be vacated is left without an established public or private right-of-way or easement connecting said land with another reestablished public or private right-of-way:*

In order to comply with this provision of the Municipal Code, in conjunction with the Preliminary Development Plan and Preliminary Plat applications, new rights-of-way for both Fife Court and Cascade Avenue will be dedicated. The vacation is occurring because the existing rights-of-way do not match the proposed development plan.

2. *That the right-of-way or easement to be vacated is no longer necessary for the public use and convenience.*

The right-of-way is not necessary for public use in its current configuration – especially considering that they are not even built or being used.

**Development Review Team Analysis**

**Current Planning**

The change in subdivision design is prompting the change in dedicated rights-of-way associated with Fife Court and a portion of Cascade Avenue. Since these rights-of-way are not built and do not include services like public utilities, vacating the rights-of-way would have no impact. Additionally, the associated Preliminary Plat would rededicate new rights-of-way to accommodate the proposed development.

**Transportation Development Review**

The vacation of portions of the existing public rights-of-way on Cascade Avenue and the turnaround at the north end of Fife Court as shown on the proposed Giuliano Fourth Subdivision Plat will not negatively impact the City's public street network. These portions of existing rights-of-way are no longer necessary with the redesign of the subdivision.

**Fire**

Staff believes that this finding can be met, due to the following:

- The vacation of the existing 12' strip of right of way along Cascade Avenue due to newly proposed lot layout of subdivision will not negatively impact fire protection or fire response time to this development or surrounding properties.

#### Water/Wastewater

The subject Right-of-Way to be vacated is the City's current service area for both water and wastewater. There are no existing water mains and/or wastewater mains in the area to be vacated. Since The Department finds that:

- The existing right-of-way to be vacated does not impact the existing water and wastewater utility configuration within and adjacent to this development.
- The existing right-of-way to be vacated is no longer necessary for public use and convenience.

#### Stormwater

Staff believes that this finding can be met, due to the following:

- The existing 12' of right-of-way along Cascade Avenue is not needed to convey Stormwater and thus is not necessary for the public use and conveyance of Stormwater.

#### Power

The existing rights-of-way do not include any power facilities. Vacating the rights-of-way would have no impact to the power that would serve the proposed development.

## **IX. RECOMMENDED CONDITIONS**

### **Preliminary Development Plan**

#### **Current Planning**

1. The detached sidewalk as presented along the east side of Fife Court shall be incorporated along all local streets where abutting open space areas.
2. Final architectural elevations of the homes shall be provided in conjunction with the Final Development Plan in conformance with the design provisions of the Giuliano Addition General Development Plan.

#### **Transportation Engineering**

3. All public improvements within the Preliminary Development Plan (PDP) and Preliminary Plat shall comply with the Larimer County Urban Area Street Standards (LCUASS).
4. Unless designed and constructed by others, the Developer shall design and construct the following public improvements prior to the issuance of any building permits within the PDP, unless otherwise approved pursuant to the provisions in Section 16.40.010.B of the Loveland Municipal Code:
  - a. 43rd Street. The Developer shall construct the portion of West 43rd Street adjacent to the PDP to the LCUASS 2-lane arterial street standard.

b. Cascade Avenue. The Developer shall design and construct that portion of Cascade Avenue adjacent to the PDP to the LCUASS major collector street standard. The City may accept cash in-lieu for all or a portion of the construction costs for such improvements, in a form acceptable to the City Attorney, if approved in writing by the City Engineer.

### **Vacation of Rights-of-Way**

#### **Current Planning**

1. Vacation of Fife Court and a portion of Cascade Avenue shall be subject to approval of the Giuliano 4<sup>th</sup> Subdivision Preliminary Development Plan and Preliminary Plat.



### **Giuliano First Subdivision Narrative – Fourth Amendment**

Hartford Homes in conjunction with Giuliano & Father Construction, Inc. requests a PDP Major Amendment in Giuliano First Subdivision Area SF-1 to create a community of detached patio homes (on single-family lots). The Amendment will create 36 approximately 65'x120' patio home lots that are highly integrated with the natural landscape. The Amendment will increase open space in the subdivision by approximately 2.27 acres, creating a more natural and feathered transition to the foothills.

Previously, SF-1 consisted of 18 estate lots. Hartford sees a diminished demand for this type of lot/product and an increased need for the proposed more affordable, age-targeted product. The proposed Amendment will have minimal impacts on local traffic and infrastructure relative to the originally proposed, and simply provides a more current and needed product to Loveland.

---

Landon Hoover, Hartford Homes

## **RESOLUTION #15-03**

### **A RESOLUTION OF THE LOVELAND PLANNING COMMISSION APPROVING THE GIULIANO 4<sup>TH</sup> SUBDIVISION PRELIMINARY DEVELOPMENT PLAN, LOCATED WITHIN THE P-58 GIULIANO ADDITION PUD, CITY OF LOVELAND, LARIMER COUNTY, COLORADO**

**WHEREAS**, a General Development Plan (“GDP”) for the P-58 Giuliano Addition Planned Unit Development (“PUD”) was signed by the City on September 5, 2000; and

**WHEREAS**, a Preliminary Development Plan (“PDP”) for the P-58 Giuliano Addition PUD was submitted to the Planning Commission for consideration pursuant to Chapter 18.41 of the Loveland Municipal Code (“City Code”); and

**WHEREAS**, the Giuliano 4<sup>th</sup> Subdivision Preliminary Plat (the “Preliminary Plat”) for the same portion of P-58 Giuliano Addition PUD was also submitted to the Planning Commission for consideration pursuant to City Code Section 16.20.060; and

**WHEREAS**, the applications for the PDP and the Preliminary Plat were considered as a joint application under Code Section 18.41.080; and

**WHEREAS**, on June 8, 2015, pursuant to Section 18.41.050.E.2. of the City Code and after due notice had been given, the Planning Commission held a public hearing regarding the PDP and Preliminary Plat; and

**WHEREAS**, at said hearing the recommendations of the Current Planning Division were received and duly considered by the Planning Commission, as well as all necessary testimony by the applicant, Hartford Homes, through its representative, Landon Hoover, and its consultants, and the public; and

**WHEREAS**, the Planning Commission considered the application for the PDP in light of the intent and objectives of Chapter 18.41 of the City Code, and more specifically the factors set forth in Sections 18.41.050.E.2.a. through c., and expressly including those set forth in Sections 18.41.050.D.4.b. and c., and determined that the PDP does satisfy these factors and shall be approved; and

**WHEREAS**, the Planning Commission approved the Preliminary Plat because the PDP was approved.

**NOW, THEREFORE, BE IT RESOLVED BY THE PLANNING COMMISSION  
FOR THE CITY OF LOVELAND, COLORADO:**

**(INSERT FINDINGS HERE)**

**ADOPTED** this 8<sup>th</sup> day of June, 2015.

PLANNING COMMISSION:

---

John Crescibene, Chairperson

ATTEST:

---

Planning Commission Secretary

APPROVED AS TO FORM:

---

Assistant City Attorney

DRAFT

**FIRST READING:** July 7, 2015

**SECOND READING:** \_\_\_\_\_

**ORDINANCE NO.** \_\_\_\_\_

**AN ORDINANCE VACATING PUBLIC RIGHT-OF-WAY FOR FIFE COURT AND A PORTION OF PUBLIC RIGHT-OF-WAY FOR WITH CASCADE AVENUE, LOCATED WITHIN THE GIULIANO FIRST SUBDIVISION, CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO**

**WHEREAS**, the City Council, at a regularly scheduled meeting, considered the vacation of a portion of the public right-of-way described below, located within the Giuliano First Subdivision, City of Loveland, County of Larimer, Sate of Colorado; and

**WHEREAS**, the City Council finds and determines that no land adjoining any right-of-way to be vacated will be left without an established public or private right-of-way connecting said land with another established public or private right-of-way or easement; and

**WHEREAS**, the City Council finds and determines that the public right-of-way to be vacated is no longer necessary for the public use and convenience; and

**WHEREAS**, the City Council further finds and determines that the application filed at the City's Current Planning Division was signed by the owners of more than fifty percent of property abutting the right-of-way to be vacated.

**NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF LOVELAND, COLORADO:**

**Section 1.** That the City Council hereby adopts and makes the findings set forth above.

**Section 2.** That, based on such findings, the following described portion of public right-of-way be and the same is hereby vacated:

Fife Court – Giuliano First Subdivision

and

A portion of Cascade Avenue – Giuliano First Subdivision more particularly described as:

A tract of land situate in the South 1/2 of Section 33, Township 6 North, Range 69 West of the Sixth P.M., City of Loveland, County of Larimer, State of Colorado, which, considering the South line of said Section 33 as bearing S89°20'42"W and with all bearings contained herein relative thereto, is contained within the boundary lines which begin at the Northwest Corner of Tract U as shown on the recorded plat of Giuliano First Subdivision, and run thence along the existing West Right-of-way line of Cascade Avenue, S01°27'25"E 1225.41 feet, and again along

the arc of a 12.00 foot radius curve concave to the Northeast a distance of 18.68 feet, whose central angle is 89°11'11", the long chord of which bears S46°03'21"E 16.85 feet to a point on the existing North right-of-way line of West 43rd Street; thence along said existing North right-of-way line, S89°20'42"W 12.00 feet; thence along the arc of a 12.00 foot radius curve concave to the Northeast a distance of 18.68 feet, whose central angle is 89°11'53", the long chord of which bears N46°03'22"W 16.85 feet; thence N01°27'25"W 1225.40 feet; thence N89°18'33"E 12.00 feet to the point of beginning, containing 14,845 square feet, more or less.

**Section 3.** That as provided in City Charter Section 4-9(a)(7), this Ordinance shall be published by title only by the City Clerk after adoption on second reading unless the Ordinance has been amended since first reading in which case the Ordinance shall be published in full or the amendments shall be published in full. This Ordinance shall be in full force and effect ten days after its final publication, as provided in City Charter Section 4-8(b).

**Section 4.** That the City Clerk is hereby directed to record this Ordinance with the Larimer County Clerk and Records after its effective date in accordance with State Statutes

ADOPTED this \_\_\_\_ day of July, 2015.

**CITY OF LOVELAND, COLORADO:**

\_\_\_\_\_  
Cecil A. Gutierrez, Mayor

**ATTEST:**

\_\_\_\_\_  
City Clerk

APPROVED AS TO FORM:

  
\_\_\_\_\_  
Assistant City Attorney

AN ORDINANCE VACATING A PUBLIC RIGHT-OF-WAY FOR FIFE COURT AND A PORTION OF PUBLIC RIGHT-OF-WAY FOR CASCADE AVENUE, LOCATED WITHIN THE GIULIANO FIRST SUBDIVISION, CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO



**DELICH ASSOCIATES** Traffic & Transportation Engineering

2272 Glen Haven Drive Loveland, Colorado 80538  
Phone: (970) 669-2061 Fax: (970) 669-5034

**MEMORANDUM**

**TO:** Landon Hoover, Hartford Homes  
Sean Kellar, City of Loveland Engineering Division

**FROM:** Joseph Delich

**DATE:** June 10, 2014

**SUBJECT:** Enchantment Ridge Traffic Impact Study  
(File: 1441ME01)



The Loveland Engineering Division has requested a traffic memorandum, as described in the Larimer County Urban Area Street Standards (LCUASS), for the proposed Enchantment Ridge, located west of Stump Avenue on the north side of West 43<sup>rd</sup> Street in Loveland, Colorado. The site location is shown in Figure 1. A scoping discussion over the phone was held with Sean Kellar, Loveland Engineering Division and a Memorandum Traffic Impact Study was requested.

Forty-Third Street is classified as a two-lane arterial street in this area. There are curb returns into the Enchantment Ridge (Fife Court) that will provide access to this development. When this occurs, at the 43<sup>rd</sup>/Fife intersection, 43<sup>rd</sup> Street will have a two-way left-turn lane and a through lane in each direction. Fife Court will have has one lane in each direction at this intersection. This intersection is considered to be a minor intersection. The posted speed on 43<sup>rd</sup> Street is 40 mph.

Recent weekday peak hour traffic counts at the 43<sup>rd</sup>/Stump intersection are shown in Figure 2. The 43<sup>rd</sup>/Stump intersection is east of the Enchantment Ridge property. Raw count data is provided in Appendix A. These peak hour counts were obtained in May 2014. The 43<sup>rd</sup>/Stump intersection was evaluated using techniques provided in the 2010 Highway Capacity Manual (for information only). Table 1 shows the operation at the 43<sup>rd</sup>/Stump intersection during the morning and afternoon peak hours. Calculation forms are provided in Appendix B. This intersection operates acceptably during the morning and afternoon peak hours. Acceptable operation is defined by the City of Loveland as level of service (LOS) C or better overall. At minor intersections any leg can operate at level of service E and any movement can operate at level of service F. A description of level of service at unsignalized intersections is also provided in Appendix B. The Loveland Motor Vehicle LOS Standards are also provided in Appendix B.

The site plan for the Enchantment Ridge development is shown in Figure 3. Figure 3 also shows the previous site plan on this parcel. Enchantment Ridge is proposed at 36 dwelling units with access to 43<sup>rd</sup> Street and future Cascade Avenue. The initial, primary

access will be to 43<sup>rd</sup> Avenue. The trip generation of the proposed Enchantment Ridge was calculated using the reference document Trip Generation, 9<sup>th</sup> Edition, ITE. Single Family Residential (Code 210) was used to estimate the trip generation of the site. Table 2 shows the calculated trip generation to/from the site on a daily and peak hour basis. The trip generation resulted in 410 daily trip ends, 35 morning peak hour trip ends, and 42 afternoon peak hour trip ends. The approved/previous development on this parcel had 18 dwelling units with a single access at the 43<sup>rd</sup>/Fife intersection. The calculated trip generation for the approved development was: 216 daily trip ends, 22 morning peak hour trip ends, and 22 afternoon peak hour trip ends. While the new proposal will generate more traffic, the increase is not significant.

The trip distribution for the Enchantment Ridge is shown in Figure 4. The trip distribution was determined using the existing traffic counts, knowledge of the existing and planned street system, development trends, and engineering judgment. Figure 5 shows the site generated traffic that is associated with the proposed Enchantment Ridge development. For this exercise, it was conservatively assumed that all site generated traffic would use the 43<sup>rd</sup>/Fife intersection.

Pending approvals, it is expected that the proposed Enchantment Ridge would be built in the next 1-2 years. Therefore, the short range future analysis year is considered to be 2017. Traffic on 43<sup>rd</sup> Street was increased at the rate of 2.0% per year. Figure 6 shows the short range (2017) background peak hour traffic on 43<sup>rd</sup> Street at the future 43<sup>rd</sup>/Fife intersection.

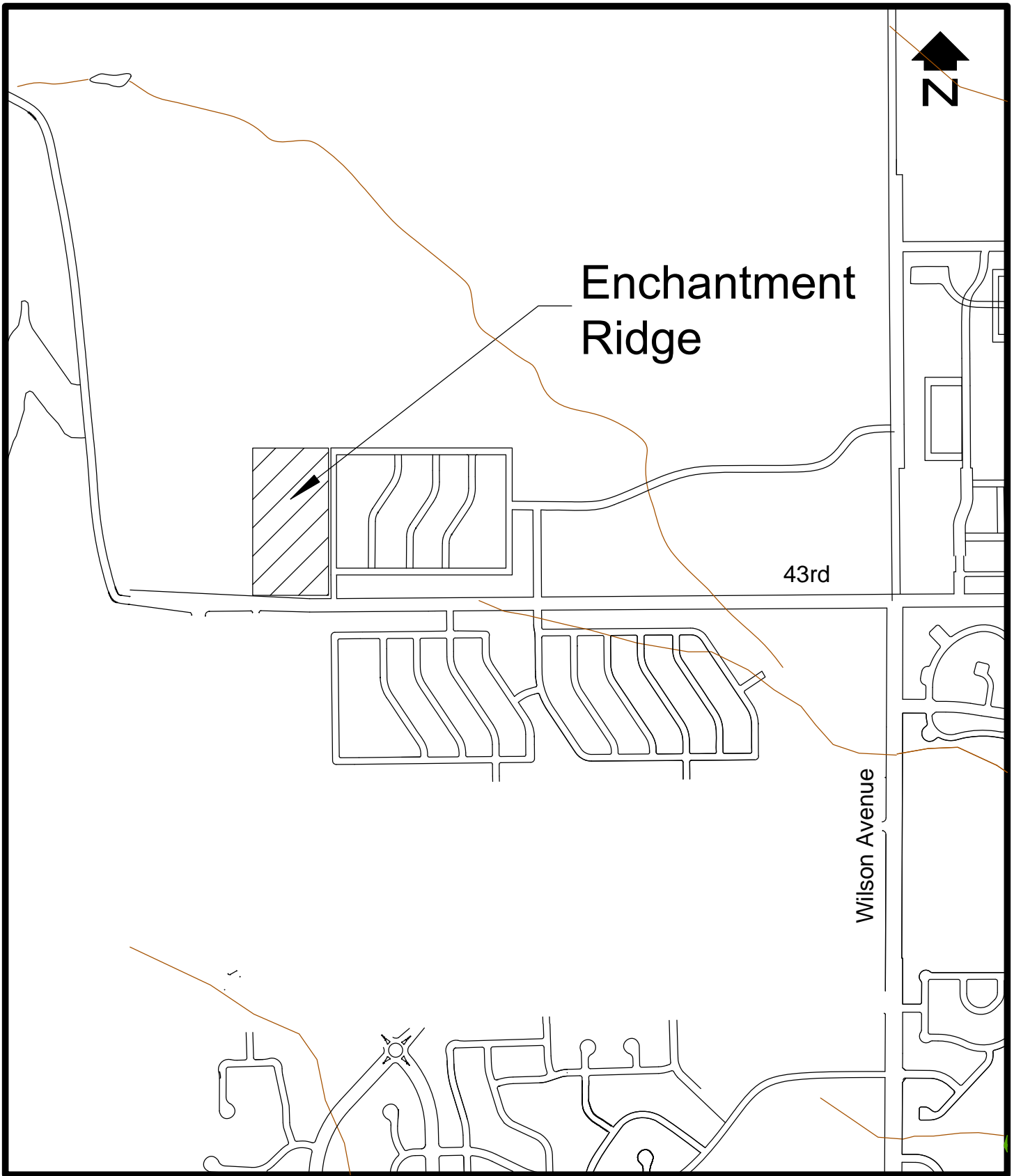
Figure 7 shows the short range (2017) total peak hour traffic at the 43<sup>rd</sup>/Fife intersection. These traffic forecasts were derived by adding the traffic in Figures 5 and 6. Table 3 shows the short range (2017) total peak hour operation at the 43<sup>rd</sup>/Fife intersection. Calculation forms are provided in Appendix C. The 43<sup>rd</sup>/Fife intersection will operate acceptably.

Table 4 shows the existing and short range (2017) link volumes for key street segments. Table 4 also shows the ACF volume thresholds for key street segments and whether the segments meet the Adequate Community Facilities Ordinance. The threshold volumes shown were calculated for this study. Calculations for the ACF threshold volumes are provided in Appendix D. Table 4 indicates that all links meet the requirements of the Adequate Community Facilities Ordinance.

The recommended short range geometry is shown in Figure 8.

There is a sidewalk along the both sides of 43<sup>rd</sup> Street, east of Fife Court. There are bike lanes along 43<sup>rd</sup> Street. The sidewalk system will be built within the Enchantment Ridge parcel.

The Enchantment Ridge will have minimal impact on traffic on the key streets. The 43<sup>rd</sup>/Fife intersection will operate acceptably. There is adequate intersection sight distance at the 43<sup>rd</sup>/Fife intersection. All intersections and links will meet the Loveland Adequate Community Facilities Ordinance criteria.

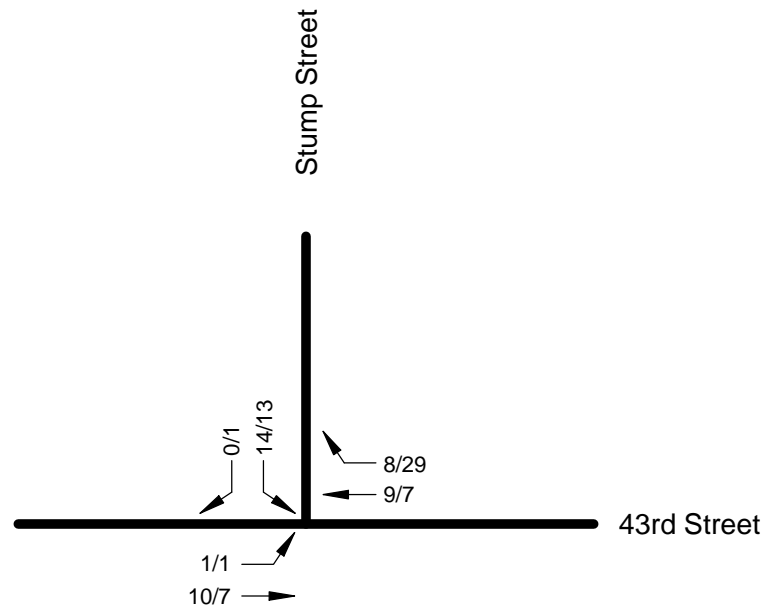
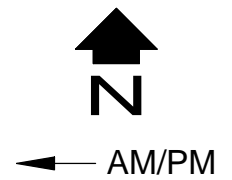


SITE LOCATION

Figure 1



PC ATTACHMENT 4



RECENT PEAK HOUR TRAFFIC

Figure 2

TABLE 1 Current Peak Hour Operation			
Intersection	Movement	Level of Service	
		AM	PM
43 <sup>rd</sup> /Stump (stop sign)(For Information Only)	EB LT	A	A
	SB LT/RT	A	A
	OVERALL	A	A

TABLE 2 Trip Generation												
Code	Use	Size	AWDTE		AM Peak Hour				PM Peak Hour			
			Rate	Trips	Rate	In	Rate	Out	Rate	In	Rate	Out
210	Single Family	36 D.U.	Eq.	410	Eq.	9	Eq.	26	Eq.	26	Eq.	16

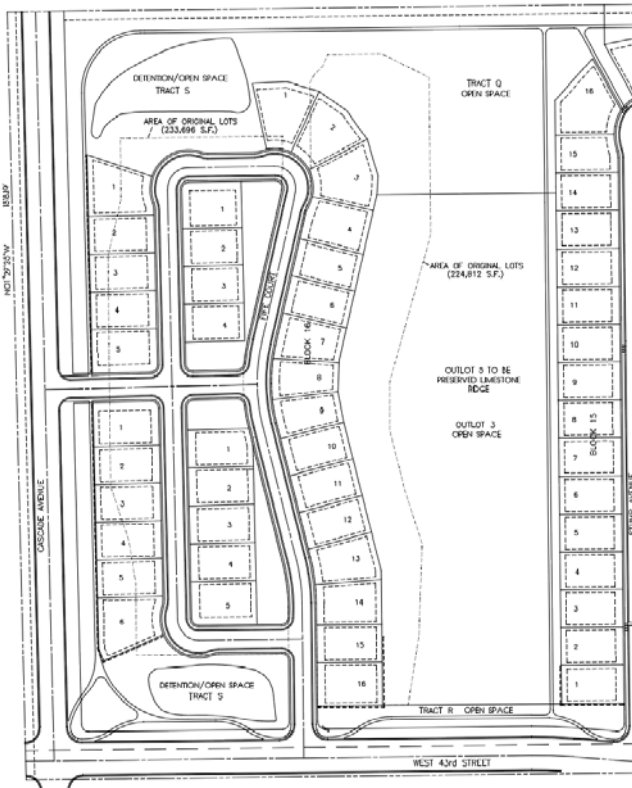
TABLE 3 Short Range (2017) Total Peak Hour Operation			
Intersection	Movement	Level of Service	
		AM	PM
43 <sup>rd</sup> /Fife (stop sign)	EB LT	A	A
	SB LT/RT	A	A
	OVERALL	A	A





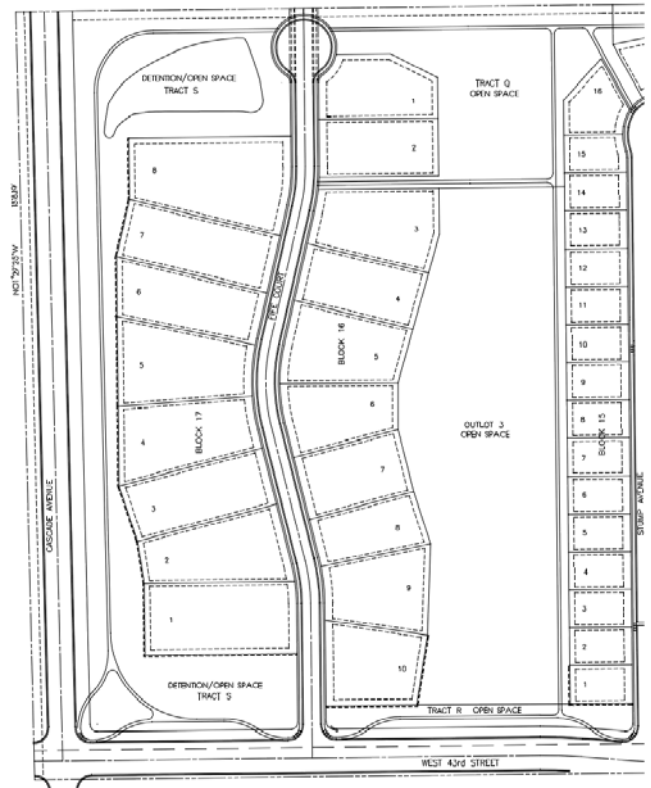
## NEW LAYOUT

ORIGINAL LOTS: 458,508 S.F.  
 NEW LOTS: 287,559 S.F.  
 NEW STREET: 72,063 S.F.  
 NEW OPEN SPACE: 98,886 S.F.  
 (2.27 acres)



## ORIGINAL LAYOUT

ORIGINAL LOTS: 458,508 S.F.



## SITE PLAN

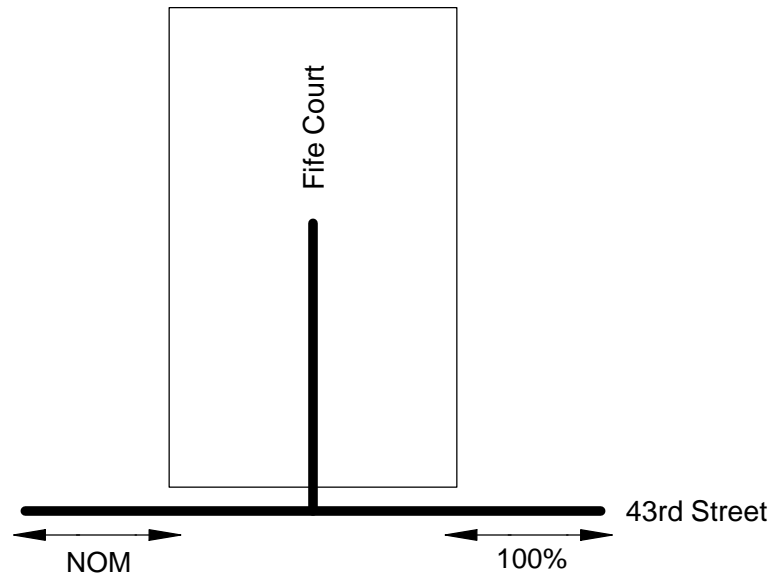


Figure 3

Enchantment Ridge TIS, June 2014

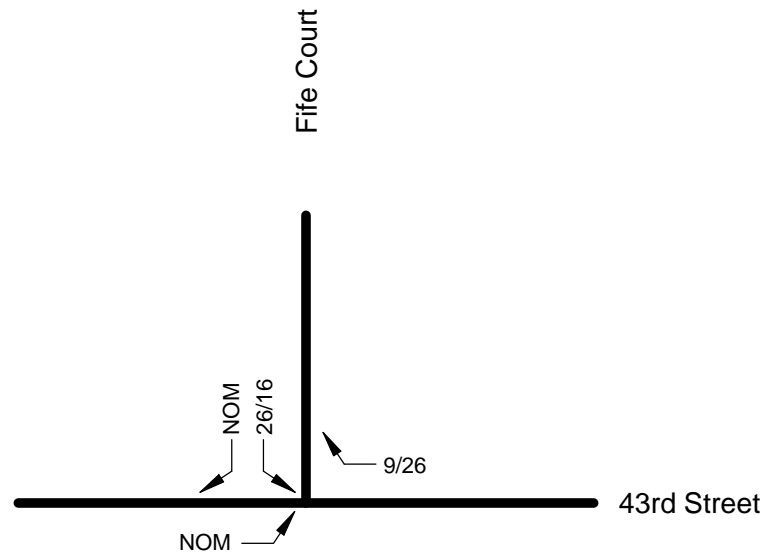
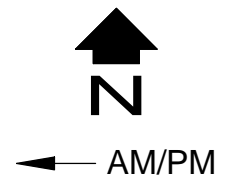
# PC ATTACHMENT 4





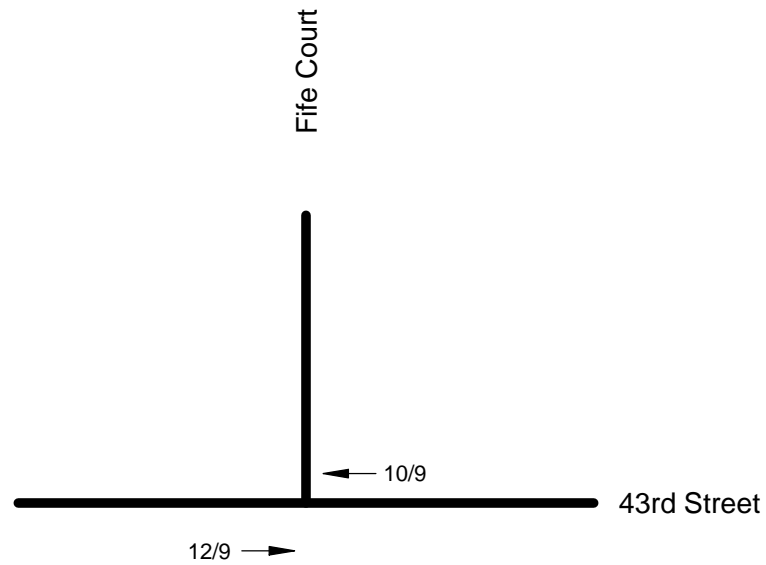
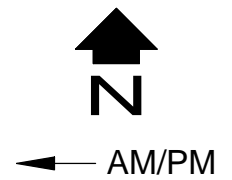
TRIP DISTRIBUTION

Figure 4



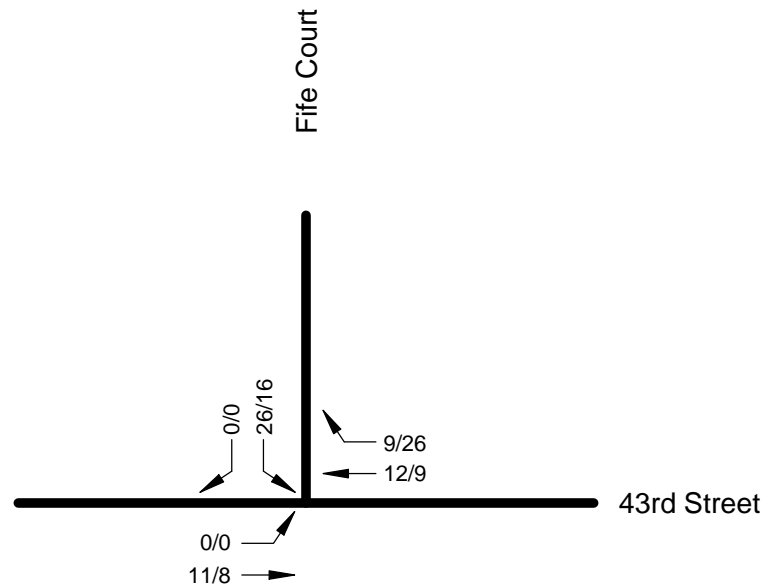
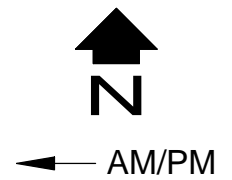
SITE GENERATED  
PEAK HOUR TRAFFIC

Figure 5



SHORT RANGE (2017) BACKGROUND  
PEAK HOUR TRAFFIC

Figure 6



SHORT RANGE (2017) TOTAL  
PEAK HOUR TRAFFIC

Figure 7

TABLE 4 Street Traffic Volume Summary Table								
Street Segment	Direction	Existing Traffic Volume (AM/PM)	Date Existing Volume Taken	Regional Growth and Traffic from Build-out of Other Proposed Development* (AM/PM)	Site Generated Traffic (2017) (AM/PM)	Total Traffic (2017) (AM/PM)	ACF Traffic Threshold	ACF Compliance (AM/PM)
1 43 <sup>rd</sup> Street west of Fife Court	EB	11/8	5/14	12/9	0/0	12/9	880	Y/Y
	WB	9/8	5/14	10/9	0/0	10/9	880	Y/Y
2 43 <sup>rd</sup> Street east of Fife Court	EB	11/8	5/14	12/9	26/16	38/25	880	Y/Y
	WB	9/8	5/14	10/9	9/26	19/35	880	Y/Y
3 Fife Court north of 43 <sup>rd</sup> Street	NB	0/0	5/14	0/0	9/26	9/26	60	Y/Y
	SB	0/0	5/14	0/0	26/16	26/16	60	Y/Y
* Approved developments, not yet built:								
* Proposed developments, not yet approved:								
Notes/Comments								



# GIULIANO ADDITION LOVELAND, COLORADO GENERAL DEVELOPMENT PLAN

FEBRUARY 2000



ALLER LINGLE  
ARCHITECTS, P.C.

748 WHALERS WAY  
BLDG E, SUITE 200  
FORT COLLINS, CO 80525  
(970) 223-1820

OWNER/DEVELOPER:  
Giuliano & Father  
Construction, Inc.  
308 Commerce Drive  
Ft. Collins, CO 80524

CIVIL ENGINEER:  
Stewart & Associates  
103 S. Meldrum  
Ft. Collins, CO 80521

## OWNER'S CERTIFICATION

KNOW ALL MEN BY THESE PRESENTS THAT:

Giuliano & Father Construction Co., Inc.  
being all the lawful record owners of the property shown on this  
General Development Plan, except any existing public streets,  
roads, or highways, do hereby certify that I/we accept the conditions  
and restrictions set forth on said plan and in the conditions of  
approval by the City of Loveland, dated 7.26.00, and that  
I/we consent to the recording of any information pertaining thereto.

Owner

Owner

STATE OF COLORADO )  
) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this 6 day  
of December, 2000, by John G. Giuliano

Witness by hand and official seal.

My commission expires: 7/14/2004  
Becky Warner Notary Public  
Address: 343 W. Drake Rd Suite 200, Fort Collins, CO 80526

## SIGNATURE BLOCKS

Approved this 15<sup>th</sup> day of January, 2001 by the  
Current Planning Manager of the City of Loveland, Colorado.

Approved this 24<sup>th</sup> day of January, 2001 by the  
City Engineer of the City of Loveland, Colorado.

Approved this 24<sup>th</sup> day of May, 2001 by the  
City Attorney of the City of Loveland, Colorado.

Approved this 5 day of May, 2001 by the  
City Planning Commission of the City of Loveland, Colorado.

Approved this 5 day of September, 2002 by the  
City Council of the City of Loveland, Colorado.

Theresa J. Williams Mayor  
Danica Vicenti Attest

KNOW ALL MEN BY THESE PRESENTS THAT:

Security Title Guarantee Company as trustee for the benefit of Wilson-  
South, Ltd.  
being all the lawful record owners of the property shown on this  
General Development Plan, except any existing public streets,  
roads, or highways, do hereby certify that I/we accept the conditions  
and restrictions set forth on said plan and in the conditions of  
approval by the City of Loveland, dated 7.26.00, and that  
I/we consent to the recording of any information pertaining thereto.

Owner

Owner

STATE OF COLORADO )  
) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this 5<sup>th</sup> day  
of December, 2000, by Jeffrey S. Wolff as Vice President of Security  
Title Guarantee Company as trustee for the benefit of Wilson-South, Ltd.

My commission expires: 11/10/02  
Becky Warner Notary Public  
Address: 343 W. Drake Rd Suite 200, Fort Collins, CO 80526

## CONDITIONS OF APPROVAL

Conditions marked with \*\* are conditions of annexation and will be included in  
an annexation agreement for consideration by the City Council. Conditions  
without the \*\* are conditions proposed for the GDP and will not be placed in  
the annexation agreement.

### ENGINEERING CONDITIONS

\*\*1. 43rd Street - Right-of-way and Improvements. If determined necessary  
by the City Engineer, the developer shall acquire and dedicate to the City,  
at no cost to the City, sufficient right-of-way along the southern portion  
of the westernmost 900 feet of the proposed addition to construct all  
required standard improvements for 43rd Street along the entire southern  
boundary of the addition. This condition shall be effective upon submittal  
of the appropriate PDP application, as determined by the City Engineer.

\*\*2. Cascade Avenue - Right-of-way and Improvements. If determined  
necessary by the City Engineer, the developer shall acquire and dedicate to  
the City, at no cost to the City, sufficient right-of-way along the western  
boundary of the proposed addition to construct all required standard  
improvements for Cascade Avenue. This condition shall be effective upon  
submittal of the appropriate PDP application, as determined by the City  
Engineer.

\*\*3. Wilson Avenue - Right-of-way and Improvements. The developer shall  
acquire and dedicate to the City, at no cost to the City, sufficient  
right-of-way along the eastern boundary of the proposed addition to  
construct all required standard improvements for Wilson Avenue. This  
condition shall be effective upon submittal of the first PDP application.

\*\*4. Transportation Master Plan. Future development applications within this  
project shall fully comply with any updates to the City of Loveland  
Transportation Master Plan adopted by the City Council for any portion of  
this property.

\*\*5. Unless constructed by others, the developer shall design and construct a  
raised median for Wilson Avenue extending from the intersection of 43rd  
Street to 200 feet south of the intersection of Glen Isle Drive. The timing  
of this improvement shall be based on which PDP application is shown to  
cause the need for mitigation, based on level-of-service standards as  
applied to the intersection of Wilson Avenue and Glen Isle Drive. Other  
mitigation measures may be substituted for this improvement if accepted  
by the City Engineer.

\*\*6. At the time of each PDP application, a Traffic Impact Study shall be  
submitted which includes a trip distribution based on the gravity model or  
an alternative distribution method acceptable to the City Engineer.

\*\*7. Notwithstanding conceptual information presented in the GDP, access  
locations and operations shall be determined at the time of application for  
PDP approval.

### PARKS AND RECREATION

\*\*8. Within the annexation parcel, the applicant shall dedicate to the City of  
Loveland a 25-foot wide tract of land to be used for a recreation trail. The  
parcel shall provide a connection from a location near the northwest corner  
of the annexation parcel to a currently undetermined point on 43rd Street.  
The trail installation shall be the responsibility of the developer and shall be  
built to City of Loveland standards. The location of the trail shall be  
determined by the City after consultation with the developer at the time of  
the first PDP/Preliminary Plan within the annexation parcel. The location of  
the trail shall not unreasonably interfere with the developers preferred  
subdivision.

### WASTE/WASTEWATER/STORM DRAINAGE CONDITIONS

\*\*9. The Developer shall design and construct a 24" water line along the south  
property line, from the west boundary of Buck 2nd Subdivision, along West  
43rd Street, to the west boundary of Giuliano Addition then north along the  
west property line to the north boundary of this addition. The City may  
approve alternate alignments at the time of a Preliminary Development Plan  
submittal.

\*\*10. The Developer shall design and construct an extension of the existing 16"  
waterline in W. 43rd Street, from its existing terminus, to connect with the  
aforementioned 24" waterline along the south property line.

\*\*11. The Developer shall design and construct waterlines within the subject  
annexation that connect to the aforementioned 24" waterline along the west  
boundary and to the existing 16" waterline in W. 43rd Street and existing  
30" waterline in N. Wilson Avenue. Specific alignments, connection points  
and sizing requirements will be determined by the City at the time of a  
Preliminary Development Plan.

\*\*12. The Developer shall complete an Engineering Analysis of the surrounding and  
other applicable water system facilities in order to determine the need for  
additional water system improvements. The City will determine the content  
and specific parameters of the Analysis at the time of a Preliminary  
Development Plan submittal.

### POWER CONDITIONS

\*\*13. A 5% surcharge will be assessed on the sale of electricity to any  
development for a period of 10 years from the date of annexation. A  
note will be required on each final plat regarding this requirement as  
follows: "A surcharge of 5% will be added to all bills for the sale of  
electric power to additional services which came into the existence after  
January 31, 1987, within the territory herein annexed, which surcharge will  
expire ten years after effective date of this annexation."

### VESTING (ADDED BY CITY COUNCIL)

\*\*13a. Waiver of Damages. In the future, the Developer may be granted vested  
property rights associated with the approval of a site specific development  
plan within the property. In the event that such vested property rights are  
granted, and the City applies an initiated or referred measure to the property  
which would (a) change any term or condition of this Agreement, (b) impose  
a moratorium on development within the property, or otherwise materially  
delay the development of the property, or (c) limit the number of building or  
utility permits to which the Developer would otherwise be entitled, the  
Developer agrees to waive any right to damages against the City to which the  
Developer may otherwise be entitled under the Vested Rights Statute.

### CURRENT PLANNING

14. PDP applications within 100 feet of the base limestone ridge shall include the  
results of the Ball's Twinpod survey. If the plan is located on the site, an  
appropriate buffer as determined by the City shall be preserved as open  
space.

15. Except as specified in the GDP for single-story garages in areas SF-3 and  
SF-4, the standard for side yard building setbacks shall be as specified in  
the Municipal Code, which is one foot for every 3 feet of building height, or  
fraction thereof, but a minimum of 5 feet.

16. Lots with less than 65 feet of frontage on a public street right-of-way shall  
not be permitted a garage where garage doors for more than two cars are  
visible as part of the front building elevation.

17. On lots with less than 65 feet of frontage on a public street, garage doors  
that are visible as part of front building elevations shall be recessed behind  
either the front facade of the living portion of the dwelling or a covered  
porch by at least four (4) feet. Covered porches referred to in this  
condition shall measure at least eight (8) feet, measured across the front of  
the building, by six (6) feet.

18. Garage doors visible as part of front building elevations shall not comprise  
more than forty (40) percent of the ground floor street-facing linear building  
frontage. Alleys and corner lots are exempt from this condition.

19. Future PDP applications shall include useable open space that is easily  
accessible to residential lots throughout a development. Open space shall be  
thoroughly integrated into residential projects.

20. To address noise impacts on residential areas, PDP applications for  
development within zoning area C-1 shall include a noise study  
demonstrating compliance with City noise standards for residential areas.  
This study shall include analysis of loading, delivery, and operational noise  
sources. Potential restrictions on these operations including limitations on  
hours of operation may be imposed to achieve compliance with City  
standards.

21. The annexation map shall be revised to include the following statement in the  
Notes section: "This annexation is subject to an annexation agreement which  
has been recorded in the real property records of Larimer County."

22. The GDP narrative shall be revised to reflect that fences other than the  
neighborhood edge fence shall be a maximum of five feet in height and  
shall match the lot line fence shown on SD1.

23. In area MF-1 the maximum number of congregate care units shall not  
exceed 50 dwelling units.

24. Front facades of single-family dwellings shall include at least two of the  
following features: bay window, porch, or gabled or hipped roof element.

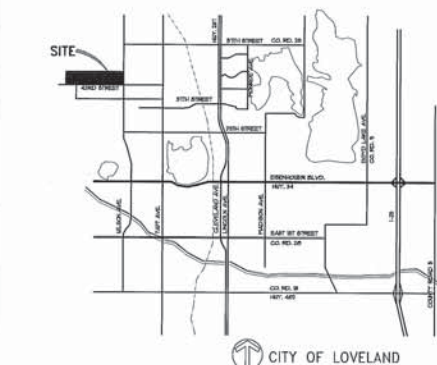
25. Notwithstanding the typical street cross-sections shown on SD-1, the  
street sections proposed with subsequent PDP applications shall comply with  
City street standards.

26. Minimum front yard building set backs for residential portions of  
single-family dwellings in areas SF-3 and SF-4 shall be varied and the  
minimum setbacks shall be 15 feet except for garage door which shall  
require a minimum setback of 20 feet.

27. Language in the GDP narrative indicating that in area SF-1 the minimum  
lot width may be 60 feet and corner lots must be at least 70 feet shall  
be deleted.

28. In areas SF-3 and SF-4, the maximum gross density shall be 5.5  
dwelling units per acre instead of 6.0.

## VICINITY MAP



## DRAWING INDEX

SD1 Site Plan, Data Table & Details  
SD2 Project Narrative

## LEGAL DESCRIPTION

A TRACT OF LAND SITUATE IN SECTION 33, TOWNSHIP 6 NORTH  
RANGE 69 WEST OF THE SIXTH P.M., LARIMER COUNTY, COLORADO  
WHICH CONSIDERING THE SOUTH LINE OF THE SAID SECTION 33 AS  
BEARING S89°20'42"W AND WITH ALL BEARINGS CONTAINED HEREIN  
RELATIVE THERETO IS CONTAINED WITHIN THE BOUNDARY LINES  
WHICH BEGIN AT A POINT ON SAID SOUTH LINE WHICH BEARS  
S89°20'42"W 30.00 FEET FROM THE SOUTHEAST CORNER OF SAID  
SECTION 33, AND RUN THENCE ALONG SAID SOUTH LINE  
S89°20'42"W 525.192 FEET TO THE SOUTHWEST CORNER OF SAID  
SECTION 33; THENCE ALONG THE WEST LINE OF THE SOUTH 1/2 OF  
THE SOUTH 1/2 OF SAID SECTION 33, N20°12'28"W 131.819 FEET TO THE  
SOUTH 1/2 CORNER ON THE WEST LINE OF SAID SECTION 33;  
THENCE ALONG THE NORTH LINE OF THE SOUTH 1/2 OF THE SOUTH 1/2  
OF SAID SECTION 33, N89°19'33"E 530.014 FEET TO THE SOUTH 1/8  
CORNER ON THE EAST LINE OF SAID SECTION 33; THENCE ALONG  
THE EAST LINE OF THE SOUTH 1/2 OF THE SOUTH 1/2 OF SAID SECTION  
33, S20°31'18"E 330.35 FEET; THENCE S89°20'42"W 30.00 FEET TO  
THE NORTHEAST CORNER OF PICABO HILLS ADDITION TO THE CITY  
OF LOVELAND, COLORADO; THENCE ALONG THE WEST LINE OF  
SAID PICABO HILLS ADDITION, AND THE WEST LINE OF THE  
MOUNTAIN VISTA ADDITION, S20°31'18"W 99.023 FEET TO THE POINT  
OF BEGINNING CONTAINING 59.6326 ACRES MORE OR LESS AND  
BEING SUBJECT TO ALL EASEMENTS AND RIGHTS-OF-WAY IN PLACE  
OR ARE NOW OF RECORD.





# GIULIANO ADDITION N. WILSON AVENUE AND W. 43RD STREET LOVELAND, COLORADO

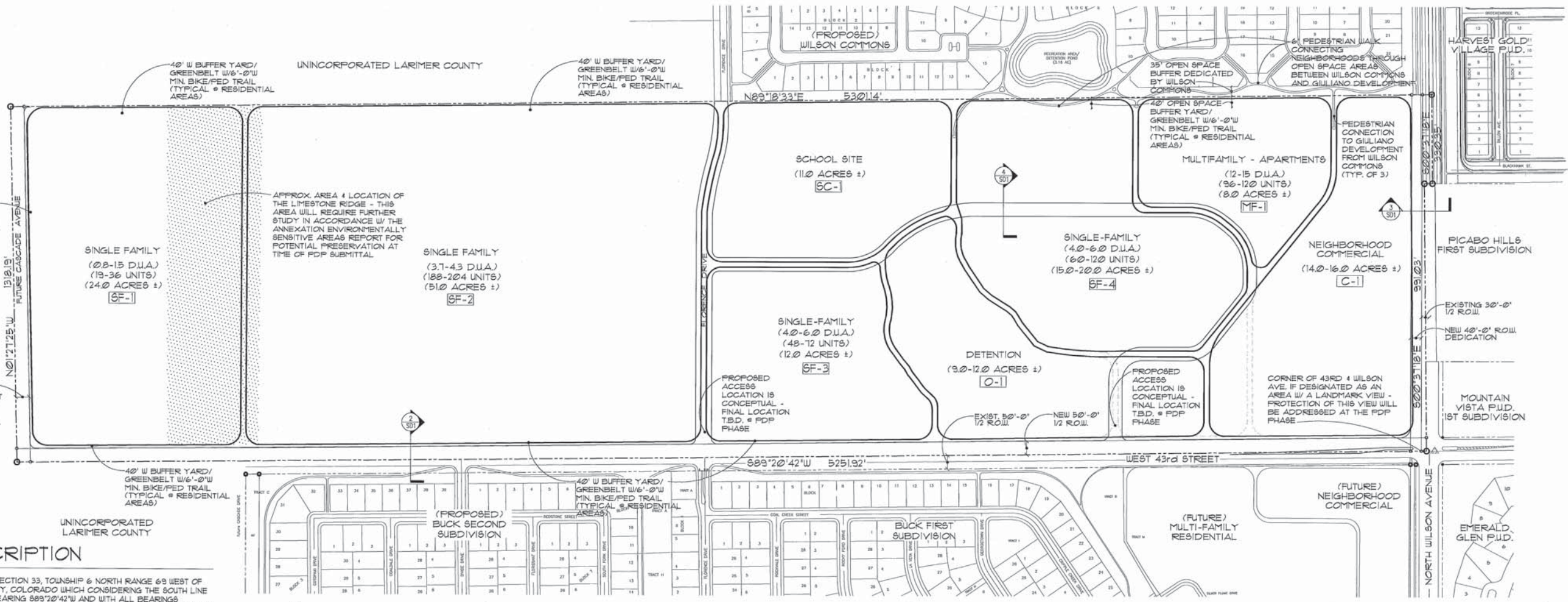


748 WHALERS WAY  
BLDG E, SUITE 200  
FORT COLLINS, CO 80525  
(970) 223-1820

4 DETAILS  
DATA TABLE  
SITE PLAN

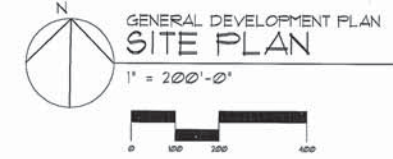
PROJECT 3845  
DATE 2-9-00  
DRAWN AA

SD1



## LEGAL DESCRIPTION

A TRACT OF LAND SITUATE IN SECTION 33, TOWNSHIP 6 NORTH RANGE 68 WEST OF THE SIXTH PM, LARIMER COUNTY, COLORADO WHICH CONSIDERING THE SOUTH LINE OF SAID SECTION 33 AS BEARING 88°20'42"W AND WITH ALL BEARINGS CONTAINED HEREIN RELATIVE THERETO IS CONTAINED WITHIN THE BOUNDARY LINES WHICH BEGIN AT A POINT ON SAID SOUTH LINE WHICH BEARS 88°20'42"W 30.00 FEET FROM THE SOUTHEAST CORNER OF SAID SECTION 33, AND RUN THENCE ALONG SAID SOUTH LINE, 88°20'42"W 5251.92 FEET TO THE SOUTHWEST CORNER OF SAID SECTION 33; THENCE ALONG THE WEST LINE OF THE SOUTH 1/2 OF THE SOUTH 1/2 OF SAID SECTION 33, N89°19'33"E 5301.14 FEET TO THE SOUTH 1/2 CORNER OF SAID SECTION 33; THENCE ALONG THE NORTH LINE OF THE SOUTH 1/2 OF THE SOUTH 1/2 OF SAID SECTION 33, 88°20'42"W 330.35 FEET; THENCE 88°20'42"W 30.00 FEET TO THE NORTHWEST CORNER OF PICABO HILLS ADDITION TO THE CITY OF LOVELAND, COLORADO; THENCE ALONG THE WEST LINE OF SAID PICABO HILLS ADDITION, AND THE WEST LINE OF THE MOUNTAIN VISTA ADDITION, 88°20'42"W 99.03 FEET TO THE POINT OF BEGINNING CONTAINING 59.6326 ACRES MORE OR LESS AND BEING SUBJECT TO ALL EASEMENTS AND RIGHTS-OF-WAY IN PLACE OR ARE NOW OF RECORD.

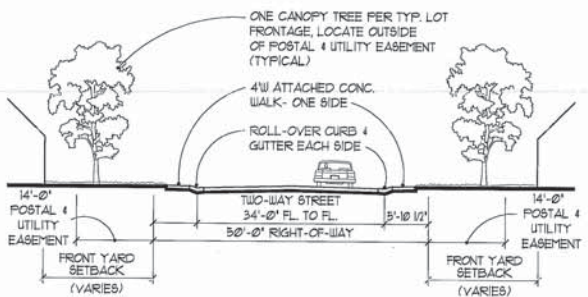


## GENERAL NOTES:

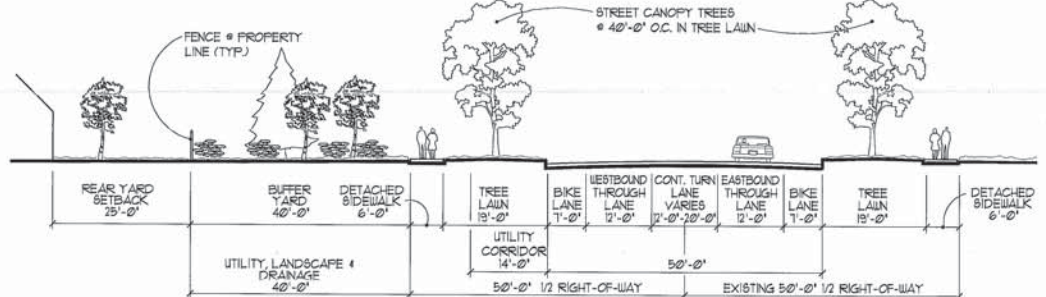
- LANDSCAPING NOT SHOWN ON GDP SITE PLAN. LANDSCAPING REVIEW WILL TAKE PLACE DURING THE PDP REVIEW PROCESS AND WILL BE BASED ON THE GDP NARRATIVE, THE CITY OF LOVELAND SITE DEVELOPMENT PERFORMANCE STANDARDS & GUIDELINES AND THE MAJOR ARTERIAL CORRIDOR PLAN.
- TREE LAWN AND BUFFERYARD WIDTHS MAY NEED TO BE INCREASED AT THE TIME OF PDP TO AVOID CONFLICTS BETWEEN UTILITIES DRAINAGE, AND LANDSCAPING.

## LAND USE DATA TABLE

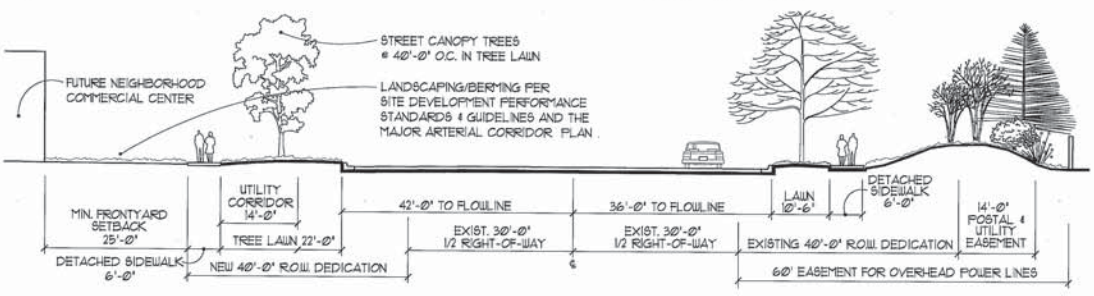
MARK	GROSS AREA	PROPOSED USE	PROPOSED GROSS DENSITY RANGE	PROPOSED MAX. BUILDABLE AREA	NO. OF UNITS	MAX. FLOOR AREA RATIO	% OF OPEN SPACE
SF-1	24.0 ACRES ±	SINGLE FAMILY DETACHED RESIDENTIAL	0.8 - 1.5 D.U.A.	N/A	18 - 36	N/A	15% MIN.
SF-2	51.0 ACRES ±	SINGLE FAMILY DETACHED RESIDENTIAL	3.1 - 4.3 D.U.A.	N/A	188 - 204	N/A	15% MIN.
SF-3	12.0 ACRES ±	SINGLE FAMILY DETACHED RESIDENTIAL	4.0 - 6.0 D.U.A.	N/A	48 - 72	N/A	15% MIN.
SF-4	15.0-20.0 ACRES ±	SINGLE FAMILY DETACHED RESIDENTIAL	4.0 - 6.0 D.U.A.	N/A	60 - 120	N/A	15% MIN.
SC-1	11.0 ACRES ±	FUTURE ELEMENTARY SCHOOL SITE	N/A	N/A	N/A	N/A	N/A
O-1	9.0-12.0 ACRES ±	REGIONAL STORMWATER DETENTION/OPEN SPACE	N/A	N/A	N/A	N/A	100%
MF-1	8.0 ACRES ±	MULTI-FAMILY RESIDENTIAL APARTMENTS	12.0 - 15.0 D.U.A.	N/A	96 - 120	N/A	25% MIN.
C-1	14.0-16.0 ACRES ±	NEIGHBORHOOD COMMERCIAL CENTER/ PROFESSIONAL OFFICES	N/A	150,000 SQFT.	N/A	0.25	20% MIN.
TOTAL	159.6 ACRES ±	(ROW DEDICATION = 10.6 ACRES ±)			411-567 Units		



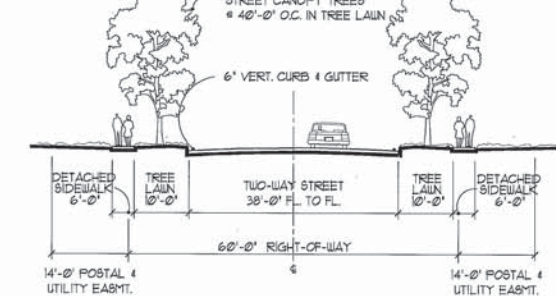
1  
SD  
1/16" = 1'-0"



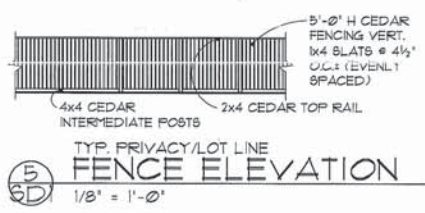
2  
SD  
1/16" = 1'-0"



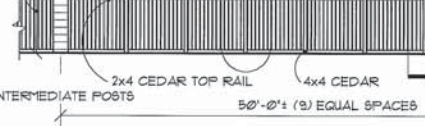
3  
SD  
1/16" = 1'-0"



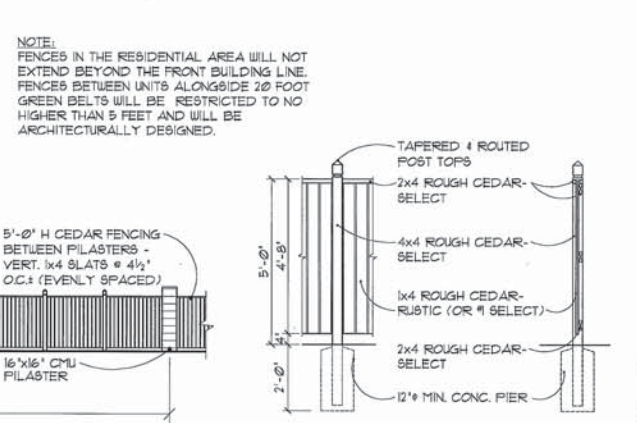
4  
SD  
1/16" = 1'-0"



5  
SD  
1/8" = 1'-0"



6  
SD  
1/8" = 1'-0"



7  
SD  
1/8" = 1'-0"

PC ATTACHMENT 5



# PROJECT NARRATIVE

## GENERAL DEVELOPMENT PLAN

### OVERALL DEVELOPMENT CONCEPT

Giuliano Addition P.U.D. is a 159.6-acre mixed-use planned unit development located in northwest Loveland. The site is bounded on the east by N. Wilson Avenue and the Picoabo Hills Addition by the south. The site is bounded on the north by undeveloped land in unincorporated Larimer County. The development concept is to provide a mix of affordably-priced, for-sale and rental housing opportunities, with supporting neighborhood commercial development, similar to the Sun Disk Village II and Buck Addition Master Plans in north Loveland. The development will contain single-family detached lots, complemented by multi-family dwelling units, and a variety of commercial uses in a neighborhood commercial center located at the intersection of N. Wilson Avenue and W. 43rd Street. An elementary school site has also been dedicated. Giuliano Addition P.U.D. will be initially developed by Giuliano and Father Construction of Fort Collins, the developer and builder of Sun Disk Village II and Buck Addition. However, other developers may be involved in subsequent phases of the project for multi-family residential or commercial development.

We are proposing a General Development Plan (G.D.P.) for the entire 159.6-acre. The single-family residential parcels designated SF-1, SF-2, SF-3 and SF-4 on the G.D.P. drawings will be the first development to occur on the site, and will be proposed as subsequent Preliminary Development Plan (P.D.P.) submittals, with lots arranged traditionally along tree lined streets.

Areas within the residential neighborhoods will be connected by an internal bicycle/pedestrian trail system, located within attractive greenbelts around the perimeter of the development and internal to the neighborhood connecting the private park/play areas. The single-family residential areas are broken up by the dedicated elementary school site, enhancement of the natural drainageways, and development of the new regional stormwater detention pond. A multi-family apartment development will be proposed as separate P.D.P. to serve the transitional use neighborhood. The neighborhood commercial center to the single-family residential neighborhood. The neighborhood commercial center will be proposed as a future P.D.P. to complement the neighborhood-based convenience centers proposed within the Picoabo Hills Addition, and the Buck Addition, and will be developed as residential growth dictates.

All uses of property within the Giuliano Addition P.U.D. shall be governed by Titles 16 and 18 of the City of Loveland Municipal Code, as amended, except as otherwise specifically provided for in this G.D.P. Allowable uses, densities, building sizes, scale and heights, architectural character, and general guidelines for materials, colors and details shall be the condition of approval of this G.D.P. and subsequent P.D.P. submittals. A development agreement will be drafted to control the Giuliano Addition P.U.D. between Giuliano and Father Construction (and any other developers who may become involved) and the City of Loveland.

### AFFORDABLE HOUSING ELIGIBILITY AND COMMITMENTS

Giuliano & Father Construction, one of Loveland's larger entry-level homebuilders, have constructed hundreds of homes over the past seven years. In maintaining this tradition, the development of Giuliano Addition P.U.D. will be an opportunity for Giuliano & Father Construction to reexamine, improve and refresh their approach to building affordable homes, as well as utilize the new strategies that City Council and staff are implementing to meet the need for affordable housing. The project meets the city's definition of an affordable housing project as 20% of the units, a total of 114-172 out of 400-526 units, will be sold to households with income at 70% or less than the Median Family Income (MFI) for Loveland, as determined by the U.S. Department of Housing and Urban Development.

Giuliano & Father Construction Inc., can construct and sell homes to affordable households through a combination of methods:

1) Creative Planning: By developing Giuliano Addition P.U.D. jointly with Buck Addition P.U.D., though they are separate projects, Giuliano & Father Construction is planning to develop a progressive community housing, encompassing both projects, which is affordable to the finest of custom homes. Although these two pieces of land were purchased at current market price and with no raw water, because of their size, they can be developed to maximize the use of the land. When completed, this project will be an example that with construction/efficiency and creative planning, a developer/builder can purchase land at competitive prices and still build affordable housing.

2) Fast-Track Review: The timely development and construction of Giuliano Addition P.U.D. will be dependent on the ongoing utilization of the City's "fast-track" development review process. Therein project momentum will rely heavily on open communication and flexibility on the part of the City. The project is currently in the review process. We foresee January 1, 2000 as our target date for a final plat, a final development plan and for final signed construction plans. The combined involvement of City staff, planning and engineering consultants, and the people to turn around staff comments and re-submittals will be crucial in meeting the target date.

3) Use Tax Credit: By meeting the City's Affordable Housing definition, Giuliano Addition P.U.D. is eligible to receive the use tax credit on the units that are sold to qualifying households with incomes at or below 70% of MFI. As the builder of the homes, Giuliano & Father Construction will use the tax credit to reduce the cost of the home. Additionally, Giuliano & Father Construction proposes to use the CHFA requirement to ensure that Giuliano Addition's affordable homes are sold to first time, entry-level buyers.

4) Development Standards Modifications: Giuliano & Father Construction has a unique proposal for using Giuliano Addition's eligibility for reduced landscape requirements. With the assumption that the home buyers of the larger homes are more likely to be financially able to upgrade their own landscaping, Giuliano & Father Construction proposes to use the 20% reduction in overall landscape requirements and the 30% reduction of the sizes of the trees and bushes in the parcels that are not classified as affordable housing, parcels designated SF-1 & SF-2 on the G.D.P. drawings. Where homes meet affordable criteria, parcels designated SF-3 & SF-4, overall landscaping will meet standard requirements.

As an established developer and builder of entry level housing there are varying incentives to building under the new Affordable Housing guidelines. For Giuliano & Father Construction, the goal is to build a community where people from all walks of life and income levels live together. Giuliano & Father Construction's commitment to build the affordable units of the project first will assure the citizens of Loveland that their need for affordable housing is being met.

### LAND USE

Giuliano Addition P.U.D. will include four groups of single-family residential areas. The parcel designated SF-1 on the G.D.P. drawings will include up to 36 single-family detached lots, with lot sizes ranging from 10,000 sq. ft. to 43,560 sq. ft., averaging 17,000-20,000 sq. ft. Gross density of this portion of the development will be 0.8-1.5 units/acre. Typical lots are 100'-150' wide by 150'-200' deep, with a minimum lot width of 60 feet and corner lots to be at least 70' wide. The parcel designated SF-2 will include up to 204 single-family detached lots, ranging from 5,500 sq. ft. to 10,000 sq. ft., averaging 7,000-7,500 sq. ft. Gross density of this portion of the development will be 3.7-4.3 units/acre. Typical lots are 65'-70' wide by 100'-110' deep, with a minimum lot width of 50 feet and corner lots to be at least 70' wide. Parcels SF-3 and SF-4 will include up to 172 single-family detached lots, ranging from 4,400 sq. ft. to 6,500 sq. ft., averaging 5,000-5,500 sq. ft. Gross density of this portion of the development will be 4.0-6.0 units/acre. Typical lots are 40'-45' wide by 50' deep, with a minimum lot width of 40 feet and corner lots to be at least 45' wide.

Building envelopes and setbacks are designed to give the community a friendly, pedestrian scale. Side setbacks will be 2'-0" from the back of the back of the sidewalk, allowing off-street parking to occur in the driveways in addition to on-street parking. The maximum building envelope coverage for individual lots will not exceed 50%. Front yard setbacks for the single-family residential lots will typically be staggered from 20'-0" to 28'-0" to the front edge of each street, with the setback to be determined with each P.D.P. proposal. Side yard setbacks shall be, one foot for each three feet of building height, but no less than 5'-0", 15'-0" for corner lots, and 3'-0" for attached garage structures in the parcels designated SF-3 and SF-4. Rear yard setbacks will be a minimum of 28'-0". Necessary setbacks for the multi-family may be placed within the individual lot as allowed by city standards. The edges of all single-family residential areas of the G.D.P. are buffered from adjacent development by minimum 40'-0" deep buffer/drainage/open space tracts. Open space from the buffer/drainage and the play areas account for a minimum of 15% of the single-family portion.

Allowable uses for the single-family residential tract are as follows:

Individual Private Lots	Single-family residences
Private Neighborhood Park/Play Areas	Accessory uses or structures as allowed by City of Loveland ordinances
Common Open Space/Greenbelts	Recreational uses
	Open space
	Playgrounds
	Recreational uses
	Open space

Development of the multi-family residential apartments, or other higher density attached housing products, will be proposed as a separate P.D.P. to be developed in accordance with the balance of the development. The minimum area of a lot in this parcel will be 8,000 sq. ft. plus 2,000 sq. ft. per dwelling unit in excess of four dwelling units. Minimum front yard setbacks will be 20'-0" from the front lot line. The minimum rear yard and side yard setbacks shall be one foot for each three feet of building height, but no less than 5'-0". On corner lots the minimum side yard setback shall be no less than 15'-0". Accessory structures may be placed within the lot as allowed by city standards. The edges of all multi-family residential areas of the G.D.P. are buffered from adjacent development by minimum 40'-0" deep buffer/drainage/open space tracts. Open space from the buffer/drainage account for a minimum of 25% of the multi-family portion.

Allowable uses for the multi-family residential tracts are as follows:

Multi-Family Residential Area	Multi-family residential apartments
	Swimming pool and other recreational uses
	Community building
	Open Space
	Congregate care or assisted living centers
	Nursing home facilities
Common Open Space/Greenbelts	Recreational uses
	Open space

The G.D.P. permits a variety of office, commercial, retail, and community uses within a neighborhood commercial center to complement and support the residential development of both the Giuliano Addition P.U.D. and the Buck Addition P.U.D. The minimum buildable area for these uses within the overall G.D.P. will be 150,000 sq. ft. Minimum yards will be 25'-0" from a street right-of-way or zoning district boundary line. Maximum building envelopes and other site development design standards will be proposed in the P.D.P. submittal for this portion of the development. A minimum of 20% of the neighborhood commercial center portion of the G.D.P. will be allocated to open space.

Allowable uses for the neighborhood commercial center are as follows:

Neighborhood Commercial Center	Commercial retail stores
	Professional offices
	Conventional restaurants, without drive-thru lanes
	Fast food or drive-in restaurants, with drive-thru lanes
	Bank or savings and loan offices, with drive-thru lanes
	Convenience stores with gas pumps and car wash facilities
	Health/athletic clubs
	Medical and dental clinics and offices
	Churches
	Small animal veterinary clinics and hospitals
	Bed and breakfast, or small hotel/motel under 20 guest rooms
	Grocery store
	Medical and dental clinics and offices
	Personal service shops
Common Open Space/Greenbelts	Recreational uses
	Open space

Excluded uses are as follows:

Neighborhood Commercial Center	Motor vehicle sales
	Motor vehicle repair and service
	Lumberyards and building materials retail stores
	Large animal veterinary clinics
	Manufacturing and light industrial
	Warehouses and outside storage yards
	Child care facilities
	Exceeding 20,000 sq. ft. retail stores, excepting 20,000 sq. ft. retail stores
	Massage parlors and adult-oriented theaters and bookstores

Giuliano Addition P.U.D. will include extensive common open space and landscaped amenities, including several small, private neighborhood parks/play areas and a 9-12 acre regional stormwater detention facility. This detention facility is intended to fulfill the on-site detention requirements of this development parcel, without the need for supplemental detention areas within each P.D.P. parcel.

Allowable uses for the regional detention facility are as follows:

Detention Areas	Recreational uses
	Open space
	Structural detention

### ZONING

The 159.6-acre site for Giuliano Addition P.U.D. is currently outside the city limits of the City of Loveland and zoned FA-Farming (County). Our proposal calls for annexation of the entire site and rezoning to P.D.P.-Planned Unit Development concurrent with the approval of this G.D.P. submittal.

### CONTIGUITY WITH ADJACENT DEVELOPMENT

Giuliano Addition P.U.D. meets the criteria for contiguity with adjacent development established in the 1994 Comprehensive Master Plan for the planned single-family residential and neighborhood commercial P.D.P.s. Existing development exists along the south boundary of the site, which meets the contiguity criteria contained in the Master Plan, specifically that at least 1/3 of N. Wilson Avenue to the west of the site is an existing development, that the adjacent portion of the Buck Addition P.U.D. is an approved Final Development Plan (F.D.P.) and that at least 10% of the lots have been built. The single-family residential P.D.P. (SF-1) appears to be outside of the contiguity requirements for development and construction of the initial single-family residential P.D.P.s are underway.

### COMPATIBILITY WITH ADJACENT DEVELOPMENT

The residential and commercial uses proposed in the Giuliano Addition P.U.D. are compatible and consistent with the current and projected development patterns for this area of Loveland. Architectural design guidelines established in this narrative and subsequent P.D.P. submittals will ensure the compatibility of the new residential and commercial structures with those in the surrounding neighborhood. The adjacent Emerald Glen and Picoabo Hills First Subdivision P.U.D.s include a variety of residential and neighborhood-based commercial uses, and the proposed Picoabo Hills Second Subdivision, and planned development to the north, including the new middle school for the school district. The development of the Giuliano Addition P.U.D. is adjacent to the Governor facility to the southeast of this development. The development of a large neighborhood employment center, which the City of Loveland encourages to reduce the length and frequency of vehicular trips throughout the city. The intersection of N. Wilson Avenue and W. 43rd Street is also identified as a major commercial/employment center on the Land Use Map, adopted as a part of the 1994 Comprehensive Master Plan, and with the construction of the middle school, and the proposed elementary school within this G.D.P., it is reasonable to assume that the adjacent undeveloped land to the north and west of the site will be developed with residential uses.

### VEHICULAR CIRCULATION

Ultimate improvements to full minor arterial standards have been designed for W. 43rd Street in conjunction with the phased P.D.P. development of the Buck Addition P.U.D., with the developers responsible for their proportionate share. The ultimate 50'-0" cross section for W. 43rd Street will be one through lane in each direction, a continuous center turn lane, plus bicycle lanes on each side. Detached 6'-0" wide sidewalks are planned for the exterior street frontages adjacent to the development. An additional 40'-0" right-of-way will be provided to the west side of N. Wilson Avenue to establish a full 70'-0" of one-half right-of-way. The other one-half right-of-way will be developed by the Picoabo Hills First Subdivision. The alignment and right-of-way dedication of Cascade Avenue, along the western edge of Giuliano Addition is conceptual. The actual location shall be determined at the time of P.D.P. submittal. Florence Drive is proposed to continue from Buck First Subdivision through Giuliano Addition and into Wilson Commons. Additional proposed access locations are conceptual only. Actual locations and operations shall be determined at the time of P.D.P. application.

The major boulevard entrances from W. 43rd Street will feature a two lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached sidewalks and tree lawns on each side. Interior vehicular circulation has been designed to provide convenient circulation through the neighborhood, reduce traffic speed, enhance the safety of children and provide a variety of attractive streetscapes. A series of local collector streets will connect the various mixed-use components of the neighborhood, connecting to a system of local residential streets throughout the single-family residential P.D.P.(s). The local street design will feature a two-lane street section with right and left turn lanes out of the development and raised, landscaped medians. The right-of-way will also include a 7'-0" wide sidewalk. The 4'-0" detached



# GIULIANO FIRST SUBDIVISION LOVELAND, COLORADO

## FINAL DEVELOPMENT PLAN MARCH 2002

### LEGAL DESCRIPTION

GIULIANO FIRST SUBDIVISION BEING A SUBDIVISION OF THE GIULIANO ADDITION, A SITUATE IN THE SOUTH 1/2 OF SECTION 33, TOWNSHIP 6 NORTH, RANGE 69 WEST OF THE SIXTH P.M. CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO.

APPROVAL OF THIS PLAN CREATES A VESTED PROPERTY RIGHT PURSUANT TO SECTION 24-68-103, C.R.S., AS AMENDED.

### OWNER'S CERTIFICATION

KNOW ALL MEN BY THESE PRESENTS THAT:

Being all the lawful record owners of the property shown on this Final Development Plan, except any existing public streets, roads, or highways do hereby certify that I/we accept the conditions and restrictions set forth on said plan and in the conditions of approval by the City of Loveland, dated 5-15-2003, and that I/we consent to the recordation of any information pertaining thereto.

Giuliano & Father Construction, Inc.  
(owner)

(John G. Giuliano, President)

STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this 15th day of May, 2003, by John G. Giuliano.

Witness by hand and official seal.

My commission expires 3-12-06

Shaleen D. Schlichter

Notary Public

STATE OF COLORADO

John T. Giuliano, Owner

STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this 15th day of May, 2003, by John T. Giuliano.

Witness by hand and official seal.

My commission expires 3-12-06

Shaleen D. Schlichter

Notary Public

STATE OF COLORADO

J & P Family, LLC

STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this 15th day of May, 2003, by John G. Giuliano.

Witness by hand and official seal.

My commission expires 3-12-06

Shaleen D. Schlichter

Notary Public

STATE OF COLORADO

John G. Giuliano, Member

STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this 15th day of May, 2003, by John G. Giuliano.

Witness by hand and official seal.

My commission expires 3-12-06

Shaleen D. Schlichter

Notary Public

STATE OF COLORADO

Giuliano Addition, LLC

STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this 15th day of May, 2003, by John G. Giuliano.

Witness by hand and official seal.

My commission expires 3-12-06

Shaleen D. Schlichter

Notary Public

STATE OF COLORADO

### OWNER/APPLICANT

Giuliano & Father Construction, Inc.  
John G. Giuliano  
Contact: John Giuliano  
308 Commerce Drive, Unit A  
Fort Collins, Colorado 80524  
(970)593-1212 phone/fax

### CIVIL ENGINEER

Stewart & Associates  
Contact: Jerry Robinson  
103 S. Meldrum  
Fort Collins, Colorado 80521  
(970)482-9331  
(970)482-9382 fax

### DRAWING INDEX

SD1	SITE PLAN (west of Florence Drive)
SD2	SITE PLAN (east of Florence Drive)
SD3	SITE PLAN DETAILS & TYPICAL STREET SECTIONS
SD4	TYPICAL SINGLE FAMILY RESIDENTIAL ELEVATIONS, FENCE DETAILS & MONUMENT DETAILS
SD5	PROJECT NARRATIVE
SD6	GDP & PDP CONDITIONS OF APPROVAL
LS1	LANDSCAPE NOTES, PLANT LIST & BUFFERYARD MAP
LS2	LANDSCAPE PLAN (west of Florence Drive)
LS3	LANDSCAPE PLAN (east of Florence Drive)
LS4	LANDSCAPE DETAILS
LS5	LANDSCAPE DETAILS

### OVERALL LAND USE SUMMARY

Existing Zoning	PUD, Planned Unit Development (Giuliano Addition PUD)
Total Project Area	159.6 acres (100%)
Total R.O.W. Area	31.9 acres (20%)
Net Area	127.7 acres (80%)
Open Space Area	35.49 acres (38%)
Total Number of Residential Lots	356

\* Areas to have future PDPs are not included in open space area calculations.

### SIGNATURE BLOCKS

Approved this 19th day of March, 2003 by the Current Planning Manager of the City of Loveland, Colorado.

Approved this 19th day of December, 2003 by the City Engineer of the City of Loveland, Colorado.

Approved this 23rd day of December, 2003 by the City Attorney of the City of Loveland, Colorado.

Approved this 4th day of December, 2003 by the City Planning Commission of the City of Loveland, Colorado.

Approved this 6th day of May, 2003 by the City Council of the City of Loveland, Colorado.

### LAND USE SUMMARY BY AREA

Area	Percent of Area	Percent of Development
------	-----------------	------------------------

#### Area O-1 - Regional Detention Pond/Open Space

Construction Phase	Phase 1	
Total Area	8.98 acres	(6%)
Open Space Area	8.98 acres	(100%)

#### Area SC-1 - School Site

Construction Phase	Phase 1	
Total Area	10.0 acres	(6%)
Open Space Area	Undetermined	

#### Area MF-1 & C-1 - Multi-family and Commercial

Construction Phase	Future PDP Application	
Total Area	22.9 acres	(14%)
Open Space Area	Undetermined	

#### Area SF-1 - Detached Single-family Homes

Construction Phase	Phase 6	
Total Number of Lots	18	
Total Area	29.7 acres	(19%)
Total Lot Area	10.5 acres	(35%)
Total R.O.W. Area	0.5 acres	(22%)
Open Space Area	12.7 acres	(43%)
Gross Density	0.60 DU/AC	
Net Density (less R.O.W.)	0.78 DU/AC	

Lot Sizes	
Minimum	17,121 sf.
Average	25,229 sf.
Maximum	39,053 sf.

#### Area SF-2 - Detached Single-family Homes

Construction Phase	Phases 3, 4 & 5	
Total Number of Lots	176	
Total Area	50.2 acres	(31%)
Total Lot Area	31.08 acres	(62%)
Total R.O.W. Area	12.24 acres	(24%)
Open Space Area	6.88 acres	(14%)
Gross Density	3.50 DU/AC	
Net Density (less R.O.W.)	4.63 DU/AC	

Lot Sizes	
Minimum	6,073 sf.
Average	7,093 sf.
Maximum	12,180 sf.

#### Area SF-3 & SF-4 - Detached Single-family Homes

Construction Phase	Phases 1 & 2	
Total Number of Lots	162	
Total Area	37.82 acres	(24%)
Total Lot Area	17.77 acres	(47%)
Total R.O.W. Area	13.16 acres	(35%)
Open Space Area	6.89 acres	(18%)
Gross Density	4.28 DU/AC	
Net Density (less R.O.W.)	6.57 DU/AC	

Lot Sizes	
Minimum	4,180 sf.
Average	4,778 sf.
Maximum	7,319 sf.

### SETBACK REQUIREMENTS

The following minimum setbacks shall apply to all residential development within this FDP. All setbacks shall be measured from the back of the sidewalk. All non-garage door setbacks are measured to the foundation wall.

#### Single-Family detached homes

Front: Staggered front yard setbacks will be required for all single family detached residential lots. (i.e., every house must be staggered a minimum 2 feet closer to, or further from, the right-of-way than the adjacent homes. See lot matrix.

Minimum of 20 feet to face of garage door.

Side: 1 foot for every 3-feet of building height, with a minimum of 5-feet.

Corner Side: 15 feet sideyard adjacent to street.

Minimum of 20 feet to face of garage door.

Rear: 25 feet

Accessory Structures

Front: 15 feet or the front setback of the primary structure, whichever is greater.

Side: 6 feet

Corner Side: 15 feet

Rear: 6 feet

Encroachments

Overhangs, fireplaces and cantilevers, including structural elements may encroach into required setback by no more than 2 feet in width and 6 feet in length.

### LOT MATRIX BY AREA

#### Area SF-1

20 foot min. front yard  
Block 1b, Lots 1, 4, 7, 10  
Block 17, Lots 1, 4, 7  
22 foot min. front yard  
Block 1b, Lots 2, 5, 8  
Block 17, Lots 2, 5, 8  
24 foot min. front yard  
Block 1b, Lots 3, 6, 9  
Block 17, Lots 3, 6

#### Area SF-2

20 foot min. front yard  
Block 9, Lots 1, 4, 7, 10, 13, 16, 19  
Block 10, Lots 1, 4, 7, 10, 13, 16, 19, 22, 25, 27  
Block 11, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 12, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 13, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 14, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 15, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 16, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 17, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 18, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 19, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 20, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 21, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 22, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 23, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 24, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 25, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 26, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 27, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 28, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 29, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 30, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 31, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 32, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 33, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 34, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 35, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 36, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 37, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 38, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 39, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 40, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 41, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 42, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 43, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 44, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 45, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 46, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 47, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 48, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 49, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 50, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 51, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 52, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 53, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 54, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 55, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 56, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 57, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 58, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 59, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 60, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 61, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 62, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 63, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 64, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 65, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 66, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 67, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 68, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 69, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 70, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 71, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 72, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 73, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 74, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 75, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 76, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 77, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 78, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 79, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 80, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 81, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 82, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 83, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 84, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 85, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 86, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 87, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 88, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 89, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 90, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 91, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 92, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 93, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 94, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 95, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 96, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 97, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 98, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 99, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 100, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27

#### Area SF-3

20 foot min. front yard  
Block 9, Lots 1, 4, 7, 10, 13, 16, 19  
Block 10, Lots 1, 4, 7, 10, 13, 16, 19, 22, 25, 27  
Block 11, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 12, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 13, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 14, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 15, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 16, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 17, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 18, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 19, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 20, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 21, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 22, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 23, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 24, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 25, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 26, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 27, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 28, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 29, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 30, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 31, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 32, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 33, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 34, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 35, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 36, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 37, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 38, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 39, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 40, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 41, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 42, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 43, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 44, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 45, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 46, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 47, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 48, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 49, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 50, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 51, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 52, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 53, Lots 1, 3, 6, 9, 12, 15, 18, 21, 24, 27  
Block 54, Lots 1, 3

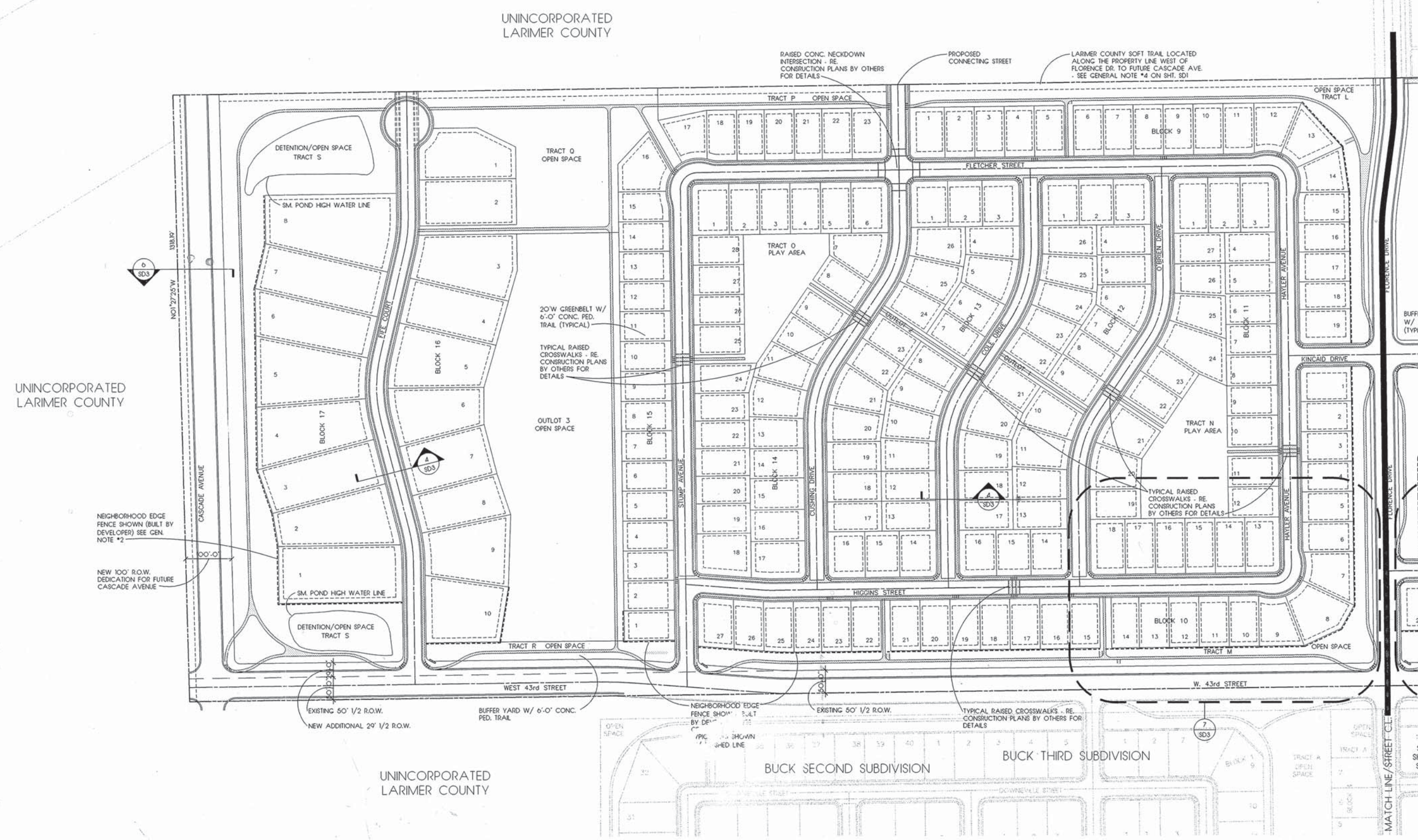


GIULIANO FIRST SUBDIVISION  
FINAL DEVELOPMENT PLAN  
LOVELAND, COLORADO

issue date  
3/1/02

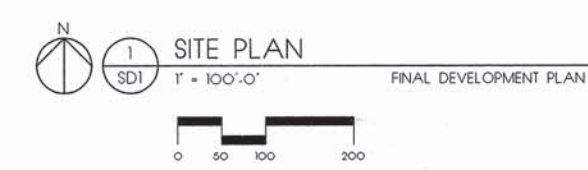
REVISIONS	
1	5/1/02
2	11/1/02
3	1/15/03

sheet title  
SITE PLAN  
sheet No.

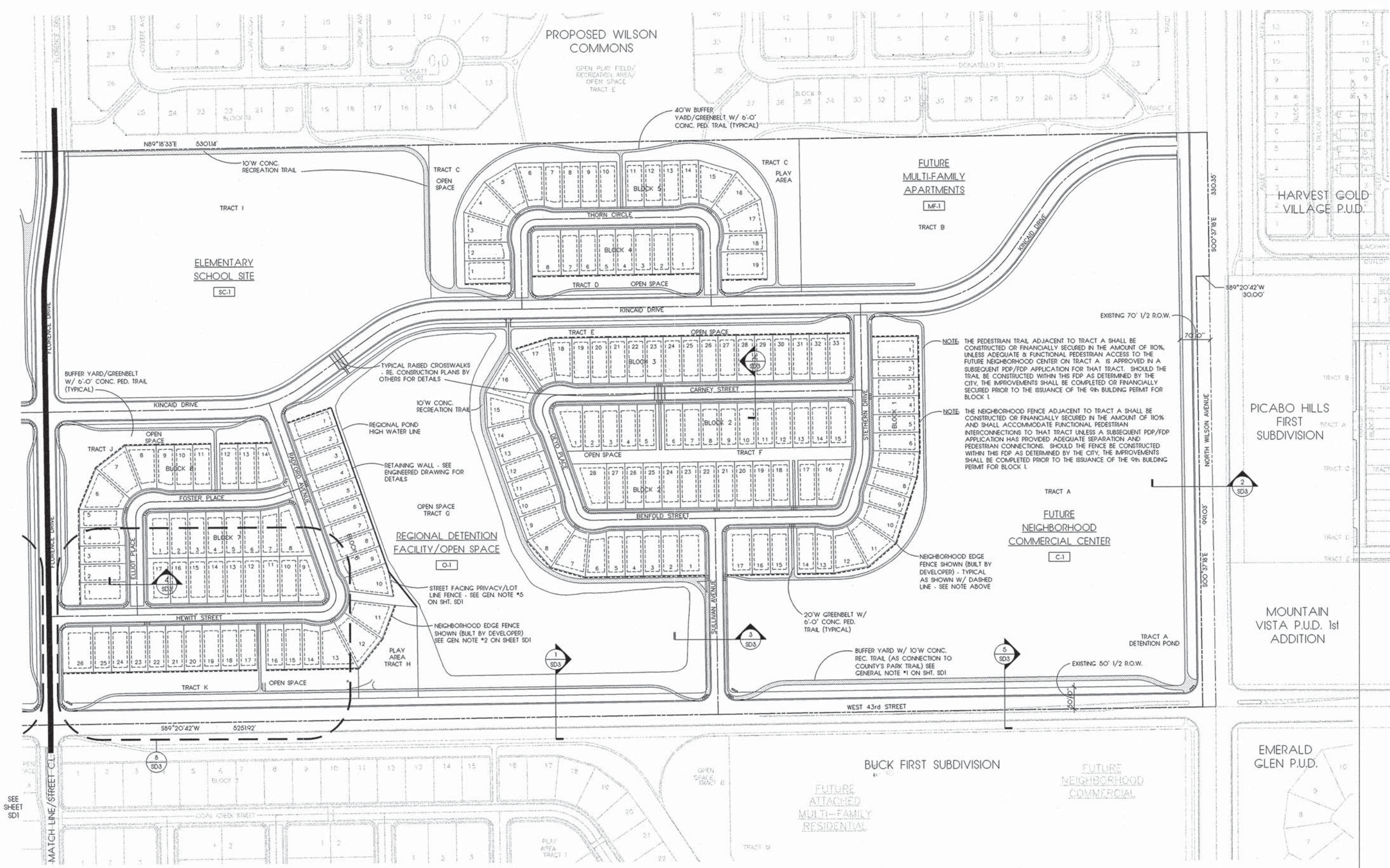


GENERAL NOTES

- THE RECREATION TRAIL AS SHOWN FROM FLORENCE DRIVE TO N. WILSON AVENUE IS TO BE CONSTRUCTED BY THE DEVELOPER AND IS TO BE MAINTAINED BY THE HOA. UNTIL THE CITY DETERMINES AND CONSTRUCTS THE FINAL CONNECTION/ALIGNMENT FROM THE WEST. THE COUNTY TRAIL WEST OF FLORENCE DRIVE IS TO BE MAINTAINED BY THE HOA. UNTIL THE COUNTY DETERMINES AND CONSTRUCTS THE FINAL CONNECTION/ALIGNMENT FROM THE WEST.
- THE LOCATIONS OF THE NEIGHBORHOOD EDGE FENCES ARE SHOWN GRAPHICALLY ON THE SITE PLAN. THE PRIVACY/LOT LINE FENCE (HOMEOWNER'S RESPONSIBILITY/OPTION) IS NOT SHOWN. SEE SHEET SD4 FOR FENCE DETAILS.
- IT IS THE DESIGN INTENT, THAT THE EXISTING SLOPE OF THE LIMESTONE RIDGE BE INCORPORATED INTO EACH INDIVIDUAL LOT OF BLOCK 10. EXISTING SLOPES IN EXCESS OF 20% ARE TO BE UNDISTURBED.
- 15'-0" WIDE PUBLIC TRAIL EASEMENT DEDICATED AND CONVEYED TO LARIMER COUNTY FOR BIKE, EQUESTRIAN AND PEDESTRIAN RECREATIONAL PURPOSES - SEE PLAT FOR RECEPTION NUMBER.
- DUE TO THE CONSTRUCTION OF THE RETAINING WALL IN THE REGIONAL POND THE DEVELOPER SHALL CONSTRUCT THE TYPICAL STREET FACING PRIVACY/LOT LINE FENCE ON TOP OF THE RETAINING WALL AND EXTENDING AROUND LOT 10, BLOCK 6 AS SHOWN ON THE PLAN - SEE SHT. SD4 FOR FENCE DETAILS.







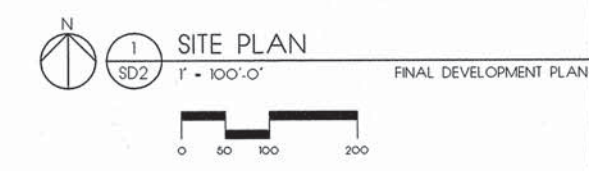
GIULIANO & FATHER  
CONSTRUCTION CO., INC.

GIULIANO FIRST SUBDIVISION  
FINAL DEVELOPMENT PLAN  
LOVELAND, COLORADO

issue date  
3/1/02

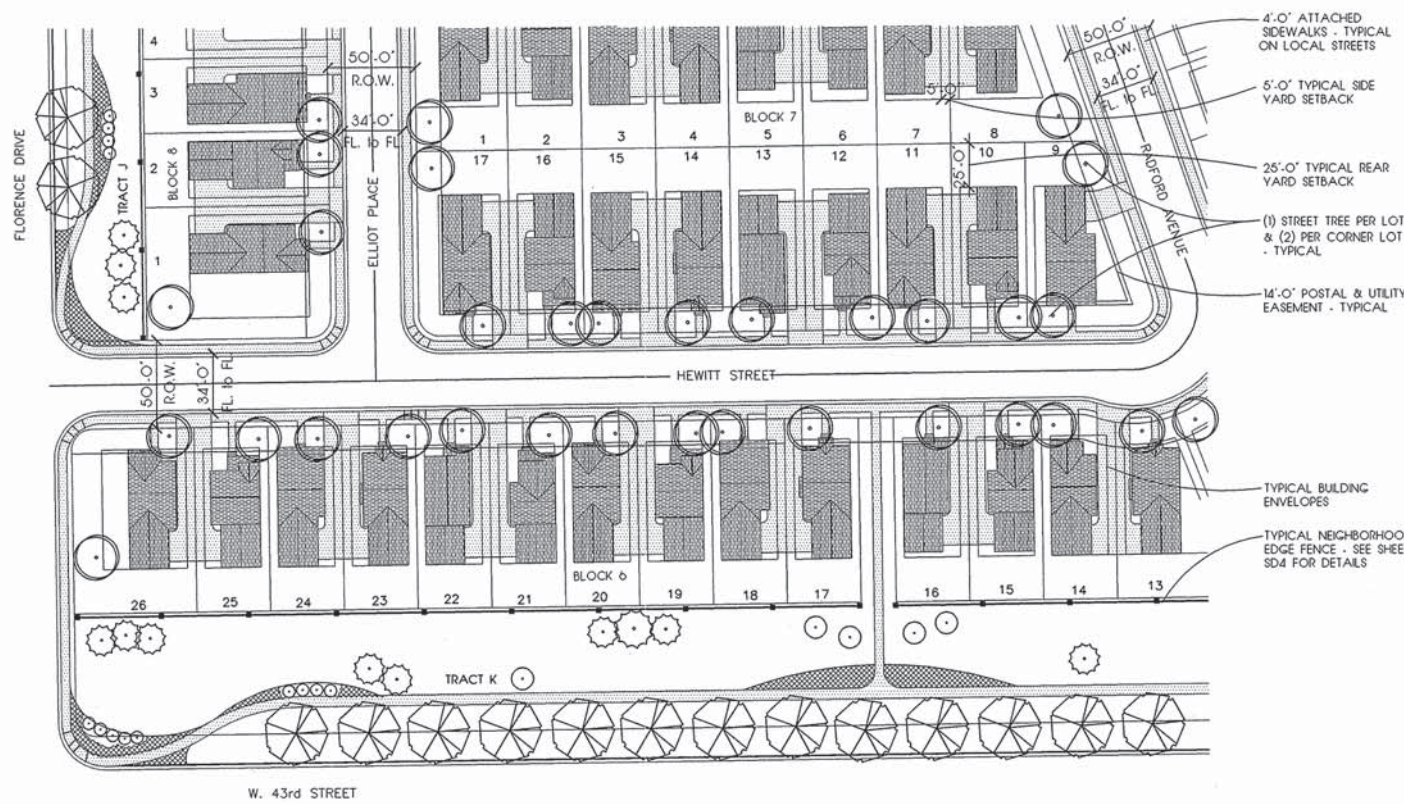
REVISIONS	
1	5/1/02
2	11/1/02
3	1/15/03

sheet title  
SITE PLAN  
SD2  
sheet no.

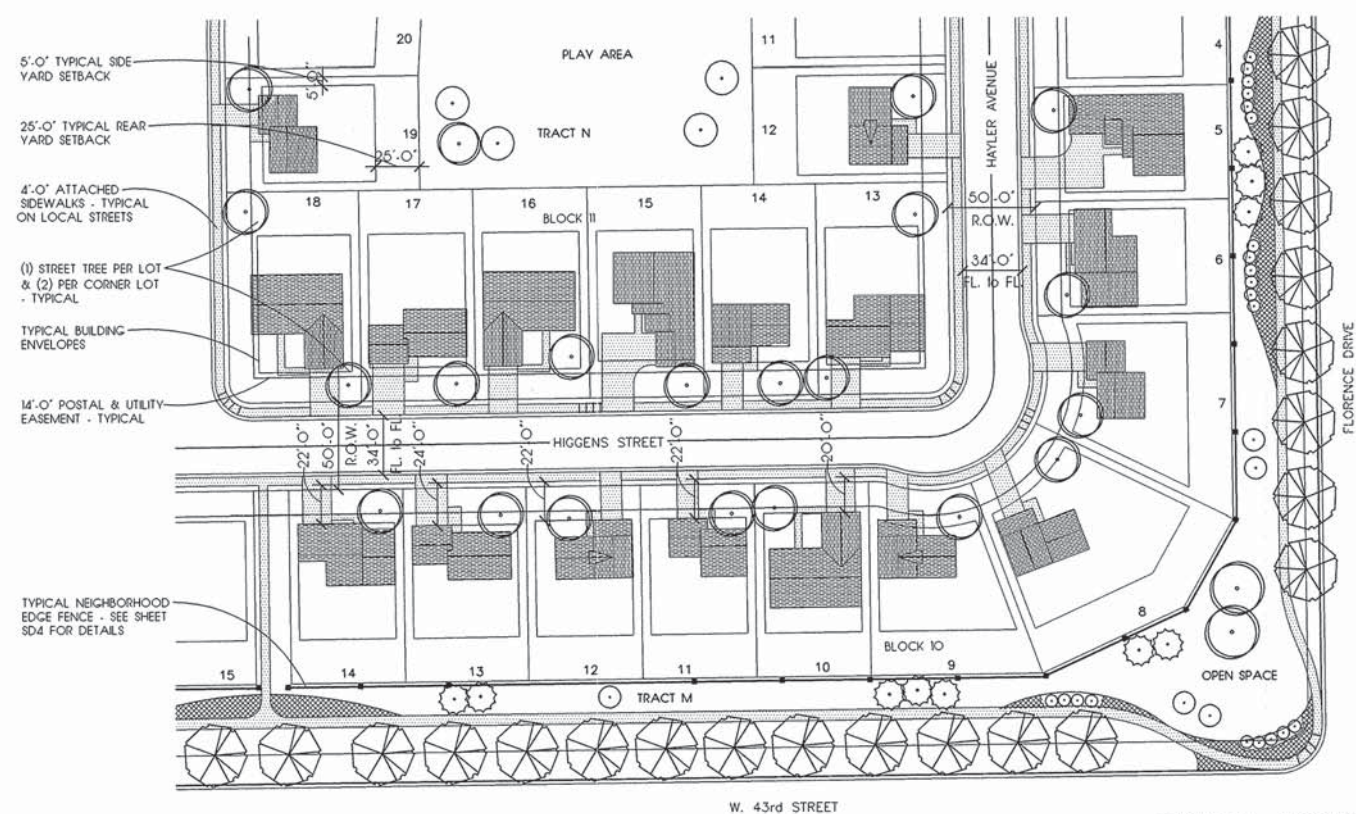


PC ATTACHMENT 6





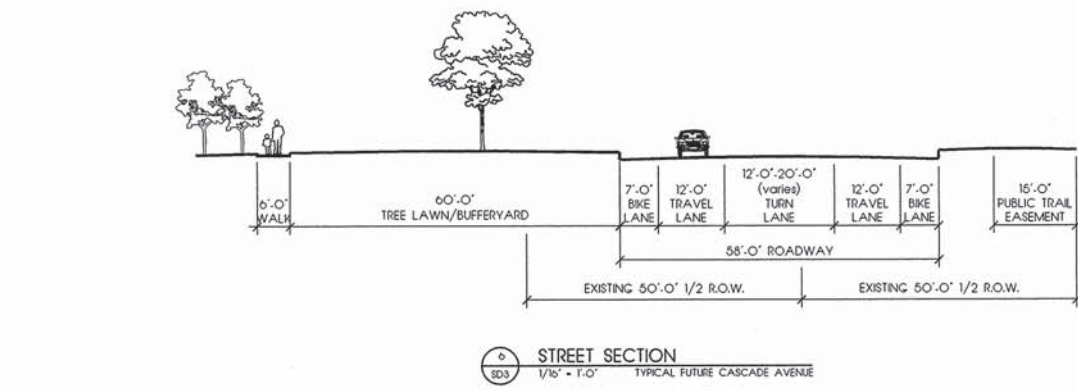
8 SITE PLAN DETAIL  
1" = 50'-0"



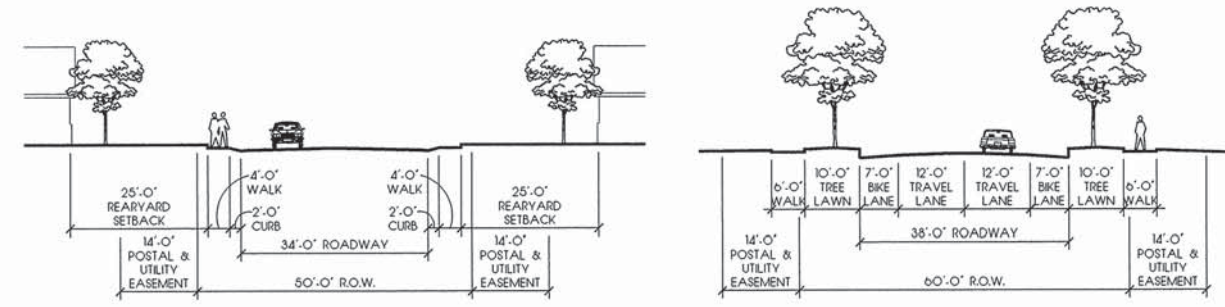
7 SITE PLAN DETAIL  
1" = 50'-0"

GENERAL NOTES:

1. THE ROOF PLANS SHOWN ON THE PARTIAL SITE PLANS ARE TO SHOW THE GENERAL INTENT ONLY.
2. FENCES ON LOT LINES, ALONGSIDE GREEN BELT CONNECTIONS, PARKS AND PARK WALKWAYS WILL BE RESTRICTED TO NO HIGHER THAN 5 FEET AND WILL BE ARCHITECTURALLY DESIGNED. SEE DETAILS ON SHEET SD4.
3. ALL DRIVEWAYS ON CORNER LOTS MUST MAINTAIN A MINIMUM SEPARATION OF AT LEAST 50 FEET FROM THE GUTTER FLOW LINE OF THE NEAREST STREET INTERSECTION.
4. FOUR FOOT ATTACHED SIDEWALKS ON THE LOCAL STREETS WERE SPECIFICALLY APPROVED WITH THE GDP.

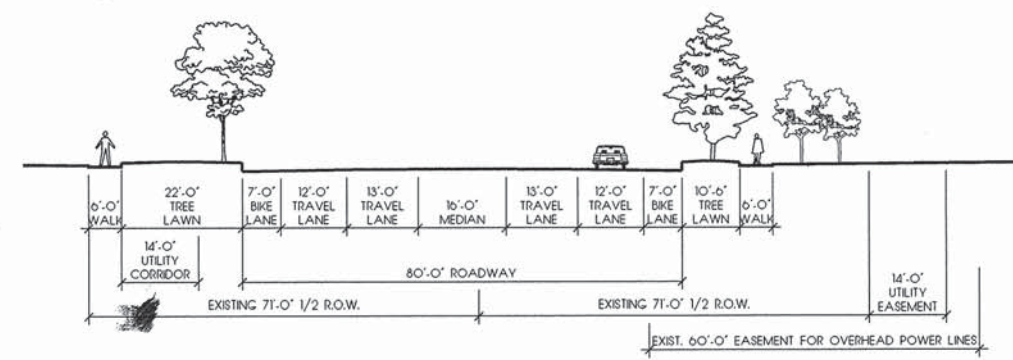


STREET SECTION  
1/16" = 1'-0"

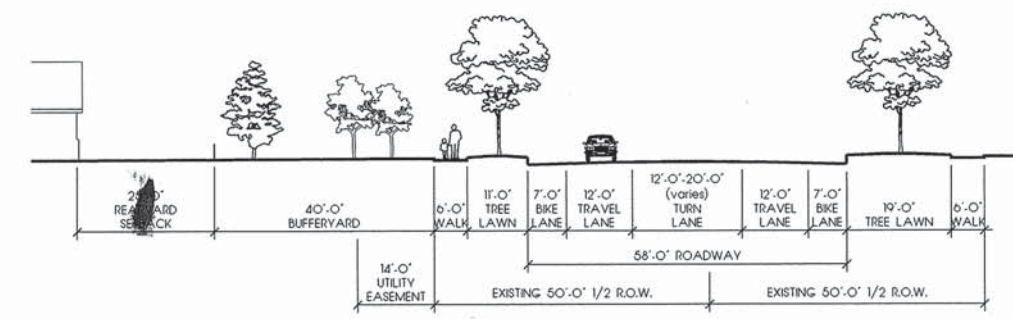


4 STREET SECTION  
1/16" = 1'-0"

3 STREET SECTION  
1/16" = 1'-0"



2 STREET SECTION  
1/16" = 1'-0"



1 STREET SECTION  
1/16" = 1'-0"

issue date
3/1/02

REVISIONS
1 5/1/02
2 11/1/02
3 1/15/03

sheet title
SITE PLAN DETAILS & TYP. STREET SECTIONS

sheet No.
SD3



Request for Minor Amendment for Garage Front, Private Driveways w/  
Expanded Front Porches on Lot 13 & 14 of Block 6 in Giuliano First Subdivision  
Area SF-3

Lois 1 & 2

The goal of the proposed alternate site layout and architectural criteria is to allow homeowners to have a 2-car garage with private driveway that is recessed behind a front porch. This minor amendment authorizes the proposed site layout on the following two lots: ~~Lot 13 & Lot 14~~ of Block 6, in the Giuliano First Subdivision.

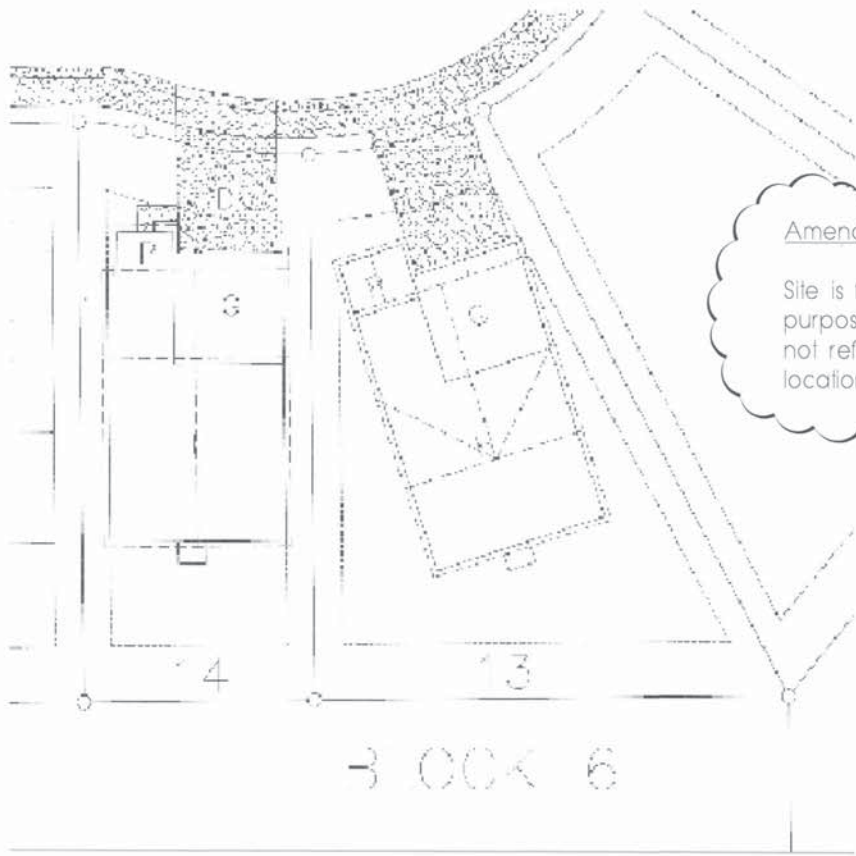
Lois 1 & 2

The following requirements must be met for the proposed garage front private driveway site layout:

- The garage doors shall be positioned behind the front porch by at least 4 feet.
- All dwellings shall have a covered front porch that is at least 10 feet in width with a minimum of 80 square feet.
- The doors for all garages shall incorporate architectural design elements, as shown on the conceptual elevations. The doors of the garages on at least 50% of the dwellings shall incorporate window features.
- The maximum driveway width shall be 18 feet.
- The minimum side yard setback shall be 5 feet for the principal dwelling unit.
- Front yard setbacks, measured from the property line to the front porch, shall be staggered by at least 2 feet. Refer to lot Matrix for lot setbacks
  - Lot 13, Block 6 shall have a minimum front yard setback of 14'.
  - Lot 14, Block 6 shall have a minimum front yard setback of 16'.
- Regardless of the front yard setbacks established above, the minimum setback measured from the back of the sidewalk to the face of the garage door shall be 20 feet.
- Architectural features of these homes shall remain generally consistent with the design features of the built portion of the subdivision in order to blend in to the current streetscape, these features as shown on the conceptual elevations, include, but are not limited to:
  - Roof Pitch shall be 6/12.
  - Each home shall contain masonry elements.
  - Accent colors shall be used on homes to create visual interest along the streetscape.
- Of the two homes permitted with this minor amendment, one home shall be a ranch style home and one home shall be a two-story design.



Elevations and site plans are conceptual drawings, actual build out plans will conform with guildlines and general concept depicted above.



Amendment 4  
Site is for illustrative purpose only and does not reflect amended lot locations.

Amendment 4  
Changes for amendment 4 are indicated by striking out non/applicable items and underlining new guidelines.

SIGNATURE BLOCK:

John Giuliano

Approved this 12<sup>TH</sup> day of SEPT.,  
2010 by the Current Planning  
Manager of the City of Loveland,  
Colorado.

Current Planning Manager

GIULIANO & FATHER  
CONSTRUCTION CO., INC

PROJECT DESCRIPTION:  
GIULIANO FIRST SUBDIVISION  
FINAL DEVELOPMENT PLAN  
LOVELAND, COLORADO

DRAWINGS PROVIDED BY:  
ASPEN HOMES OF COLORADO, INC.  
3037 N. TAFT AVE.  
LOVELAND, CO 80538

DATE:  
7/15/2010  
Revision 4 9/1/11  
Sheet title  
Lot 13 & 14 Blk 6  
Minor Amend

SHEET:  
SD3A



PDP / FDP MINOR AMENDMENT



10  
SD4  
EXTERIOR ELEVATIONS  
Not to Scale  
SF-3 (GDP designated) AREA

Request for PDP / FDP Minor Amendment - ~~Test Case for new Models in area SF-3 of the Giuliano First Subdivision~~

Proposed Alternate Site Layout  
The two ~~four~~ new models will feature side loaded garages located at the front of the lot with the drive to remain shared. These new models will be on no more than ~~(4)~~ lots available for block 6, lots 3-14 in area SF-3 for this test case. The new models will be constructed in pairs, one of each at two different locations with both completed at the one location prior to the start of any model at the other location. Location "A" will be Block 7, Lots 12 & 13 with the new two-story plan located on lot 13 and the ranch located on lot 12. Location "B" will be Block 6, Lots 12 & 13 with the new ranch located on lot 12 and the new two-story located on lot 13 with at least one single story paired with a two story or two single story units paired together.

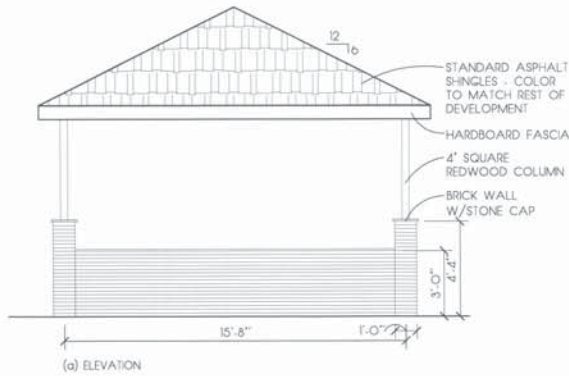
Architecture  
These housing designs will be conventional wood frame construction, one single story plan and one two-story plan, both with 3-bedrooms and two-car garages. Exteriors will be constructed of painted wood/cement composite siding and masonry. Gable and or hipped roofing with a consistent type and color of shingles used throughout the area. Front facades shall have at least two of the following features: bay window, porch, gabled or hipped roof elements or front entry door set as the existing models in the area. The new two-story model will also have living quarters above the garage with a setback of no more than six feet from the front of the garage.

Conclusion  
With the proposed alternate site layout, and architectural criteria the two new models are to be tested for their compatibility in the neighborhood with the existing models as well as a test for the marketability of the neighborhood as a whole.

SIGNATURE BLOCK

Approved this 17<sup>th</sup> day of SEPT. 2009 2011 by the Current Planning Manager of the City of Loveland, Colorado.

*[Signature]*  
Current Planning Manager



(a) ELEVATION

(b) PLAN

NOTE: LOCATIONS AND QUANTITIES OF MAILBOXES ARE DETERMINED BY THE POST OFFICE. TYPICAL 9'-ELTERS TO BE CONSTRUCTED AT THOSE LOCATIONS WHERE APPLICABLE.

9  
SD4  
MAILBOX SHELTERS  
1/4" = 1'-0"  
TYPICAL THROUGHOUT NEIGHBORHOODS

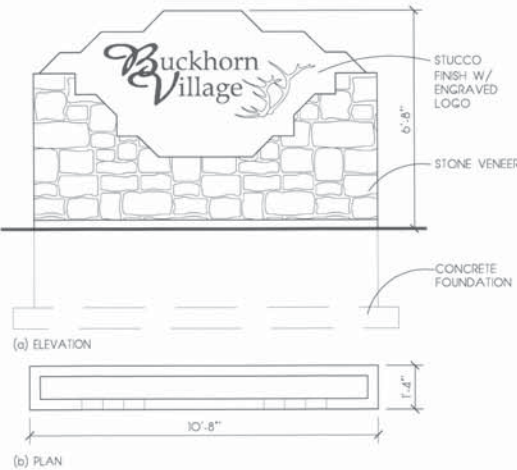


1  
SD4  
EXTERIOR ELEVATIONS  
Not to Scale  
SF-3 & SF-4 (GDP designated) AREAS

Amendment 4  
Changes for amendment 4 are indicated by striking out non/applicable items and underlining new guidelines.



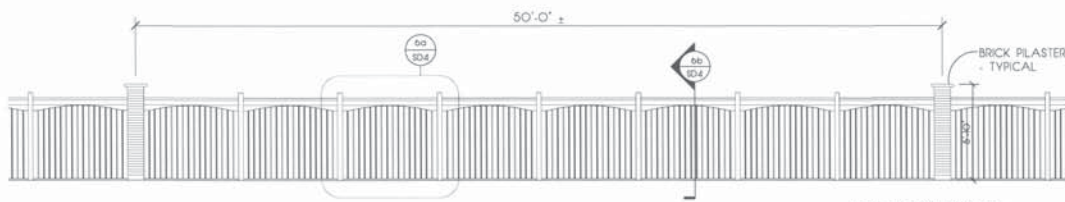
2  
SD4  
EXTERIOR ELEVATIONS  
Not to Scale  
SF-2 (GDP designated) AREA



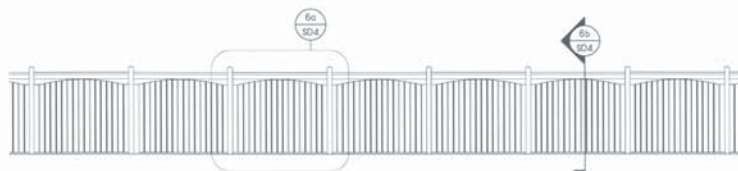
(a) ELEVATION

(b) PLAN

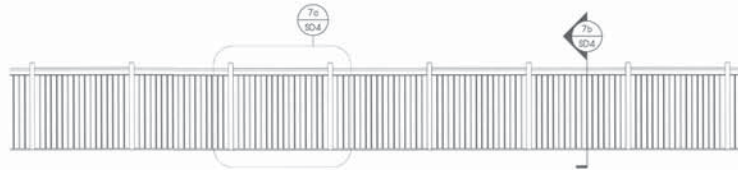
8  
SD4  
MONUMENT DETAIL  
3/8" = 1'-0"  
TYPICAL MONUMENT STYLE ENTRY SIGNAGE



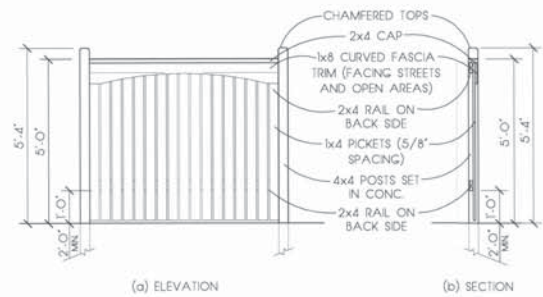
3  
SD4  
FENCE ELEVATIONS  
3/16" = 1'-0"  
TYPICAL NEIGHBORHOOD EDGE FENCE



4  
SD4  
FENCE ELEVATIONS  
3/16" = 1'-0"  
TYPICAL STREET FACING PRIVACY/LOT LINE FENCE



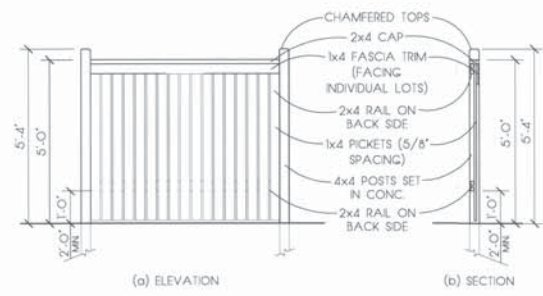
5  
SD4  
FENCE ELEVATIONS  
3/16" = 1'-0"  
TYPICAL PRIVACY/LOT LINE FENCE



(a) ELEVATION

(b) SECTION

6  
SD4  
FENCE DETAILS  
3/8" = 1'-0"



(a) ELEVATION

(b) SECTION

7  
SD4  
FENCE DETAILS  
3/8" = 1'-0"



PROJECT NARRATIVE

Overall Development Concept

Giuliano First Subdivision is the first Preliminary/Final Development Plan for the Giuliano Addition PUD, a 159.6-acre mixed-use planned unit development located in northwest Loveland recently annexed into the city with the approval of the General Development Plan and Annexation Map. The development concept is to provide a mix of residential and commercial uses within the same neighborhood, provide a variety of housing types from affordable housing to fine custom homes, and develop a coordinated and cohesive design approach for the overall development.

Affordable Housing Eligibility and Commitments

Giuliano & Father Construction, one of Loveland's larger entry-level homebuilders, has constructed hundreds of homes over the past fifteen years. In maintaining this tradition, the development of Giuliano Addition PUD will be an opportunity for Giuliano & Father Construction to reexamine, improve and refresh their approach to building affordable homes, as well as utilize the new strategies that City Council and staff are implementing to reduce the time and cost of producing housing. Giuliano First Subdivision meets the city's definition of an affordable housing project as over 20% of the lots, a total of 163 out of 356 lots, will be sold to households with income at 70% or less than the Median Family Income (MFI) for Loveland, as determined by the U.S. Department of Housing and Urban Development.

Giuliano & Father Construction Inc. can construct and sell homes to affordable households through a combination of methods:

1) Creative Planning: By developing Giuliano Addition PUD, jointly with Buck Addition PUD, though they are separate projects, Giuliano & Father Construction is planning to develop a progressive community with housing encompassing multiple price levels. Although these two pieces of land were purchased at current market price with no raw water, because of their size, they can be developed to provide multiple land uses including affordable housing. When completed, this project will be an example of construction efficiency and creative planning.

2) Fast-Track Review: The timely development and construction of Giuliano First Subdivision will be dependent on the ongoing utilization of the City's Fast-track development review process. The project's momentum will rely heavily on open communication and flexibility among all parties involved. We foresee December 2001 as our target date for a final plat, a final development plans and final construction plans.

3) Use Tax Credit: Giuliano & Father Construction will use the Use Tax Credit to reduce the cost of the homes. Additionally, Giuliano & Father Construction will follow the CHAFA requirement ensuring that Giuliano Addition's affordable homes are sold to first time, entry-level buyers.

4) Development Standards Modifications: Based on the assumption that the homebuyers of the larger homes are more likely to be financially able to upgrade their own landscaping, Giuliano & Father Construction proposes to use the 20% reduction in overall landscape requirements and the 50% reduction of the sizes of the trees and bushes in the parcels that are not classified as affordable housing.

As an established developer and builder of entry-level housing, Giuliano & Father Construction's incentive to build affordable housing is to develop a community where people from all walks of life and income levels live together and to assure the citizens of Loveland that their need for affordable housing is being met.

Land Use

The limits of the Giuliano First Subdivision include the entire 159.6 acres of Giuliano Addition PUD. The overall concept is to locate the lower density areas on the west end of the property and the higher density areas on the east. Within this concept we have also integrated other land uses such as open spaces and the school site into the higher density areas, thus changing its overall density.

The highest density/ use areas of this project are the multi-family and the neighborhood commercial center. These areas are located on the far east end of the property and will be developed in future FDP applications.

Included in this FDP application are three separate single-family areas all of which have different densities. These single-family areas are referred to as SF-1, SF-2, SF-3, and SF-4 (see sheet CVR, "GDP designation map"). A detailed breakdown of the residential land uses is as follows, and on sheet CVR of the FDP drawing set.

SF-4 will be constructed in Phase 1. SF-3 will be constructed in Phase 2, along with an interim detention pond. Combined, SF-3 and SF-4 have a total of 163 lots resulting in a net density of 6.67 units per acre.

SF-2 will be constructed in Phases 3, 4 and 5 with the total of 176 lots, resulting in a net density of 4.63 units per acre.

SF-1 will be constructed in Phase 6 with the total of 18 lots, resulting in a net density of 176 units per acre.

Circulation

West 43rd Street provides all of the current accesses into the project and future accesses will be provided via Florence Drive to 50th Street. A series of local collector streets will connect the various mixed-use components of the neighborhood. Local residential streets throughout the single-family areas will connect to the collector streets. Convenient access to Buck First Subdivision, the proposed Wilson Commons and adjacent undeveloped land has also been incorporated in the vehicular circulation design. A number of traffic calming devices are proposed throughout the neighborhood including several raised pedestrian crossing/speed humps located to break streets and to connect bicycle and pedestrian trails and green belts. Additionally, a raised intersection has been included in the design where there is a 4-way intersection.

The dedication of the R.O.W. and the design of the improvements for W. 43rd Street to minor arterial standards have been designed in conjunction with the annexation of Giuliano Addition PUD and the phased PDP development of the Buck Addition PUD. Similarly, with the development of adjacent properties, N. Wilson Ave. will be improved to major arterial standards. Refer to sheet SD-4 for typical street cross sections and final street construction plans by others.

Bicycle/pedestrian circulation has been planned to run through the single-family residential development, connecting into the private neighborhood park/play areas and the larger bicycle path network. Giuliano First Subdivision is providing a widened trail to serve as the connection between the Larimer County park trail and the City of Loveland's trail system. This trail is located along W. 43rd Street from N. Wilson Avenue to Sullivan Avenue, then north between the parcel designated SF-4 and the detention pond and the school, then west ending at Florence Drive. The recreation trail east of Florence Drive is to be maintained by the H.O.A. until the city determines and constructs the final connection alignment from the west. A 15-foot public trail easement shall be dedicated and conveyed to Larimer County west of Florence Drive along the north property line for bikes, equestrian and pedestrian recreational purposes; see plat for reception number. This trail easement shall be maintained by the H.O.A. until Larimer County begins trail construction, and thereafter Larimer County shall assume maintenance of the trail and trail easement. Other trails encircle the development with the buffer yard /greenbelt areas and provide points of connection into the proposed Wilson Commons and adjacent undeveloped land parcels.

Public & Private Utilities

All public utilities and services are immediately available to the site along W. 43rd Street or otherwise adjacent to the site, and will be installed underground. Refer to the final utility drawings for specific details regarding the exact line locations and sizes.

Water service will be provided by a westward extension of the existing 16" waterline in W. 43rd Street with a new 24" waterline. Cascade Avenue will also house a new 24" waterline that will tie into the westward extension. Three new services will be installed off of W. 43rd Street as follows:

- 8" waterline along Sullivan Avenue to serve MF-1, C-1, and SF-4.
- 12" waterline along Florence Drive to serve SF-2, SF-3, and SC-1.
- 8" waterline along Fife Court to serve SF-1.

Sanitary sewer will be provided by the extension of a new 8" service line along Sullivan Avenue from the existing 12" main located in W. 43rd Street. The remainder of the development will be serviced with new 8" sewer lines.

All utility lines shall be coordinated with the required street trees and other landscaping materials to be in conformance with the following City of Loveland standards:

- Trees shall be planted no closer than 4'-5' from existing or proposed electrical lines.
- Trees shall be planted no closer than 4' from existing or new natural gas lines.
- Trees shall be planted no closer than 10' from water/wastewater lines.
- Trees shall be planted no closer than 10' from storm sewer lines.

A SWMM model has been developed for the Caddoa Basin in which Giuliano Addition PUD is located. The proposed regional pond will be sized in accordance with the revised SWMM. All flows from this property will be to the regional pond, with the exception of the parcels designated MF-1 & C-1, and the back of the lots of SF-4, which will flow undetained to the existing regional pond in the Buck First Subdivision. The construction of the regional pond will be included in Phase 2 of this project. Refer to the Final Drainage and Erosion Control Report and plans for details on this Regional Pond.

Public utility services will be provided by the following jurisdictions:

- Water: City of Loveland
- Sanitary Sewer: City of Loveland
- Electric: City of Loveland
- Gas: Public Service of Colorado
- Telephone: Qwest
- Cable TV: Comcast Communications

Public Facilities Providers

The City of Loveland will provide police and fire protection, with street widths and other infrastructure designed to accommodate emergency vehicles. The entire site lies outside of the 1 1/2 mile response diamonds for all existing or planned fire stations, therefore all multi-family and commercial structures over 5,000 gross sq. ft. in area will be required to be protected by automatic fire sprinkler systems.

Several private neighborhood park/play areas are proposed in conjunction with the single-family residential areas of the development. These parks will be centrally located and be connected by bicycle/pedestrian trails. Bicycle pedestrian facilities are discussed in another section of this narrative.

School facilities serving the development will be Centennial Elementary, Lucile Erwin Middle School and Loveland High School. The proposed elementary school in this development will result in some school boundary realignment. An impact report will be prepared by the school district for this development.

Landscaping

A traditional, tree-lined streetscape has been designed for the Giuliano First Subdivision along N. Wilson Avenue, W. 43rd Street, and along the local collector streets. Clusters of some species large shade trees including varieties of Honey locust, Ash, and Maple will be planted at approximately 40' o.c., providing variety in the branching and color. Minimum size for all street trees will be 2" caliper.

The developer will provide full front yard landscape packages with the sale of the lot, in the parcels designated SF-3 and SF-4. This landscaping will be constructed prior to the issuance of a Certificate of Occupancy.

Lots in area SF-2 will receive sod certificates for the front yards and a certificate for a 2" caliper tree at closing to be installed by the homeowner. Similarly, homeowners in area SF-1 will also receive a certificate for a 2" caliper canopy tree at closing to be installed by the homeowner. The landscaping in these areas will be constructed within 90 days after the issuance of a Certificate of Occupancy by the homeowner. If weather prohibits installation, all landscaping shall be installed during the next growing season, no later than June 20th of the following year. All residential lots will be required to landscape the front yard with a minimum of 70% living plant materials.

The private neighborhood parks will be sodded, and the greenbelts and other common open spaces will be seeded with a blend of Kentucky Bluegrass. Both areas will be irrigated with an underground automatic sprinkler system. The detention pond will be non-irrigated and seeded with native dryland grasses. A temporary irrigation system will be installed for this area, tract 'C' and shall remain until all vegetation is healthy and grasses have germinated. Phasing of the landscaping is consistent with the proposed construction phasing. The landscaping in each phase will be installed prior to the issuance of any building permits for that phase. Refer to phasing map on sheet CVR of the FDP drawing set. Landscaping in the playfields shall be maintained by the homeowner's association and shall have no effect to the amount of capital expenditures fees otherwise imposed under Chapter 16.38.

An architectural fence has been exclusively designed for this neighborhood. The design for the typical neighborhood edge wood fences will be used to delineate the single-family residential development parcels along the W. 43rd Street frontage. Brick masonry piers will break up edge fences. A maximum of 5' high privacy/lot line fences of the same design will be permitted on the lot lines of individual lots except that the brick piers will not be required. Fences in the residential areas will not extend beyond the front building line. Refer to fence details on sheet SD4 of the FDP drawing set. Phasing of the fencing installation is consistent with the proposed construction phasing. The fencing, in which the developer is responsible for in each phase, will be constructed prior to the issuance of any building permits for that phase. Individual lot fencing should be the homeowner choose to install a fence, will be constructed after the issuance of a Certificate of Occupancy. Refer to phasing map on sheet CVR of the FDP drawing set.

Architectural Concept

Giuliano First Subdivision will be traditional in character and designed in an architecturally consistent manner. Through the use of respective design elements, and recurring architectural details, the design of the structures will be similar in character, unifying the community. Additionally, each lot will have set building envelope with staggered front yard setbacks to add variety in the streetscape. Refer to the lot setback matrix on sheet CVR of the FDP drawing set. Color schemes will also be consistent throughout the project using primarily earth tones, and accent colors will be used in limited amounts for other architectural elements. All architectural standards will be incorporated into covenants for the neighborhood, and then used to evaluate proposed housing designs by the City of Loveland and the Architectural Control Committee. A draft copy of the Architectural Control Committee and Homeowners Association covenants accompanies this FDP submittal.

The following are architectural guidelines as required by the GDP conditions of approval and will in turn substantially bring this project into conformity with the proposed Title 16 requirements:

Except as specified in the GDP for single-story garages in areas SF-3 and SF-4, the standard for side yard building setbacks shall be as specified in the Municipal Code, which is one foot for every 3 feet of building height, or fraction thereof, but a minimum of 5 feet.

Lots with less than 65 feet of frontage on a public street right-of-way shall not be permitted a garage where garage doors for more than two cars are visible as part of the front building elevation.

On lots with less than 65 feet of frontage on a public street, garage doors that are visible as part of front building elevations shall be recessed behind either the front facade of the living portion of the dwelling or a covered porch by at least four (4) feet. Covered porches referred to in this condition shall measure at least eight (8) feet, measured across the front of the building by six (6) feet.

Garage doors visible, as part of front building elevations shall not comprise more than forty (40) percent of the ground floor street-facing linear building frontage. Alleys and corner lots are exempt from this condition.

Front facades of single-family dwellings shall include at least two of the following features: bay window, porch, or gabled or hipped roof element.

Minimum front yard building set backs for residential portions of single-family dwellings in areas SF-3 and SF-4 shall be varied and the minimum setbacks shall be 15 feet except for garage door which shall require a minimum setback of 20 feet.

The single-family residential parcels SF-3 & SF-4 are unique in that the construction of the entire areas will be planned. Tentatively, eight different house plans, without basements have been designed for this area. These housing designs will be conventional wood frame construction one and two story floor plans with 2 to 4 bedroom and a two-car garage. Plans will range from 850 to 1800 square feet exclusive of garage, and marketed to first time homebuyers. Exteriors will be constructed of painted wood/cement composite siding, masonry, or stucco. Gable and or hipped roofing with a consistent type and color of shingles will be used throughout the area. Front facades shall have at least two of the following features: bay window, porch, gabled or hipped roof element, or front entry door.

Basements will be an option for the residential lot in parcel SF-2. The submitted house plans will be equal to or better than the plans in the Buck Addition and Windsor/Sun Disk Village developments. Front yard setback will be staggered with a minimum of 20 feet and a maximum of 24 feet as measured from the back of the sidewalk, depicted by lot and block number in a matrix and submitted with the FDP application. Conventional wood frame construction for the one and two story floor plans with 2 to 4 bedrooms and a two-car garage will range from 850 to 2000 square feet. Additionally 75% of the front elevation options shall include one of the following elements: aside load garage, the garage doors recessed a minimum of (4) four feet behind the front facade, a covered porch, or a box bay window. Front facades shall also include a minimum of 2 windows for a single story house and a minimum of 3 windows for a two-story dwelling.

The single-family residential parcel SF-1 will be one and two story floor plans with 3 to 5 bedrooms and an attached garage housing a minimum of three vehicles. Plans will range from 1800 to 4000 square feet exclusive of garages. Utility infrastructure and grading designs should allow the construction of basements or walkout levels in all of these residences. Setbacks shall be consistent with the City of Loveland's requirements. All exteriors will be constructed of painted wood/cement composite siding, various forms of masonry or stucco. Roofing materials will be, but not limited to, clay tiles and laminated fiberglass shingles with a minimum roof pitch of 5/12 for primary roof planes. The location of the garage will be even with or behind the front facade. Front facade shall also include a minimum of 3 windows for a single story house and a minimum of 4 windows for a two-story dwelling.

Landform Modification

The existing land use on the Giuliano Addition PUD is dryland farming, and the land slopes in a south/southeasterly direction. No significant landform modifications are necessary as a part of this development, other than for construction of the regional stormwater detention facility. Cut and fill will be required to provide adequate drainage away from proposed structures and to create the necessary drainage swales to the on-site detention facility.

Vegeting/Construction Phasing

Vegeting of property rights for Giuliano First Subdivision will occur upon final approval of this FDP/FDP.

The single family residential development will be built in multiple phases, with total build out planned for 7-8 years.

Construction phasing is proposed as follows: (see also the phasing map on sheet CVR of the FDP drawing set)

- Phase 1: Single-family area SF-4, including 105 lots and the development of the private park/play area tract B
- Phase 2: Single-family area SF-3, including 57 lots, the regional pond, the development of the private park/play area tract H, and the School Site area SC-1
- Phase 3: A portion of Single-family area SF-2, including 69 lots and the development of the private park/play area tract N
- Phase 4: A portion of Single-family area SF-2, including 69
- Phase 5: A portion of Single-family area SF-2, including 38 lots and the development of the private park/play area tract O
- Phase 6: Single-family area SF-1, including 18 lots

Implementation/Regulatory Procedures

A homeowner's association will be established to govern the residential areas of the community and to maintain the neighborhood edge fencing, signage, neighborhood parks, and landscaping in the common open spaces as well as the landscaping in the public right-of-ways throughout the residential development. Protective covenants will be developed to insure the quality of all construction, establishing design standards for all residential building types. The Architectural Control Committee will also review and approve all proposed housing of the Giuliano Addition PUD prior to the issuance of building permits. A letter of this approval will be submitted with each building permit application.

The current annexation and zoning status of Giuliano Addition PUD allows the developer the necessary design freedom and flexibility to ensure the economic feasibility of the project as long as the conditions of approval are met.

Notwithstanding any information presented graphically in the FDP or accompanying document, all public improvement shall conform to the standards adopted June 1, 1999 unless specifically approved in writing a variance.

Request for Variance/Exception of Giuliano First Subdivision - Loveland, Colorado \*1

This request for a variance is being submitted in conjunction with the submission of Preliminary Development Plan for the Giuliano First Subdivision project. Information regarding this request is also included in the Regulatory Procedures section of the Preliminary Development Plan. It is our intention to substantiate and justify this request for a variance for the required intersection spacing by identifying the standard(s) to be waived, describe the proposed alternate design, compare the proposed alternate design to the standard, and justify the request.

Applicable City of Loveland Development Street Standards  
4.2.4 Intersection Spacing  
Specific distances shall be as shown in Table 4.2 of these Standards.  
Table 4.2 - Street Standards - Technical Design Criteria

Proposed Alternate Design  
The proposed intersection spacing between future Cascade Avenue and Fife Court is designed to be 440' from centerline to centerline.

Comparison to Standards  
The proposed intersection spacing of 440' is less than the required 1320' for an unsignalized minor arterial street without a raised median.

Justification  
The variance request is justified as follows:  
1. The size and location of the parcel is such that any access point into the parcel, whether off Cascade Avenue or W. 43rd Street, would require a variance.  
2. The Fire Department is not requiring a secondary access into the parcel as long as each residence is sprinkled. The developer has agreed to this condition. Therefore, if the variance is granted, the City of Loveland will not have the maintenance responsibility of future Cascade Avenue and a portion of W. 43rd Street.  
3. This variance will not affect the financial responsibility for the construction of the future Cascade Avenue, or the dedication of the easement.  
4. According to the preliminary grading plan, the site distance requirements are satisfied with the proposed street layout.

Conclusion  
The applicant respectfully requests that the standards regarding intersection spacing be waived as proposed because access into this parcel is limited and this proposed location meets the intent of the standards as applicable in this particular instance.

Request for Variance/Exception of Giuliano First Subdivision - Loveland, Colorado \*2

This request for a variance is being submitted in conjunction with the submission of Preliminary Development Plan for the Giuliano First Subdivision project. Information regarding this request is also included in the Regulatory Procedures section of the Preliminary Development Plan. It is our intention to substantiate and justify this request for a variance for the required intersection spacing by identifying the standard(s) to be waived, describe the proposed alternate design, compare the proposed alternate design to the standard, and justify the request.

Applicable City of Loveland Development Street Standards  
4.2.4 Intersection Spacing  
Specific distances shall be as shown in Table 4.2 of these Standards.  
Table 4.2 - Street Standards - Technical Design Criteria

Proposed Alternate Design  
The proposed intersection spacing between Fife Court and Slump Avenue is designed to be 590' from centerline to centerline.

Comparison to Standards  
The proposed intersection spacing of 590' is less than the required 1320' for an unsignalized minor arterial street without a raised median.

Justification  
The variance request is justified as follows:  
1. The ideal location for Slump Avenue would be at the cross section with Divide Drive, the entrance into Buck Second Subdivision. But through an inspection of the traffic analysis an offset intersection at adequate spacing vs. a four-leg intersection creates less delay to the minor street, therefore moving Slump Avenue to the west.  
2. Relocation of Slump Avenue to the west to accommodate the addition of an acceleration lane in the striping plan between Slump and Divide manages the lack of vertical site distance.  
3. In addition to the acceleration lane added, the vertical elevation of the intersection of Slump Avenue and W. 43rd Street will be raised during the final design of W. 43rd Street to again relieve the lack of vertical site distance.

Conclusion  
The best scenario to meet the street standards would be to completely eliminate this access all together, but the Fire Department requires a secondary access for this parcel. Therefore, the applicant respectfully requests that the standards regarding intersection spacing be waived as proposed because the design of the access mitigates outstanding issues and meets the intent of the standards as applicable in this particular instance.

REVISIONS

1	5/1/02
2	11/1/02
3	1/15/03



GDP CONDITIONS OF APPROVAL

ENGINEERING

1. 43rd Street - Right-of-way and Improvements. If determined necessary by the City Engineer, the developer shall acquire and dedicate to the City, at no cost to the City, sufficient right-of-way along the southern portion of the westernmost 900 feet of the proposed addition to construct all required standard improvements for 43rd Street along the entire southern boundary of the addition. This condition shall be effective upon submittal of the appropriate PDP application, as determined by the City Engineer.
2. Cascade Avenue - Right-of-way and Improvements. If determined necessary by the City Engineer, the developer shall acquire and dedicate to the City, at no cost to the City, sufficient right-of-way along the western boundary of the proposed addition to construct all required standard improvements for Cascade Avenue. This condition shall be effective upon submittal of the appropriate PDP application, as determined by the City Engineer.
3. Wilson Avenue - Right-of-way and Improvements. The developer shall acquire and dedicate to the City, at no cost to the City, sufficient right-of-way along the eastern boundary of the proposed addition to construct all required standard improvements for Wilson Avenue. This condition shall be effective upon submittal of the first PDP application.
4. Transportation Master Plan. Future development applications within this project shall fully comply with any updates to the City of Loveland Transportation Master Plan adopted by the City Council for any portion of this property.
5. Unless constructed by others, the developer shall design and construct a raised median for Wilson Avenue extending from the intersection of 43rd Street to 200 feet south of the intersection of Glen Isle Drive. The timing of this improvement shall be based on which PDP application is shown to cause the need for mitigation, based on level-of-service standards as applied to the intersection of Wilson Avenue and Glen Isle Drive. Other mitigation measures may be substituted for this improvement if accepted by the City Engineer.
6. At the time of each PDP application, a Traffic Impact Study shall be submitted which includes a trip distribution based on the gravity model or an alternative distribution method acceptable to the City Engineer.
7. Notwithstanding conceptual information presented in the GDP, access locations and operations shall be determined at the time of application for PDP approval.

PARKS AND RECREATION

8. Within the annexation parcel, the applicant shall dedicate to the City of Loveland a 25-foot wide tract of land to be used for a recreation trail. The parcel shall provide a connection from a location near the northwest corner of the annexation parcel to a currently undetermined point on 43rd Street. The trail installation shall be the responsibility of the developer and shall be built to City of Loveland standards. The location of the trail shall be determined by the City after consultation with the developer at the time of the first PDP/Preliminary Plat within the annexation parcel. The location of the trail shall not unreasonably interfere with the developers preferred subdivision.

WATER/WASTEWATER/STORM DRAINAGE

9. The Developer shall design and construct a 24" water line along the south property line, from the west boundary of Buck 2nd Subdivision, along West 43rd Street, to the west boundary of Giuliano Addition, then north along the west property line to the north boundary of this addition. The City may approve alternate alignments at the time of a Preliminary Development Plan submittal.
10. The Developer shall design and construct an extension of the existing 16" waterline in W. 43rd Street from its existing terminus, to connect with the aforementioned 24" waterline along the south property line.
11. The Developer shall design and construct waterlines within the subject annexation that connect to the aforementioned 24" waterline along the west boundary and to the existing 16" waterline in W. 43rd Street and existing 30" waterline in N. Wilson Avenue. Specific alignments, connection points and sizing requirements will be determined by the City at the time of a Preliminary Development Plan.
12. The Developer shall complete an Engineering Analysis of the surrounding and other applicable water system facilities in order to determine the need for additional water system improvements. The City will determine the content and specific parameters of the Analysis at the time of a Preliminary Development Plan submittal.

POWER

13. A 5% surcharge will be assessed on the sale of electricity to any development for a period of 10 years from the date of annexation. A note will be required on each final plat regarding this requirement as follows: "A surcharge of 5% will be added to all bills for the sale of electric power to additional services which come into the existence after January 31, 1987, within the territory herein annexed, which surcharge will expire ten years after effective date of this annexation."

VESTING (added by City Council)

- 13a. Waiver of Damages. In the future, the Developer may be granted vested property rights associated with the approval of a site specific development plan within the property. In the event that such vested property rights are granted, and the City applies an initiated or referred measure to the property which would (a) change any term or condition of this Agreement, (b) impose a moratorium on development within the property, or otherwise materially delay the development of the property, or (c) limit the number of building or utility permits to which the Developer would otherwise be entitled, the Developer agrees to waive any right to damages against the City to which the Developer may otherwise be entitled under the Vested Rights Statute.

CURRENT PLANNING

14. PDP applications within 100 feet of the base limestone ridge shall include the results of the Bell's Twinpod survey. If the plant is located on the site, an appropriate buffer as determined by the City shall be preserved as open space.
15. Except as specified in the GDP for single-story garages in areas SF-3 and SF-4, the standard for side yard building setback shall be as specified in the Municipal Code, which is one foot for every 3 feet of building height, or fraction thereof, but a minimum of 5 feet.
16. Lots with less than 65 feet of frontage on a public street right-of-way shall not be permitted a garage where garage doors for more than two cars are visible as part of the front building elevation.
17. On lots with less than 65 feet of frontage on a public street, garage doors that are visible as part of front building elevations shall be recessed behind either the front facade of the living portion of the dwelling or a covered porch by at least four (4) feet. Covered porches referred to in this condition shall measure at least eight (8) feet, measured across the front of the building, by six (6) feet.
18. Garage doors visible as part of front building elevations shall not comprise more than forty (40) percent of the ground floor street-facing linear building frontage. Alleys and corner lots are exempt from this condition.
19. Future PDP applications shall include useable open space that is easily accessible to residential lots throughout a development. Open space shall be thoroughly integrated into residential projects.
20. To address noise impacts on residential areas, PDP applications for development within zoning area C-1 shall include a noise study demonstrating compliance with City noise standards for residential areas. This study shall include analysis of loading, delivery, and operational noise sources. Potential restrictions on these operations including limitations on hours of operation may be imposed to achieve compliance with City standards.
21. The annexation map shall be revised to include the following statement in the Notes section. This annexation is subject to an annexation agreement which has been recorded in the real property records of Larimer County.
22. The GDP narrative shall be revised to reflect that fences other than the neighborhood edge fence shall be a maximum of five feet in height and shall match the lot line fence shown on SD1.
23. In area MF-1 the maximum number of congregated care units shall not exceed 50 dwelling units.
24. Front facades of single-family dwellings shall include at least two of the following features: bay window, porch, or gabled or hipped roof element.
25. Notwithstanding the typical street cross-sections shown on SD-1, the street sections proposed with subsequent PDP applications shall comply with City street standards.
26. Minimum front yard building set backs for residential portions of single-family dwellings in areas SF-3 and SF-4 shall be varied and the minimum setbacks shall be 15 feet except for garage door which shall require a minimum setback of 20 feet.
27. Language in the GDP narrative indicating that in area SF-1 the minimum lot width may be 60 feet and corner lots must be at least 70 feet shall be deleted.
28. In areas SF-3 and SF-4, the maximum gross density shall be 5.5 dwelling units per acre instead of 6.0.

PDP CONDITIONS OF APPROVAL

Stormwater:

1. The Developer shall design the stormwater drainage plan for this PDP to provide 100 year level detention with a 2-year historic release rate, as identified in the master plan update, until such time that the regional detention pond is constructed. Prior to issuance of the first building permit in this PDP, the Developer shall dedicate to the City the land for the future regional detention pond as depicted on the PDP. The Developer shall be eligible for reimbursement through standard City reimbursement policies based on undeveloped land cost.
- Water / Wastewater:
2. The Developer shall coordinate the design and installation of the project related utilities with the City's proposed waterline in West 43rd Avenue. The final construction plans shall reflect specific alignments and connection points.
3. The Developer shall design and construct the proposed sanitary sewer main and manholes to be entirely outside any portion of the 100-year water surface elevation limits for the proposed storm water detention pond. Additionally, access for heavy maintenance vehicles along the entire reach of sanitary sewer main and manholes, within the proposed storm water detention pond area, must be provided by a minimum 20' all weather access roadway. The specific alignment and configuration of the sanitary sewer main and access roadway can be determined with the Final Development Plan.
4. At no cost to the City, the Developer shall design and install a public water booster station. The station shall be installed prior to issuance of any building permit within that portion of the PDP situate west of Florence Drive. At a minimum, the station shall be sized to accommodate all proposed lots west of Florence Drive as well as the entire adjacent development known as the Buck 2nd Subdivision. The specific location of the water booster station shall be shown on the final development plan. The Developer shall be responsible for the cost, creation and/or acquisition of any public easements necessary to accommodate the herein mentioned water booster station and all associated main line extension(s) and related appurtenances.

Fire Prevention:

5. All dwellings on Fire Court shall be protected with automatic sprinkler systems in accordance with NFPA 13R.
6. A temporary emergency access roadway shall be provided from Kincald Drive to Wilson Avenue prior to issuance of any building permits in Phase 2. The construction plans for such temporary roadway shall be subject to approval by the City of Loveland Fire Department and City Engineer.

Parks and Recreation:

7. The trail locations as proposed on the PDP shall conform to Larimer County and City of Loveland requirements and shall be subject to review and approval by the City of Loveland Parks and Recreation Department. All such trails shall be depicted on the FDP and all final grades and width requirements shall be depicted on the final construction drawings. The final design of all such trails shall be coordinated with the design of storm water or detention channels and basins proposed within this PDP and on the adjoining property to the north. Detailed cross sections of the trail alignment along the north property line of this PDP, adjacent to the school site and west of Florence Drive, shall be submitted for review with the FDP. Such alignment and cross sections shall be subject to approval by the City of Loveland Parks and Recreation Department.
8. The recreation trail shall not be located within any part of the drainage conveyance system (north of the school or in the County trail area) and shall be located above and outside the surface elevation of the 20 year storm event. The surface elevation of the 20 year storm event shall be depicted on the FDP.
9. The recreational trail shall be designed and constructed by the Developer according to City of Loveland recreation trail standards and shall not exceed ADA standards for maximum grades. Construction drawings for said trail shall be subject to approval by the City of Loveland Parks and Recreation Department prior to approval of the FDP.

Engineering:

11. Notwithstanding any information shown in the accompanying application documents, all public improvements within this PDP shall conform to the standards adopted on June 1, 1999, unless specific variances are requested and approved by the City in writing. The variances approved as part of this PDP are as follows:
- a. This PDP shall not be subject to the connectivity standard adopted by the City after January 2001.
- b. Any off-site improvements to North Wilson Avenue south of 29th Street shall comply with the design plans for such improvements on-file with the City.
12. Unless designed and constructed by others, the Developer shall design and construct the following improvements prior to issuance of any building permits in this PDP, or by the time otherwise specified for the following improvements:
- a. 43rd Street. The Developer shall construct that portion of West 43rd Street adjacent to the Giuliano Addition to minor arterial street standards.
- b. Cascade Avenue. The Developer shall design and construct that portion of Cascade Avenue adjacent to the Giuliano Addition to minor arterial street standards. The City may accept cash in-lieu for all or a portion of the construction costs for such improvements, in a form acceptable to the City Attorney, if approved by the City Engineer.
- c. Wilson Avenue.
- (i) Adjacent Portion. The Developer shall construct the portion of North Wilson Avenue adjacent to the Giuliano Addition to major arterial street standards including off-site tapers to the north.
- (ii) Widening to 29th Street. The Developer shall construct North Wilson Avenue from 43rd Street to 22nd Street as a major arterial street. Design standards shall be as described in Condition 11b.
13. The intersection of Slump Avenue and West 43rd Street shall be designed and constructed in accordance with the letter of variance dated September 26, 2001 and approved September 26, 2001.

Current Planning:

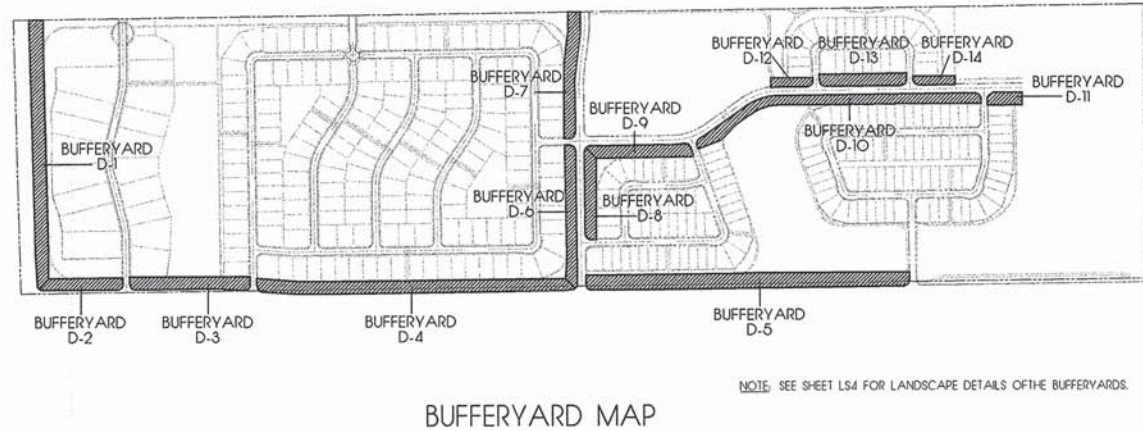
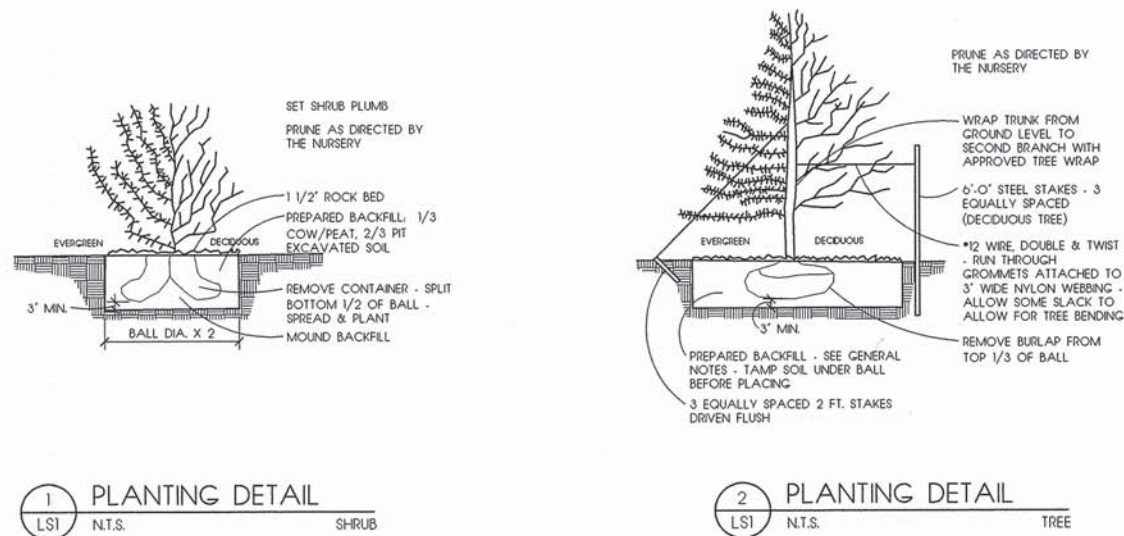
14. In accordance with the requirements set forth in the Giuliano Addition GDP, the landscape plan of the FDP shall include a habitat enhancement plan designed to mitigate the loss of wetlands on the project site. The landscape plan shall also include a temporary irrigation system for the area in and around the regional stormwater detention facility and such system shall remain until all vegetation planted in and around the regional stormwater detention facility is healthy, and grasses have germinated. Such plan must also include the variety of riparian species as listed in the GDP. No building permits shall be issued once construction of the regional stormwater facility is completed unless the Developer posts a cash security with the City for the landscaping, mitigation measures, and irrigation system required under this condition. In no case shall building permits be issued for Phase 2 until such security has been posted with the City. The City shall install the landscaping, mitigation measures and irrigation system required under this condition as part of its design and construction of the regional stormwater detention facility. The Developer's security required under this condition shall be transferred to the City, which shall use the funds to install the landscaping, mitigation measures and irrigation system required under this condition.
15. Prior to issuance of any building permits in Phase 1, a native seed mix shall be planted to cover the total area of Tract F, including the temporary stormwater detention facility. The FDP shall identify the type of native seed mix as well as the quantity and method that will be used for seeding. A temporary irrigation system shall be installed and remain until all grasses have germinated. The landscape security for Phase 1 shall include the costs of planting the seed mix and installing the irrigation system required under this condition. That portion of the security for the grasses and temporary irrigation system shall not be released until the City has determined that all grasses have germinated and are in a health condition.
16. The smaller detention areas within area SF-1 shall be landscaped with native grasses (wetland mix), shrubs and trees compatible for prairie riparian areas below 6,000 feet.
17. Overlaid grading within Block 1b of the plat shall be minimized and building envelopes depicted on the FDP and plat to avoid disturbance to the limestone ridge located on Outlot 3.
18. The location of the required water booster station shall be approved by all departments at time of FDP.
19. In accordance with the requirements set forth in the Giuliano Addition GDP, a cluster mailbox system to service groups of single family homes shall be included with any development proposal. The FDP shall depict the specific architectural treatment and landscaping for these clustered mailboxes.
20. In accordance with the requirements set forth in the Giuliano Addition GDP, private neighborhood park/play areas and greenbelts shall be landscaped in a softer, less formal manner, using groupings or clustered trees and shrub masses to delineate a variety of active and passive activities. The FDP landscape plan shall be consistent with this requirement.
21. In accordance with the requirements set forth in the Giuliano Addition GDP, a homeowner's association shall be established to govern the residential areas of the community and to maintain the landscaping, fencing, signage, neighborhood parks, and common open space throughout the residential development. Protective covenants shall also be developed to insure the quality of all construction and establishing design standards for all residential building types. The developer shall also create an architectural review committee to review and approve all proposed housing for the Giuliano Addition GDP, to be established prior to the issuance of the first building permit within the GDP. Notwithstanding the foregoing, the City shall not be bound by the decisions of the architectural review committee and shall incur no liability for the issuance of building permits approved without approval from the architectural review committee.
22. A separate sign permit shall be required for the entry monument sign.
23. In accordance with the requirements set forth in the Giuliano Addition GDP, a minimum of 20% of the single family detached dwellings in this PDP shall be sold to households with incomes at 70% or less of the Median Family Income (MFI) for Loveland, as determined by the U.S. Department of Housing and Urban Development.

Planning Commission Condition:

24. Phase 1 shall include an open space play area of at least 0.5 acres. Phase 2 shall include an open space play area of at least 1.0 acre.

1	5/1/02
2	11/1/02
3	1/15/03





### LANDSCAPE LEGEND



### GENERAL LANDSCAPE NOTES

- STREET CANOPY TREES FOR THE SINGLE FAMILY LOTS ARE INDICATED BY THE MARK 'CT' ON THE LANDSCAPE PLAN AND THE PLANT LIST. ONE TREE PER LOT, AND TWO TREES PER CORNER LOTS ALONG LOCAL STREETS WILL BE PROVIDED BY THE DEVELOPER THROUGH THE ISSUANCE OF A CERTIFICATE TO THE LOT OWNER FOR THE INSTALLATION OF THE TREE OF THEIR CHOICE FROM THE CITY OF LOVELAND'S APPROVED LIST OF SPECIES.
- PLAY AREAS SHALL BE MAINTAINED BY THE HOMEOWNERS ASSOCIATION AND SHALL HAVE NO EFFECT TO THE AMOUNT OF CAPITAL EXPENDITURES FEES OTHERWISE IMPOSED UNDER CHAPTER 16.38.
- LANDSCAPE PLAN IS DIAGRAMMATIC. TREE AND SHRUB LOCATION MAY BE ADJUSTED, AS NEEDED, AT TIME OF PLANTING TO MAINTAIN A MINIMUM SEPARATION FROM UTILITY LINES AS FOLLOWS:
  - TREES NO CLOSER THAN 4'-5' FROM ELECTRICAL LINES.
  - TREES NO CLOSER THAN 4' FROM NATURAL GAS LINES.
  - TREES NO CLOSER THAN 10' FROM WATER/WASTEWATER LINES.
  - TREES NO CLOSER THAN 10' FROM STORM SEWER LINES.
  - SHRUBS 4' MINIMUM SEPARATION.
- ALL LANDSCAPING ILLUSTRATED BY THIS PLAN SHALL BE INSTALLED AND MAINTAINED BY THE DEVELOPER UNLESS OTHERWISE INDICATED.
- SCHEDULED SIZES STATED REPRESENT MINIMUM CALIPER AND HEIGHT. SIZES PROVIDED MAY EXCEED MINIMUM REQUIRED, BUT IN NO EVENT SHALL SMALLER PLANT MATERIAL SIZES BE PROVIDED.
- ALL LANDSCAPE AREAS SHALL BE IRRIGATED WITH AN AUTOMATIC UNDERGROUND IRRIGATION SYSTEM, AND IRRIGATED WITH DOMESTIC WATER, UNLESS NOTED OTHERWISE.
- UNDERGROUND UTILITIES SHALL BE LOCATED PRIOR TO INSTALLATION OF LANDSCAPE AND IRRIGATION.
- SIGHT DISTANCE EASEMENTS RESTRICTS PERMANENT OBJECTS TALLER THAN 24' ABOVE THE PAVEMENT, WITHIN THE EASEMENT. STREET TREES SHALL BE PERMITTED AS REQUIRED BY SITE DEVELOPMENT STANDARDS AND SHALL BE 'LIMBED UP' WITH NO BRANCHES EXTENDING BELOW 7' ABOVE PAVEMENT.
- STEEL EDGERS SHALL BE PROVIDED AROUND ALL PLANTING BEDS ADJACENT TO TURF AREAS. STEEL EDGERS SHALL BE GALVANIZED, 1/8" X 4" DEEP.
- GRAPHIC SYMBOLS PRESIDE OVER WRITTEN PLANT QUANTITIES.
- THIS LANDSCAPE PLAN INCLUDES (NOT SHOWN ON PLANS) A TEMPORARY IRRIGATION SYSTEM FOR THE AREA IN THE REGIONAL DETENTION POND TRACT C. THIS SYSTEM SHALL REMAIN UNTIL ALL VEGETATION IS HEALTHY AND GRASSES HAVE GERMINATED.
- DEVELOPER SHALL ENSURE THAT THE LANDSCAPE PLAN IS COORDINATED WITH THE PLANS DONE BY OTHER CONSULTANTS SO THE PROPOSED GRADING, STORM DRAINAGE, OR OTHER CONSTRUCTION DOES NOT CONFLICT NOR PRECLUDE INSTALLATION AND MAINTENANCE OF LANDSCAPE ELEMENTS ON THIS PLAN. CHANGES IN THE LANDSCAPE PLACEMENT MAY BE ALLOWED DUE TO CHANGES IN SITE CONSTRUCTION.
- DECIDUOUS TREES SHALL BE PLANTED A MINIMUM OF 8' FROM THE EDGE OF THE RECREATIONAL TRAIL. EVERGREEN TREES SHALL BE PLANTED A MINIMUM OF 12' FROM THE EDGE OF THE RECREATIONAL TRAIL.

### BUFFERYARD PLANT LIST

(See Bufferyard Map for locations)

Bufferyard	Linear Feet x Depth	Bufferyard Requirement	
Bufferyard D-1	Linear Feet x Depth 1200' x 55'	Canopy Trees 24 Flowering Trees or Large Shrubs 24 Shrubs 150 Evergreens/Conifers 18	
Bufferyard D-2	Linear Feet x Depth 400' x 45'	Canopy Trees 8 Flowering Trees or Large Shrubs 8 Shrubs 50 Evergreens/Conifers 6	
Bufferyard D-3	Linear Feet x Depth 550' x 45'	Canopy Trees 11 Flowering Trees or Large Shrubs 11 Shrubs 69 Evergreens/Conifers 9	
Bufferyard D-4	Linear Feet x Depth 1500' x 55'	Canopy Trees 30 Flowering Trees or Large Shrubs 30 Shrubs 188 Evergreens/Conifers 23	
Bufferyard D-5	Linear Feet x Depth 1500' x 55'	Canopy Trees 30 Flowering Trees or Large Shrubs 30 Shrubs 188 Evergreens/Conifers 23	
Bufferyard D-6	Linear Feet x Depth 675' x 50'	Canopy Trees 14 Flowering Trees or Large Shrubs 14 Shrubs 85 Evergreens/Conifers 11	
Bufferyard D-7	Linear Feet x Depth 600' x 50'	Canopy Trees 12 Flowering Trees or Large Shrubs 12 Shrubs 75 Evergreens/Conifers 9	
Bufferyard D-8	Linear Feet x Depth 425' x 50'	Canopy Trees 9 Flowering Trees or Large Shrubs 9 Shrubs 54 Evergreens/Conifers 7	
Bufferyard D-9	Linear Feet x Depth 500' x 50'	Canopy Trees 10 Flowering Trees or Large Shrubs 10 Shrubs 63 Evergreens/Conifers 8	
Bufferyard D-10	Linear Feet x Depth 1400' x 50'	Canopy Trees 28 Flowering Trees or Large Shrubs 28 Shrubs 175 Evergreens/Conifers 21	
Bufferyard D-11	Linear Feet x Depth 150' x 50'	Canopy Trees 3 Flowering Trees or Large Shrubs 3 Shrubs 19 Evergreens/Conifers 3	
Bufferyard D-12	Linear Feet x Depth 200' x 40'	Canopy Trees 5 Flowering Trees or Large Shrubs 5 Shrubs 30 Evergreens/Conifers 4	
Bufferyard D-13	Linear Feet x Depth 400' x 50'	Canopy Trees 8 Flowering Trees or Large Shrubs 8 Shrubs 50 Evergreens/Conifers 6	
Bufferyard D-14	Linear Feet x Depth 200' x 40'	Canopy Trees 5 Flowering Trees or Large Shrubs 5 Shrubs 30 Evergreens/Conifers 4	

### PLANT SELECTION LIST

MRK	QTY	COMMON NAME	LATIN NAME	PLANTING SIZE
Large Shade Trees				
APA	39	Autumn Purple Ash	Fraxinus americana 'Autumn Purple'	2' CAL
MA	36	Marshall Seedless Ash	Fraxinus pennsylvanica lanceolata 'Marshall'	2' CAL
PA	2	Palmore Ash	Fraxinus pennsylvanica lanceolata 'Palmore'	2' CAL
SA	29	Summit Ash	Fraxinus pennsylvanica lanceolata 'Summit'	2' CAL
NM	27	Emerald Queen Norway Maple	Acer platanoides 'Emerald Queen'	2' CAL
ABM	5	Autumn Blaze Maple	Acer x freemanii 'Jeffersred'	2' CAL
EO	37	English Oak	Quercus robur	2' CAL
SWO	1	Swamp White Oak	Quercus bicolor	2' CAL
GSL	6	Greenspire Linden	Tilia cordata 'Greenspire'	2' CAL
NLC	3	Narrowleaf Cottonwood	Populus angustifolia	2' CAL
WP	2	White Poplar	Populus alba	2' CAL
SMH	27	Shademaster Honeylocust	Gleditsia tri inermis 'Shademaster'	2' CAL
CT	303	CANOPY TREE SELECTED BY LOT OWNER - SEE GENERAL NOTE #1		
Ornamental Trees				
NP	12	Newport Plum	Prunus speciosa	1 1/2' CAL
QA	22	Quaking Aspen	Populus tremuloides	1 1/2' CAL
RC	10	Radiant Crabapple	Malus 'Radiant'	1 1/2' CAL
SSC	8	Spring Snow Crabapple	Malus 'Spring Snow'	1 1/2' CAL
TA	8	Thineaf Alder	Alnus tenuifolia	1 1/2' CAL
AM	12	Amur Maple	Acer ginnala	1 1/2' CAL
ABP	8	Autumn Blaze Pear	Pyrus calleryana 'Autumn Blaze'	1 1/2' CAL
RMM	9	Rocky Mountain Maple	Acer glabrum	6" H (multi-trunk)
Coniferous / Evergreen Trees				
AP	30	Austrian Pine	Pinus nigra	6H
PP	34	Ponderosa Pine	Pinus ponderosa	6H
RMJ	32	Rocky Mountain Juniper	Juniperus scopulorum	6H
CS	51	Colorado Spruce	Picea pungens	6H
High Deciduous and Coniferous Shrubs (4'-15' in height)				
CL	4	Chinese Lilac	Syringa vulgaris	5 GAL
SB	9	Serviceberry	Amelanchier	5 GAL
F	10	Forsythia	Forsythia x intermedia	5 GAL
VD	12	Variiegated Dogwood	Cornus alba argentea-marginala	5 GAL
CBB	6	Compact Burning Bush	Euonymus alata	5 GAL
CB	28	Black Chokeberry	Aronia melanocarpa elata	5 GAL
GCJ	39	Gray Gleam Juniper	Juniperus scopulorum	5 GAL
GDP	27	Gold Drop Potentilla	Potentilla fruticosa farreri 'Gold Drop'	5 GAL
Medium Deciduous and Coniferous Shrubs (18'-4' in height)				
SMS		Snowmound Spirea	Spirea snowmound	5 GAL
DEC		Dwarf European Cranberry	Viburnum opulus	5 GAL
DRB		Dwarf Redleaf Barberry	Berberis thunbergii	5 GAL
RMS		Rocky Mountain Sumac	Shus glabra var. cismontana	5 GAL
DKL		Dwarf Korean Lilac	Syringa meyeri	5 GAL
TJ		Tammy Juniper	Juniperus sabina 'Tamariscifolia'	5 GAL
APC		Alpine Currant	Ribes alpinum	5 GAL
CE		Coloneaster, Cranberry	Coloneaster apiculatus	5 GAL
Low Deciduous and Coniferous Shrubs (up to 18' in height)				
BSJ		Blue Star Juniper	Juniperus squamata 'Blue Star'	5 GAL
BMJ		Broadmoor Juniper	Juniperus	5 GAL
PWC		Purpleleaf Wintercreeper	Euonymus fortunei 'Coloratus'	5 GAL
CCF		Creeping Cinquefoil	Potentilla neumaniana	5 GAL
VC		Virginia Creeper	Parthenocissus quinquefolia	5 GAL
Wetland Replacement Species				
CC		Native Chokeberry	Prunus virginiana melanocarpa	5 GAL
CW		Cyote Willow	Salix exigua	10-12' rooted cutting
PLW		Peachleaf Willow	Salix amygdaloides	5 GAL
TRB		Tall Blue Rabbilbrush	Chrysothamnus sp. albicaulis	5 GAL
PC		Plains Cottonwood	Populus targentii	5 GAL
ACR		Austrian Copper Rose	Rosa foetida 'bicolor'	5 GAL
RLR		Red Leaved Rose	Rosa glauca	5 GAL
RM		Rocky Mountain Iris	Iris missouriensis	5 GAL
CCH		Creeping Colorado Holly	Mahonia repens	5 GAL

### SEED MIXES

#### Drought Tolerant Turf Seed Mix

Common Name	% of Total
Lincoln Smooth Brome	40%
Alta Tall Fescue	60%
Seeding Rate:	
Irrigated: 4 lbs./ 1000 SF.	

#### Foothills Native/ Wetland Seed Mix

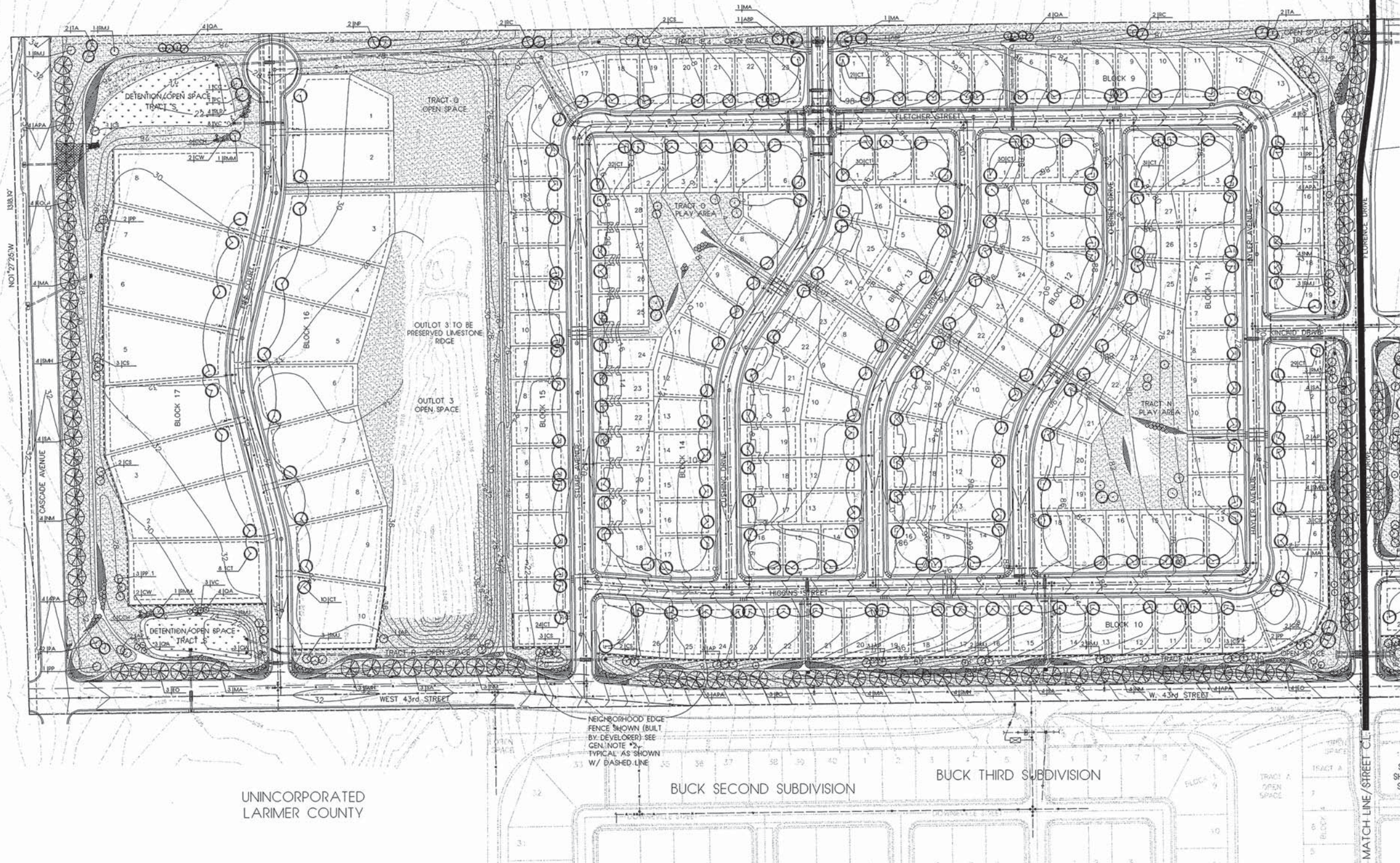
Common Name	% of Total
Crested Wheatgrass	25%
Mountain Brome	20%
Hard Fescue	15%
Annual Ryegrass	15%
Kentucky Bluegrass	10%
Sideoats Grama	5%
Lille Bluestem	5%
Blue Grama	4%
Sand Dropseed	1%



UNINCORPORATED  
LARIMER COUNTY

UNINCORPORATED  
LARIMER COUNTY

UNINCORPORATED  
LARIMER COUNTY



LANDSCAPE LEGEND

- PROPOSED DECIDUOUS CANOPY TREES
- PROPOSED DECIDUOUS STREET TREES
- PROPOSED CONIFEROUS/EVERGREEN TREES
- PROPOSED ORNAMENTAL TREES
- PROPOSED HIGH DECIDUOUS & CONIFEROUS SHRUBS
- PROPOSED SHRUB BEDS (to contain a mix of medium and low deciduous & coniferous shrubs)
- BRICATED SOO
- BRICATED TURF BED MIX
- NONBRICATED NATIVE/ WETLAND BED MIX
- AREA OF NONBRICATED RE-ESTABLISHED WETLANDS



LANDSCAPE PLAN  
1 LS2  
1" = 100'-0"



FINAL DEVELOPMENT PLAN

GIULIANO FIRST SUBDIVISION

FINAL DEVELOPMENT PLAN  
LOVELAND, COLORADO

GIULIANO & FATHER  
CONSTRUCTION CO., INC.

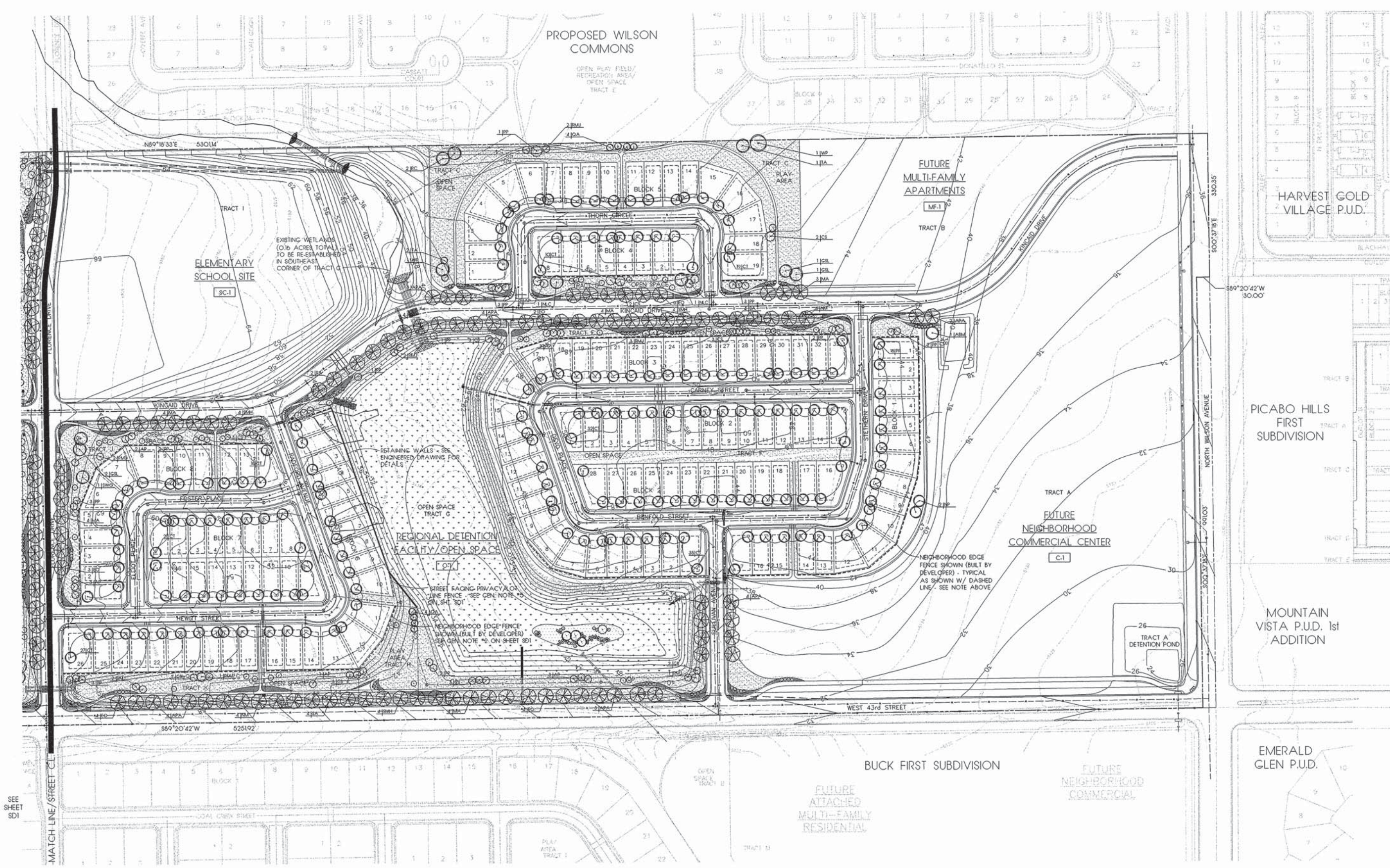
issue date  
3/1/02

REVISIONS	
1	5/1/02
2	11/1/02
3	1/15/03

sheet title  
LANDSCAPE  
PLAN

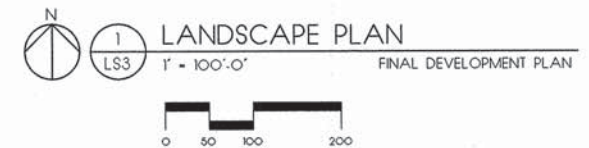
sheet NO.





SEE SHEET SD1

- LANDSCAPE LEGEND**
- PROPOSED DECIDUOUS CANOPY TREES
  - PROPOSED DECIDUOUS STREET TREES
  - PROPOSED CONIFEROUS/EVERGREEN TREES
  - PROPOSED ORNAMENTAL TREES
  - PROPOSED HIGH DECIDUOUS & CONIFEROUS SHRUBS
  - PROPOSED SHRUB BEDS (to contain a mix of medium and low deciduous & coniferous shrubs)
  - IRRIGATED SOO
  - IRRIGATED TURF REED MIX
  - NONIRRIGATED NATIVE/ WETLAND REED MIX
  - AREA OF NONIRRIGATED REESTABLISHED WETLANDS



**PC ATTACHMENT 6**

**GIULIANO & FATHER**  
CONSTRUCTION CO., INC.

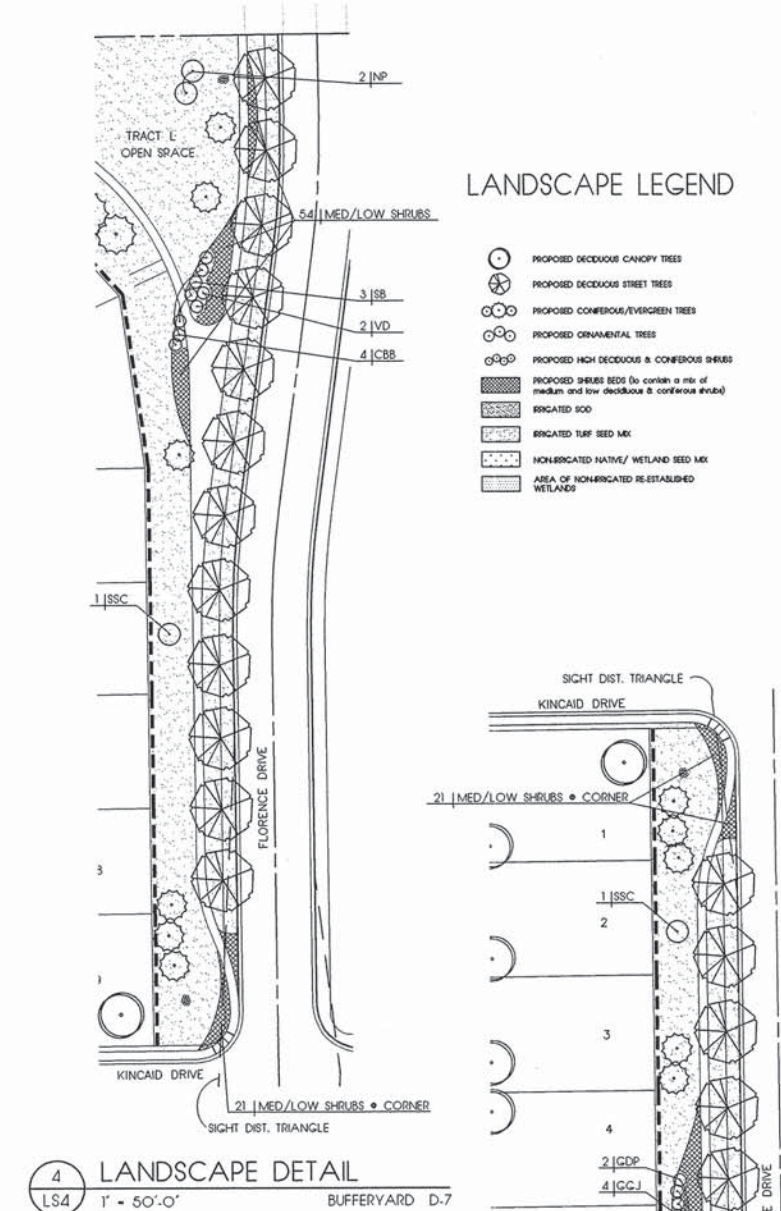
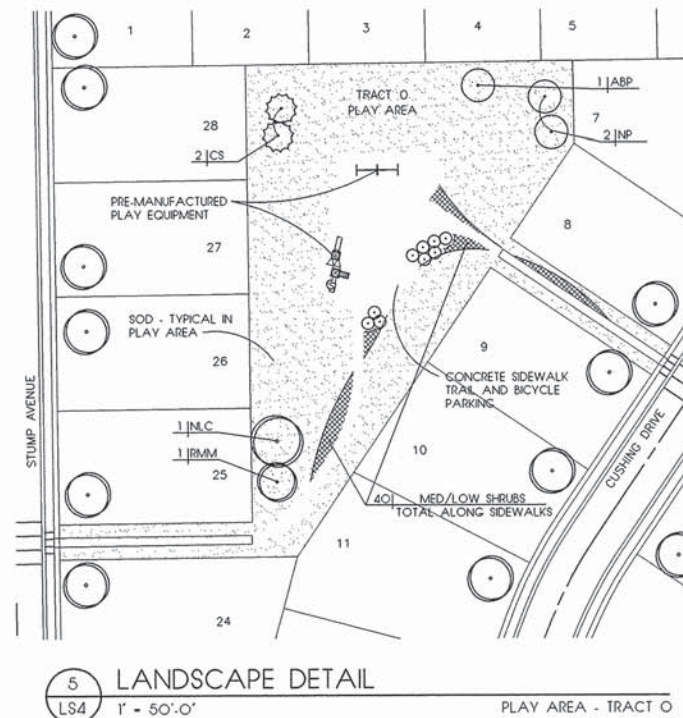
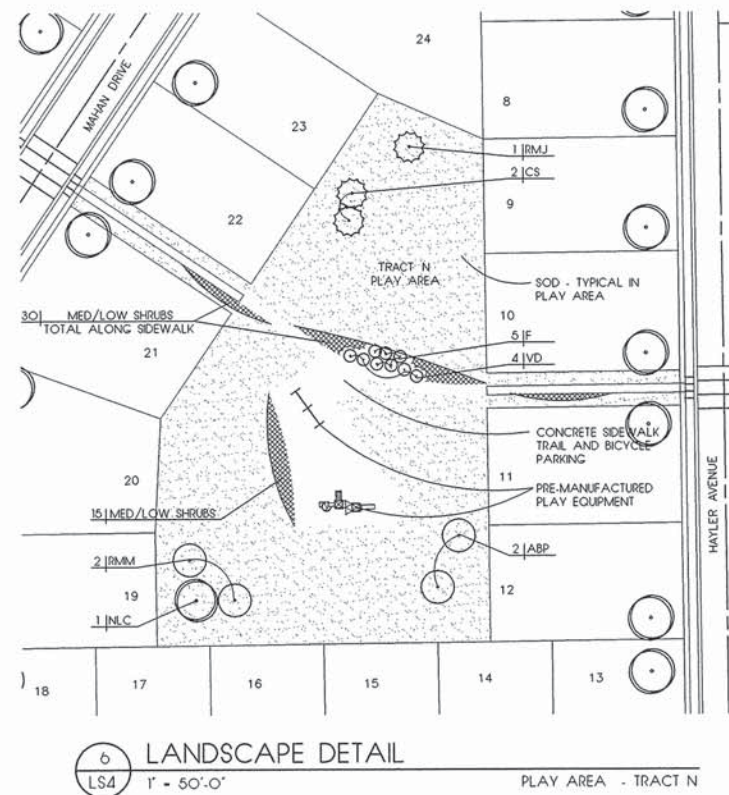
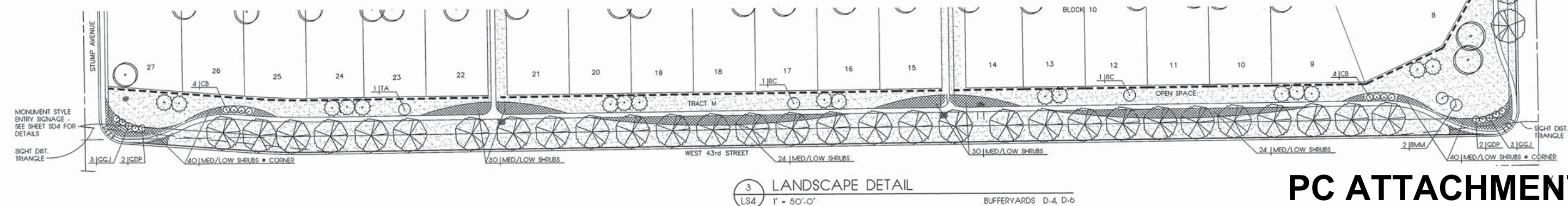
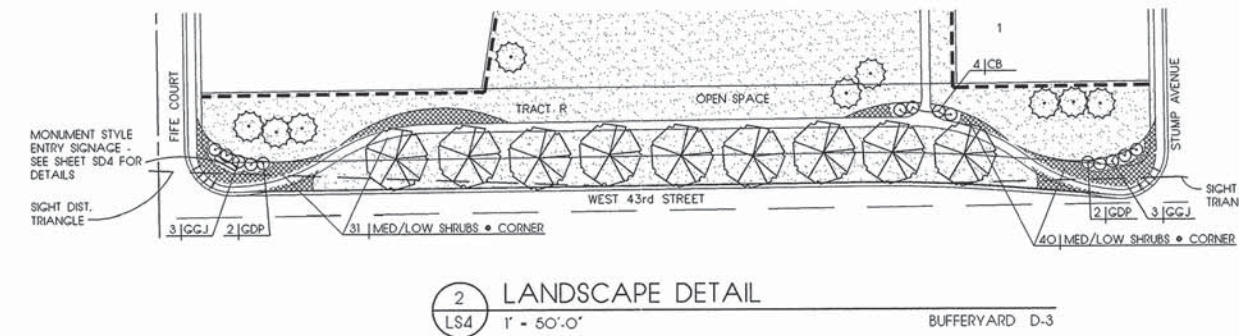
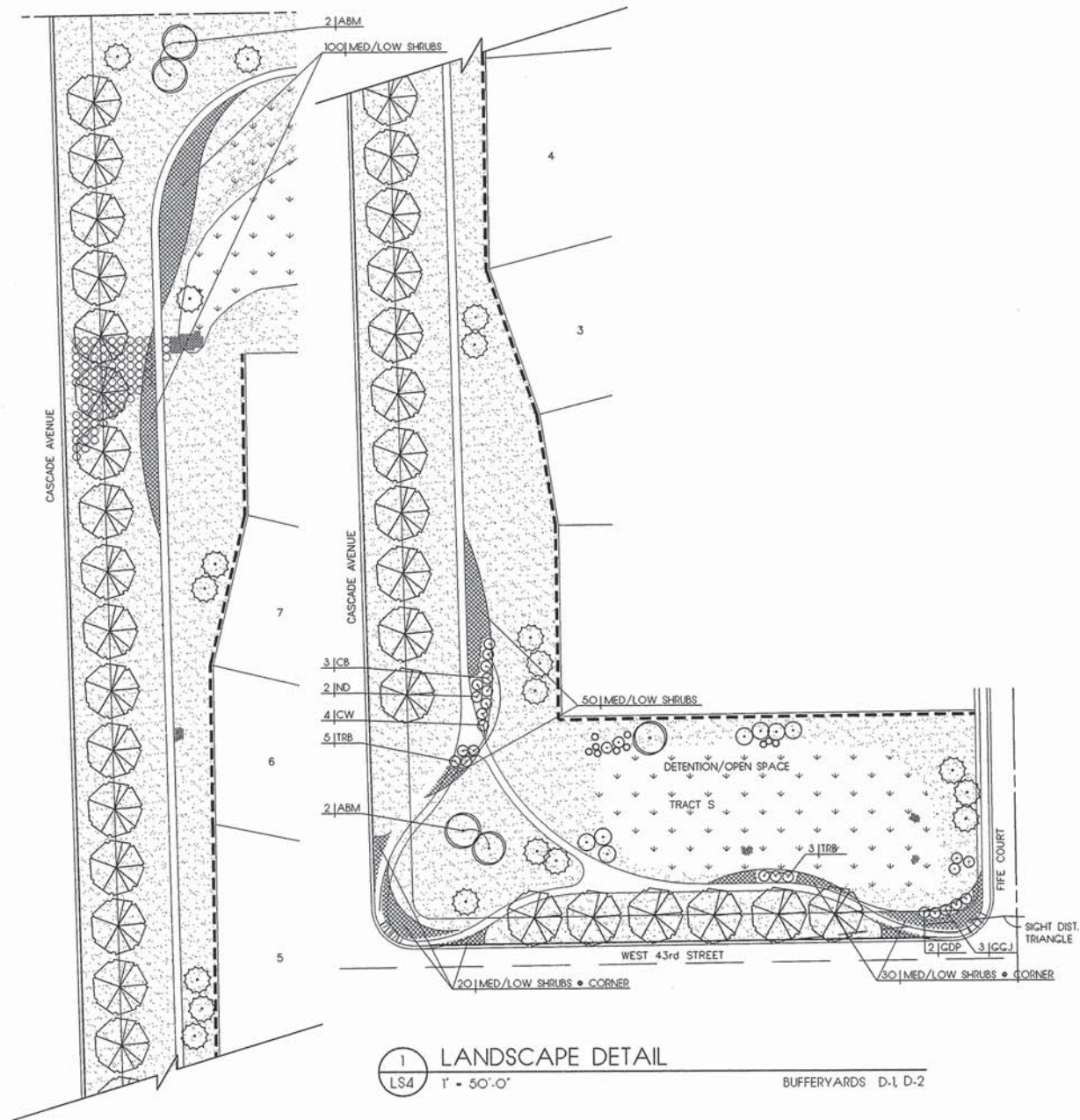
**GIULIANO FIRST SUBDIVISION**  
FINAL DEVELOPMENT PLAN  
LOVELAND, COLORADO

issue date  
3/1/02

REVISIONS	
1	5/1/02
2	11/1/02
3	1/15/03

sheet title  
LANDSCAPE PLAN  
LS3  
sheet NO.





# LANDSCAPE LEGEND

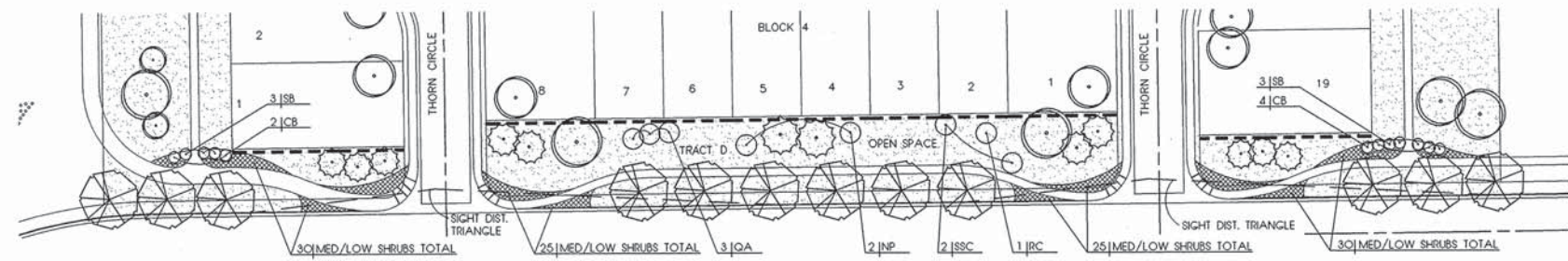
- PROPOSED DECIDUOUS CANOPY TREES
- PROPOSED DECIDUOUS STREET TREES
- PROPOSED CONIFEROUS/EVERGREEN TREES
- PROPOSED ORNAMENTAL TREES
- PROPOSED HIGH DECIDUOUS & CONIFEROUS SHRUBS
- PROPOSED SHRUB BEDS (to contain a mix of medium and low deciduous & coniferous shrubs)
- IRRIGATED SOO
- IRRIGATED TURF SEED MIX
- NON-IRRIGATED NATIVE/ WETLAND SEED MIX
- AREA OF NON-IRRIGATED RE-ESTABLISHED WETLANDS

1	5/1/02
2	11/1/02
3	1/15/03

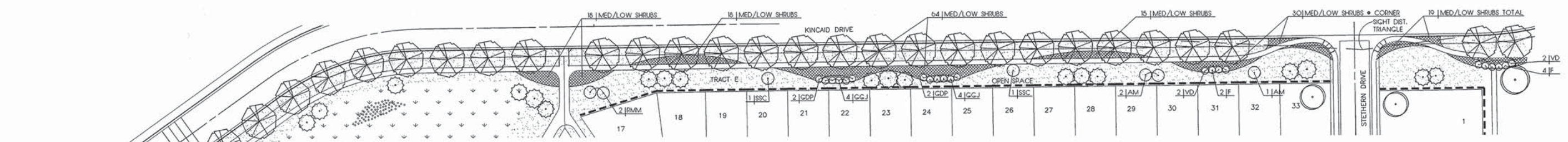


LANDSCAPE LEGEND

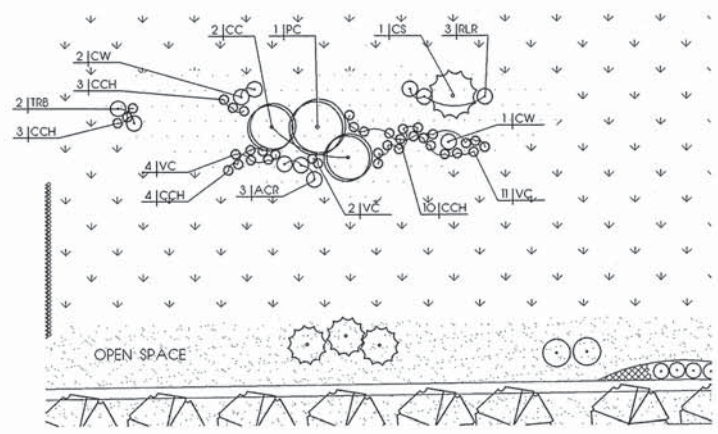
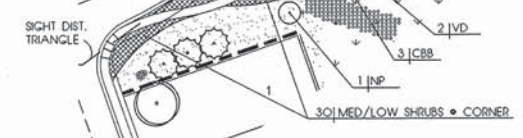
- PROPOSED DECIDUOUS CANOPY TREES
- PROPOSED DECIDUOUS STREET TREES
- PROPOSED CONIFEROUS/EVERGREEN TREES
- PROPOSED ORNAMENTAL TREES
- PROPOSED HIGH DECIDUOUS & CONIFEROUS (SHRUBS)
- PROPOSED SHRUB BEDS (to contain a mix of medium and low deciduous & coniferous shrubs)
- IRRIGATED SOO
- IRRIGATED TURF SEED MIX
- NONIRRIGATED NATIVE/ WETLAND SEED MIX
- AREA OF NONIRRIGATED RE-ESTABLISHED WETLANDS



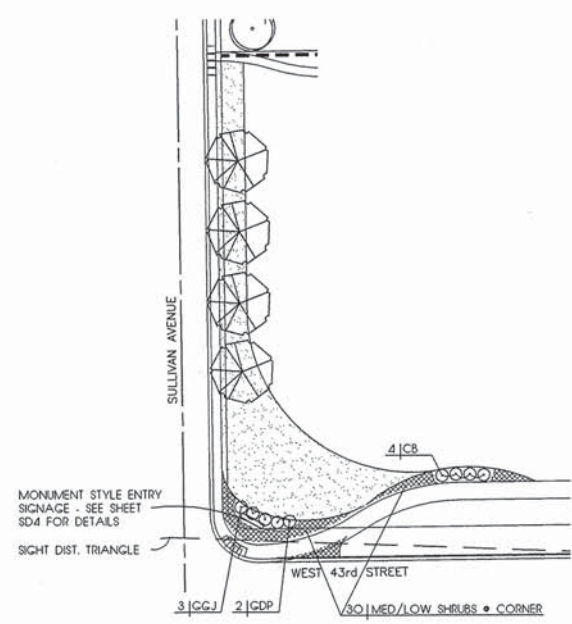
5 LANDSCAPE DETAIL  
1" = 50'-0" BUFFERYARDS D-12, D-13, D-14



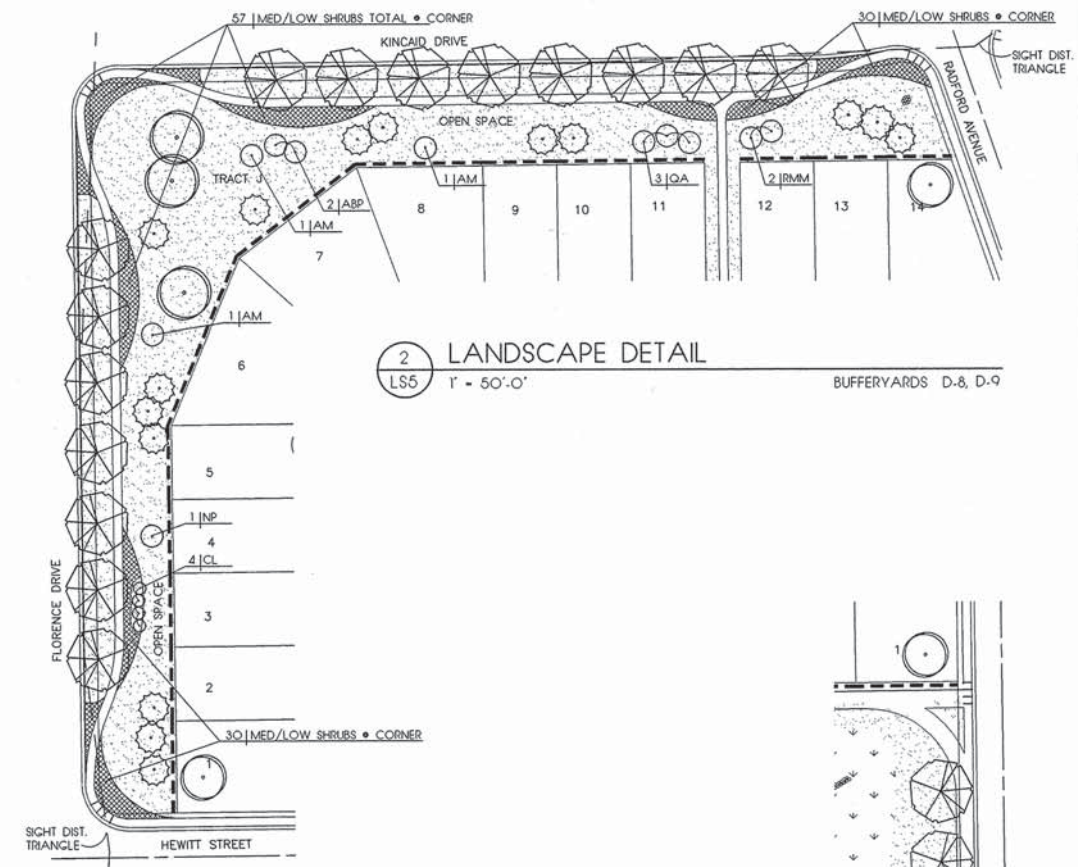
4 LANDSCAPE DETAIL  
1" = 50'-0" BUFFERYARD D-10



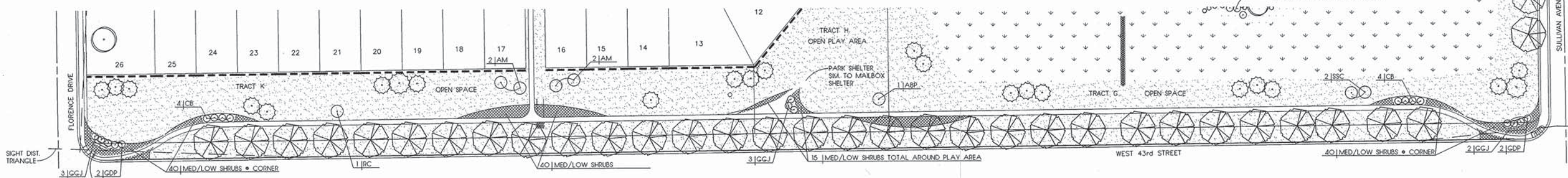
6 LANDSCAPE DETAIL  
1" = 40'-0" RE-ESTABLISHED WETLANDS



3 LANDSCAPE DETAIL  
1" = 50'-0" EAST SIDE OF SULLIVAN AVE. ENTRANCE



2 LANDSCAPE DETAIL  
1" = 50'-0" BUFFERYARDS D-8, D-9



1 LANDSCAPE DETAIL  
1" = 50'-0" BUFFERYARD D-5

REVISIONS	
1	5/1/02
2	11/1/02
3	1/15/03



# GIULIANO FOURTH SUBDIVISION

## LOVELAND, COLORADO

### PRELIMINARY DEVELOPMENT PLAN

#### LEGAL DESCRIPTION

BEING A SUBDIVISION OF BLOCKS 16 & 17, TRACTS S & U, AND OUTLOT 1 OF GIULIANO FIRST SUBDIVISION, AS RECORDED AT RECEPTION NO. 2004008797, A SITUATE IN THE SOUTH 1/2 OS SECTION 33, TOWNSHIP 6 NORTH, RANGE 69 WEST OF THE SIXTH P.M., CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO.

APPROVAL OF THIS PLAN CREATES A VESTED PROPERTY RIGHT PURSUANT TO SECTION 24-68-103, C.R.S., AS AMENDED.

#### OWNER'S CERTIFICATION

KNOW ALL MEN BY THESE PRESENTS THAT:

Being all the lawful record owners of the property shown on this Final Development Plan, except any existing public streets, roads, or highways, do hereby certify that I/we accept the conditions and restrictions set forth on said plan and in the conditions of approval by the City of Loveland, dated \_\_\_\_\_, and that I/we consent to the recordation of any information pertaining thereto.

Giuliano & Father Construction Inc.  
(owner)

(John G. Giuliano, President)  
STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 2015, by \_\_\_\_\_.

Witness by hand and official seal.

My commission expires: \_\_\_\_\_

Notary Public

(John T. Giuliano, Owner)

STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 2015, by \_\_\_\_\_.

Witness by hand and official seal.

My commission expires: \_\_\_\_\_

Notary Public

J & P Family, LLLP

(John G. Giuliano, Member)  
STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 2015, by \_\_\_\_\_.

Witness by hand and official seal.

My commission expires: \_\_\_\_\_

Notary Public

Giuliano Addition, LLLP

(John G. Giuliano, Member)  
STATE OF COLORADO ) ss.  
COUNTY OF LARIMER )

The foregoing agreement was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 2015, by \_\_\_\_\_.

Witness by hand and official seal.

My commission expires: \_\_\_\_\_

Notary Public

#### OWNER/APPLICANT

Giuliano & Father Construction, Inc.  
John G. Giuliano  
Contact: John Giuliano  
308 Commerce Drive, Unit A  
Fort Collins, Colorado 80524  
(970)593-1212 phone/fax

#### CIVIL ENGINEER

Stewart & Associates  
Contact: Jerry Robinson  
103 S. Meldrum  
Fort Collins, Colorado 80521  
(970)482-9331  
(970)482-9382 fax

#### DRAWING INDEX

SD1 SITE PLAN  
SD2 FENCE & MONUMENT DETAILS & TYPICAL STREET SECTIONS  
SD3 PROJECT NARRATIVE  
SD4 GDP & PDP CONDITIONS OF APPROVAL  
LS1 LANDSCAPE PLAN, NOTES, PLANT LIST & BUFFERYARD MAP

#### OVERALL LAND USE SUMMARY

Existing Zoning:	P.U.D. Planned Unit Development (Giuliano Addition P.U.D.)
Total Project Area:	159.6 acres (100%)
Total R.O.W. Area:	33.8 acres (21%)
Net Area:	125.8 acres (79%)
Open Space Area:	* 38.95 acres (24%)
Total Number of Residential Lots:	374

\* Areas to have future PDPs are not included in open space area calculations.

#### SIGNATURE BLOCKS

Approved this \_\_\_\_ day of \_\_\_\_\_, 2015 by the Current Planning Manager of the City of Loveland, Colorado.

Current Planning Manager

Approved this \_\_\_\_ day of \_\_\_\_\_, 2015 by the City Engineer of the City of Loveland, Colorado.

City Engineer

Approved this \_\_\_\_ day of \_\_\_\_\_, 2015 by the City Attorney of the City of Loveland, Colorado.

City Attorney

Approved this \_\_\_\_ day of \_\_\_\_\_, 2015 by the City Planning Commission of the City of Loveland, Colorado.

Chairperson

#### LAND USE SUMMARY BY AREA

		Percent of Area	Percent of Development
Area O-1 - Regional Detention Pond/Open Space			
Construction Phase	Phase 1		
Total Area:	8.98 acres		(6%)
Open Space Area:	8.98 acres	(100%)	

##### Area SC-1 - School Site

Construction Phase	Phase 1		
Total Area:	10.0 acres		(6%)
Open Space Area:	Undetermined		

##### Area MF-1 & C-1 - Multi-family and Commercial

Construction Phase	Future PDP Application		
Total Area:	22.9 acres		(14%)
Open Space Area:	Undetermined		

##### Area SF-1 - Detached Single-family Homes

Construction Phase	Phase 6		
Total Number of Lots:	36		
Total Area:	29.7 acres		(19%)
Total Lot Area:	7.1 acres	(24%)	
Total R.O.W. Area:	2.9 acres	(10%)	
Open Space Area:	19.7 acres	(66%)	
Gross Density:	1.21 DU/AC		
Net Density: (less R.O.W.)	1.34 DU/AC		

Lot Sizes:	
Minimum:	7,800 s.f.
Average:	8,400 s.f.
Maximum:	11,100 s.f.

##### Area SF-2 - Detached Single-family Homes

Construction Phase	Phases 3, 4 & 5		
Total Number of Lots:	176		
Total Area:	50.2 acres		(31%)
Total Lot Area:	31.08 acres	(62%)	
Total R.O.W. Area:	12.24 acres	(24%)	
Open Space Area:	6.88 acres	(14%)	
Gross Density:	3.50 DU/AC		
Net Density: (less R.O.W.)	4.63 DU/AC		

Lot Sizes:	
Minimum:	6,073 s.f.
Average:	7,693 s.f.
Maximum:	12,180 s.f.

##### Area SF-3 & SF-4 - Detached Single-family Homes

Construction Phase	Phases 1 & 2		
Total Number of Lots:	162		
Total Area:	37.82 acres		(24%)
Total Lot Area:	17.77 acres	(47%)	
Total R.O.W. Area:	13.16 acres	(35%)	
Open Space Area:	6.89 acres	(18%)	
Gross Density:	4.28 DU/AC		
Net Density: (less R.O.W.)	6.57 DU/AC		

Lot Sizes:	
Minimum:	4,186 s.f.
Average:	4,778 s.f.
Maximum:	7,319 s.f.

#### SETBACK REQUIREMENTS

The following minimum setbacks shall apply to Area SF-1. All setbacks shall be measured from the back of the sidewalk. All non-garage door setbacks are measured to the foundation wall.

##### Single-Family detached homes

Front: Staggered front yard setbacks will be required for all single family detached residential lots. (i.e., every house must be staggered a minimum 2 feet closer to, or further from, the right-of-way than the adjacent homes. See lot matrix.

Minimum of 20 feet to face of garage door.

Side: 1-foot for every 3-feet of building height, with a minimum of 5-feet.

Corner Side: 15 feet sideyard adjacent to street.  
Minimum of 20 feet to face of garage door.

Rear: 25 feet

##### Accessory Structures

Front: 15 feet or the front setback of the primary structure, whichever is greater.

Side: 6 feet

Corner Side: 15 feet

Rear: 6 feet

##### Encroachments

Overhangs, fireplaces and cantilevers, including structural elements may encroach into required setback by no more than 2 feet in width and 6 feet in length.

#### LOT MATRIX

##### Area SF-1

20 foot min. front yard

Block 16: Lots: 1, 4, 7, 10, 13, 16  
Block 17: Lots: 1, 4  
Block 18: Lots: 2  
Block 19: Lots: 2, 5  
Block 20: Lots: 2, 5

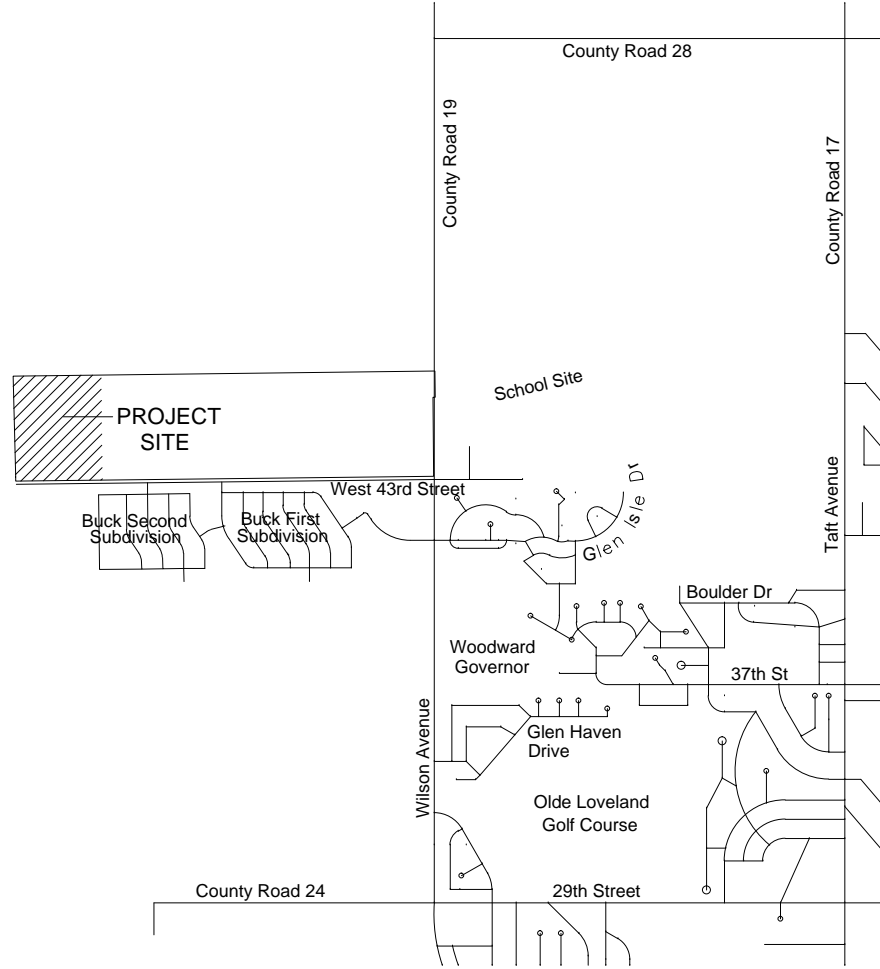
22 foot min. front yard

Block 16: Lots: 2, 5, 8, 11, 14  
Block 17: Lots: 2, 5  
Block 18: Lots: 3  
Block 19: Lots: 3, 6  
Block 20: Lots: 3

24 foot min. front yard

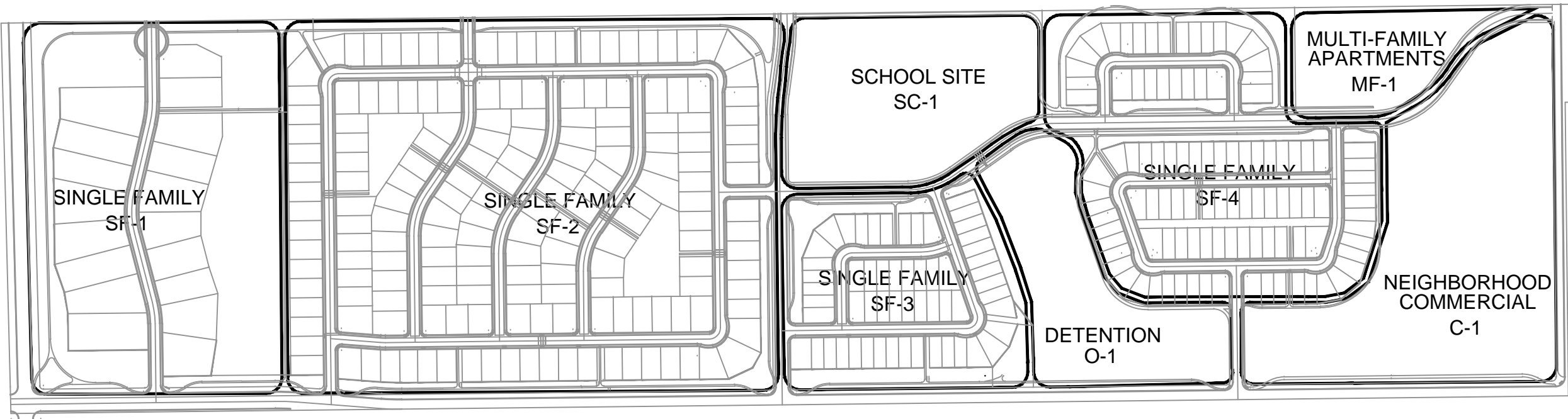
Block 16: Lots: 3, 6, 9, 12, 15  
Block 17: Lots: 3  
Block 18: Lots: 1, 4  
Block 19: Lots: 1, 4  
Block 20: Lots: 1, 4

#### VICINITY MAP

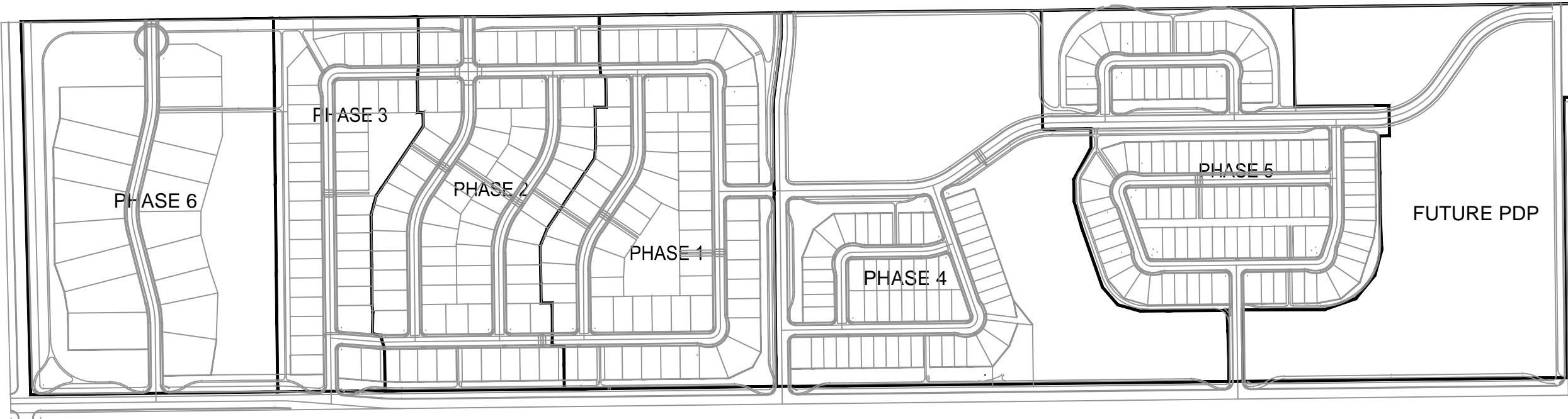


N  
CITY OF LOVELAND

#### GDP DESIGNATION MAP



#### PHASING MAP





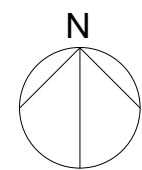
UNINCORPORATED  
LARIMER COUNTY

UNINCORPORATED  
LARIMER COUNTY

### GENERAL NOTES

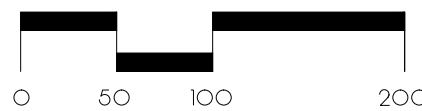
- THE RECREATION TRAIL AS SHOWN FROM FLORENCE DRIVE TO N. WILSON AVENUE IS TO BE CONSTRUCTED BY THE DEVELOPER AND IS TO BE MAINTAINED BY THE H.O.A. UNTIL THE CITY DETERMINES AND CONSTRUCTS THE PRELIMINARY CONNECTION/ALIGNMENT FROM THE WEST.  
THE COUNTY TRAIL WEST OF FLORENCE DRIVE IS TO BE MAINTAINED BY THE H.O.A. UNTIL THE COUNTY DETERMINES AND CONSTRUCTS THE PRELIMINARY CONNECTION/ALIGNMENT FROM THE WEST.
- THE LOCATIONS OF THE NEIGHBORHOOD EDGE FENCES ARE SHOWN GRAPHICALLY ON THE SITE PLAN. THE PRIVACY/LOT LINE FENCE (HOMEOWNER'S RESPONSIBILITY/OPTION) IS NOT SHOWN. SEE SHEET SD4 FOR FENCE DETAILS
- IT IS THE DESIGN INTENT, THAT THE EXISTING SLOPE OF THE LIMESTONE RIDGE BE INCORPORATED INTO EACH INDIVIDUAL LOT OF BLOCK 16. EXISTING SLOPES IN EXCESS OF 20% ARE TO BE UNDISTURBED.

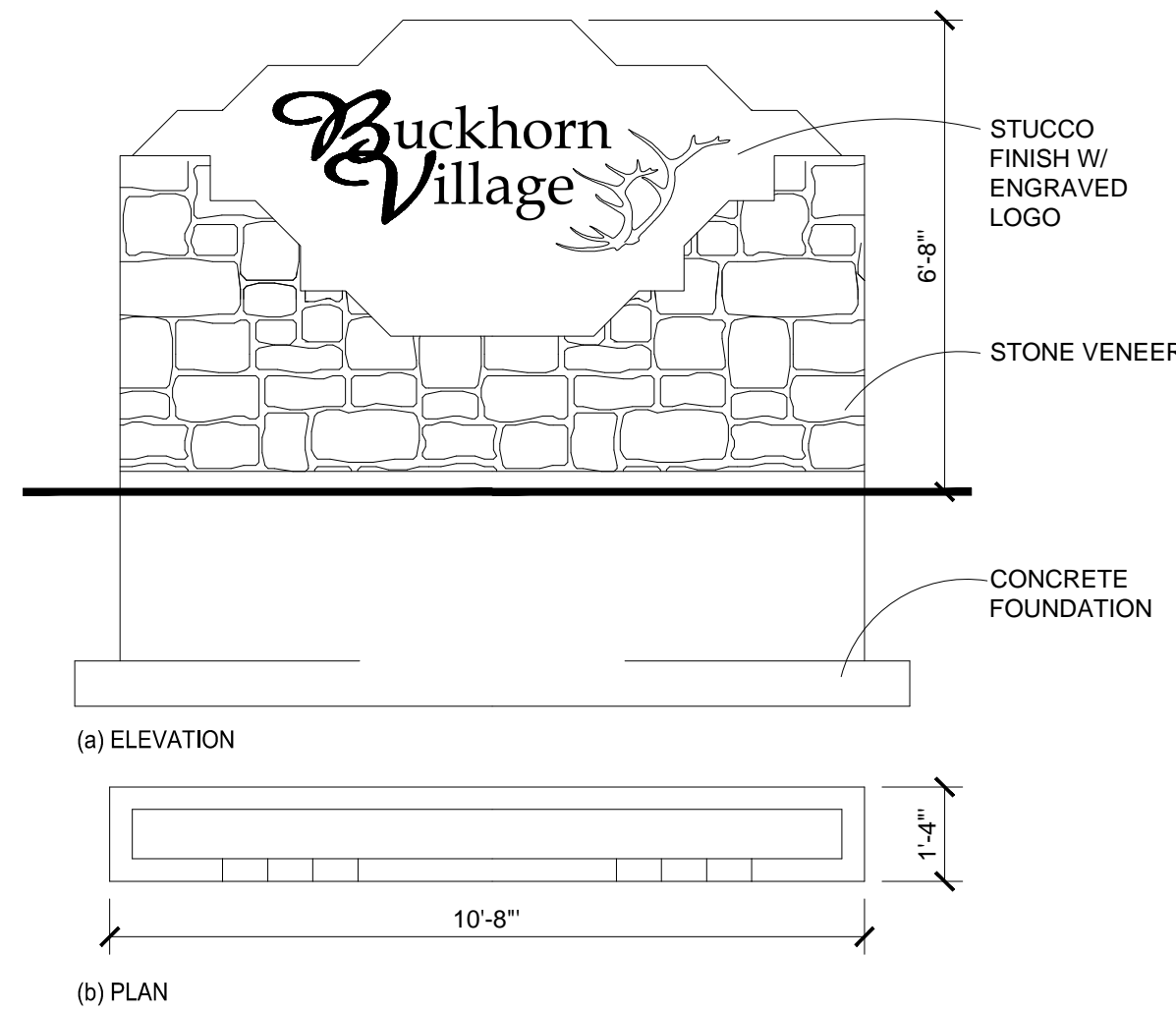
- 15'-0" WIDE PUBLIC TRAIL EASEMENT DEDICATED AND CONVEYED TO LARIMER COUNTY FOR BIKE, EQUESTRIAN AND PEDESTRIAN RECREATIONAL PURPOSES - SEE PLAT FOR RECEPTION NUMBER.
- DUE TO THE CONSTRUCTION OF THE RETAINING WALL IN THE REGIONAL POND THE DEVELOPER SHALL CONSTRUCT THE TYPICAL STREET FACING PRIVACY/LOT LINE FENCE ON TOP OF THE RETAINING WALL AND EXTENDING AROUND LOT 10, BLOCK 6 AS SHOWN ON THE PLAN - SEE SHT. SD4 FOR FENCE DETAILS.



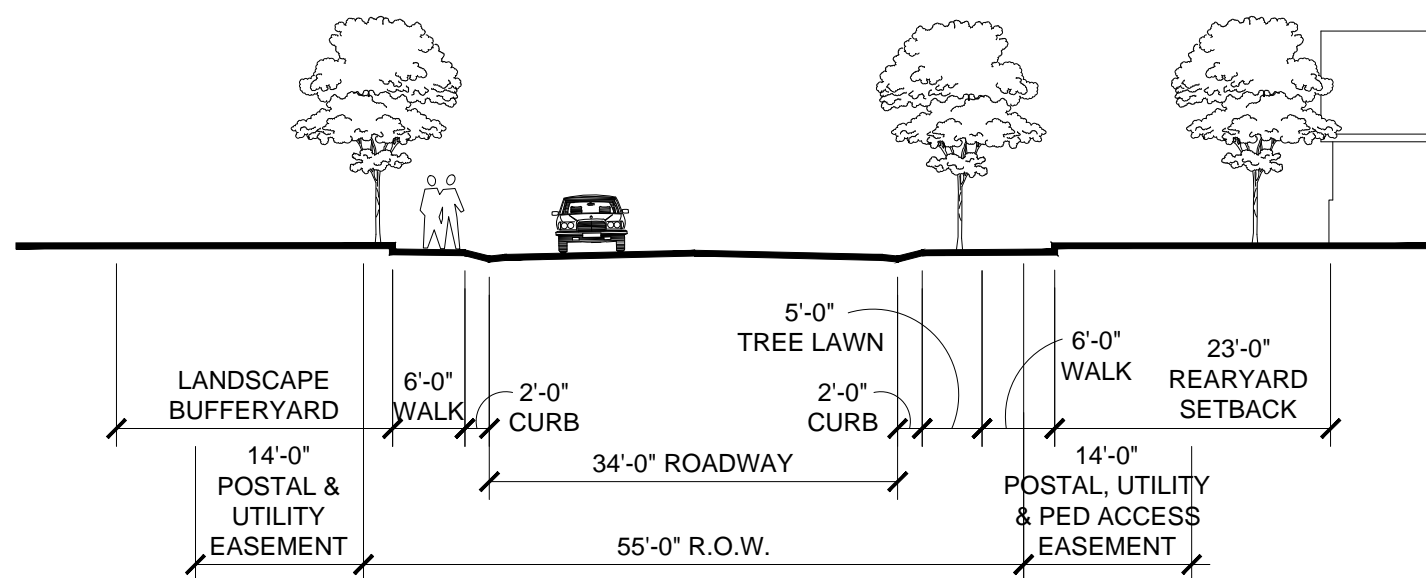
1  
SD1

SITE PLAN  
1" = 100'-0"

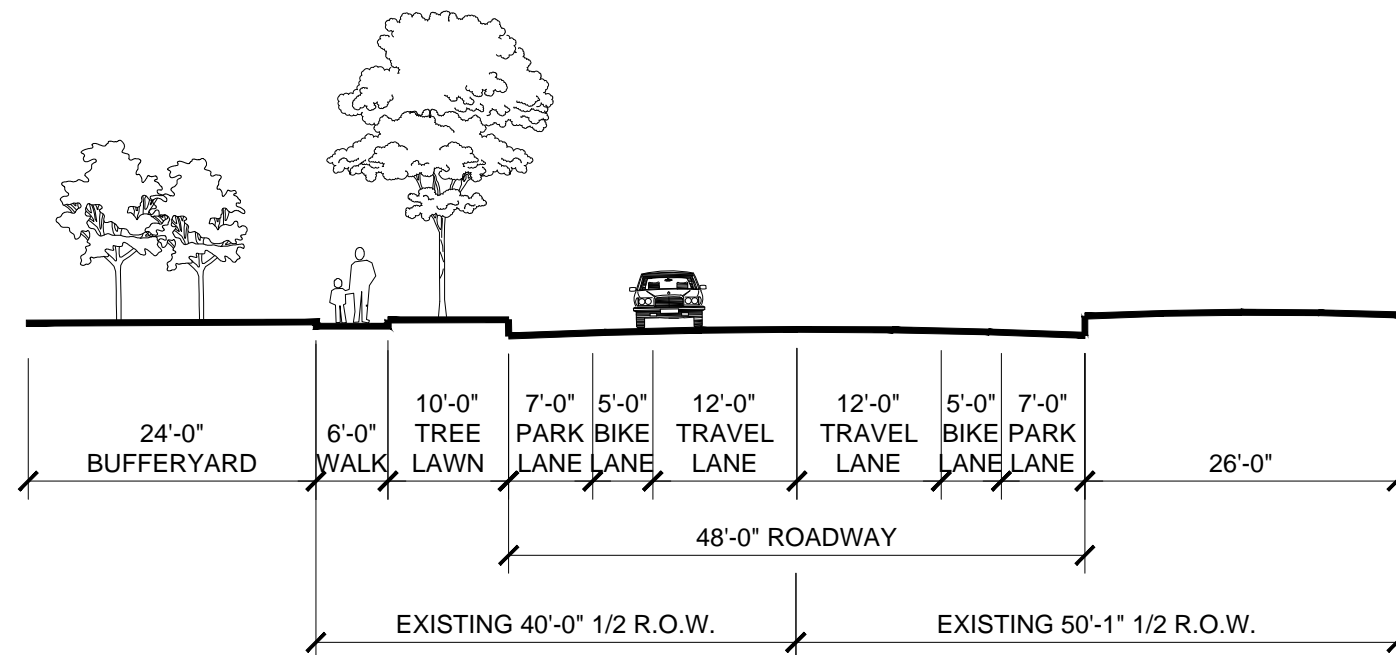




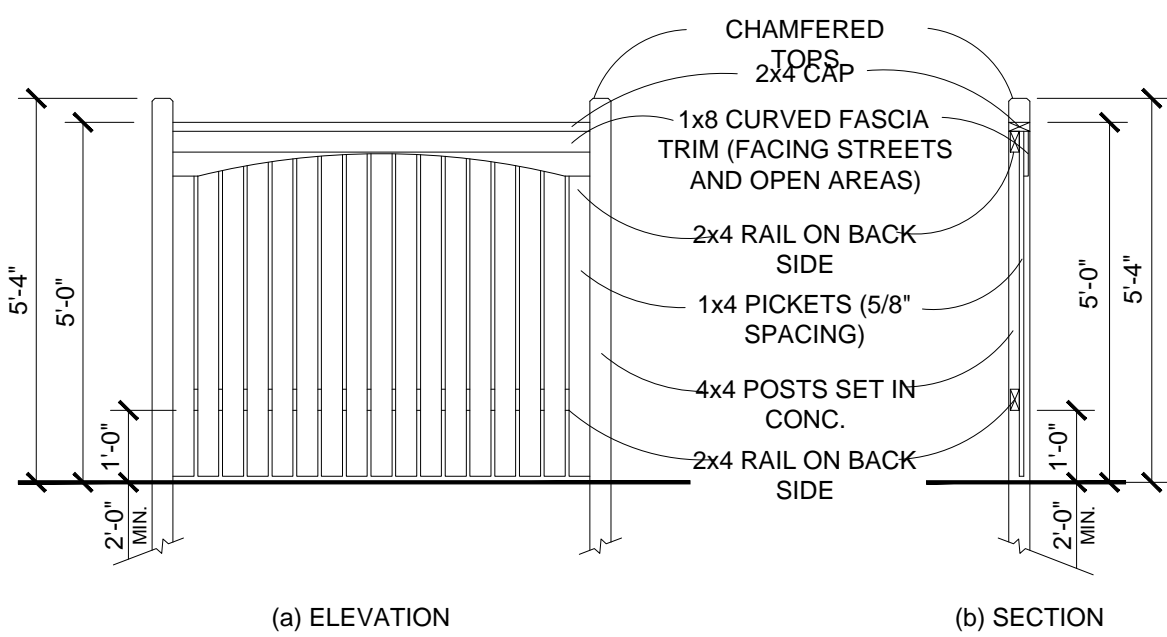
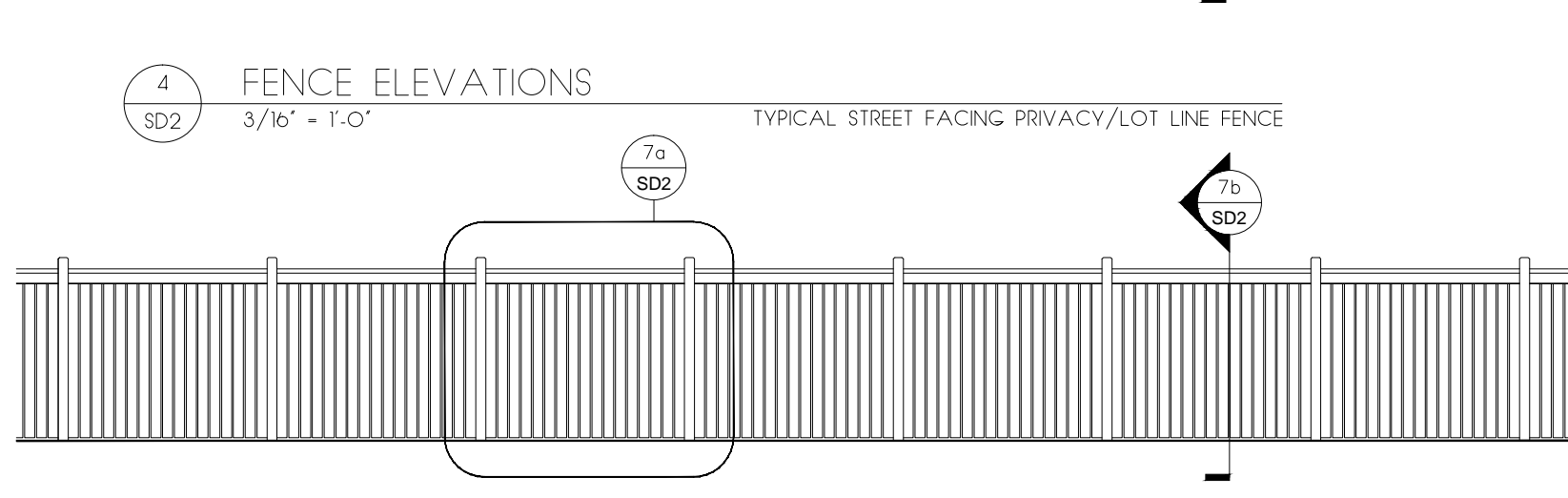
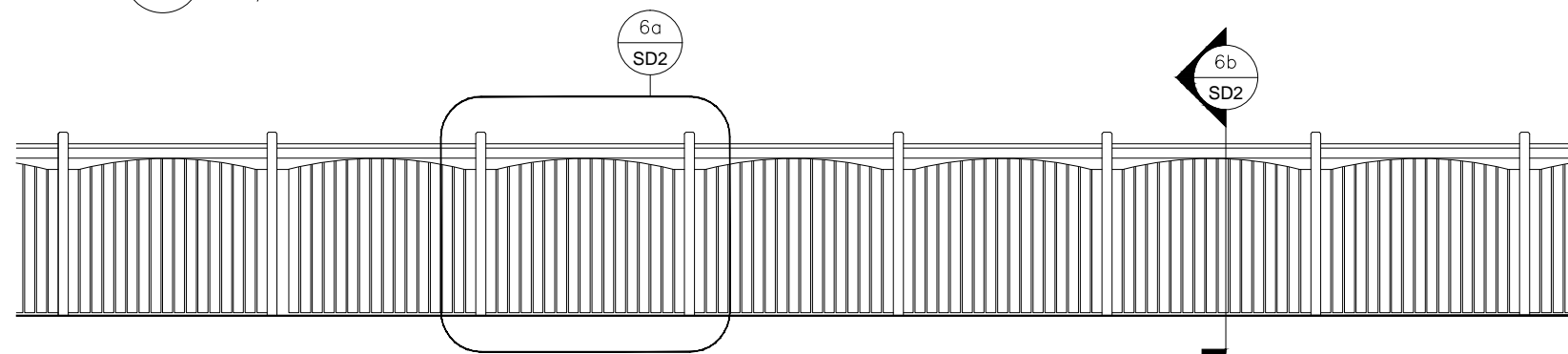
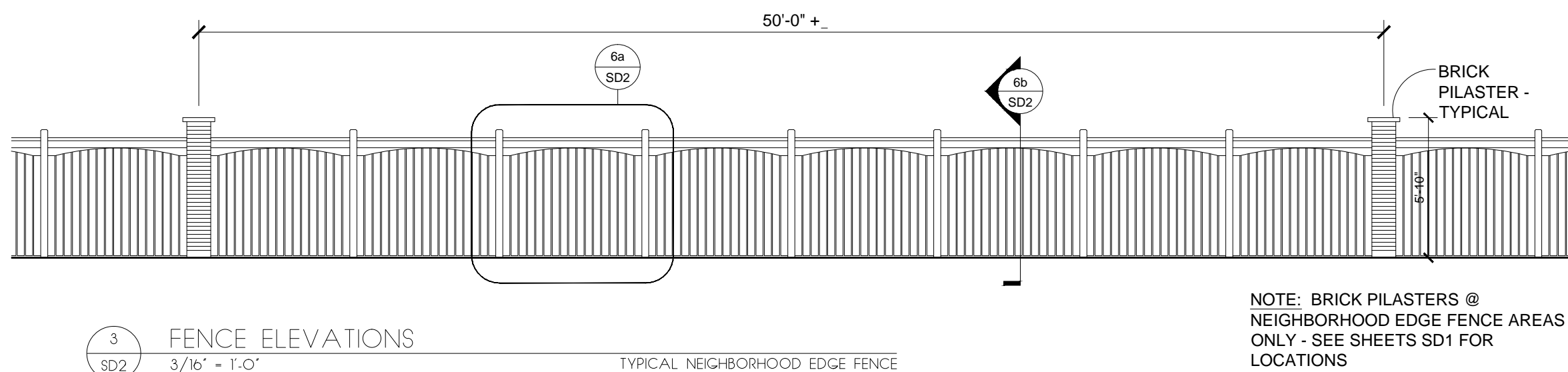
8  
SD2 MONUMENT DETAIL  
3/8" = 1'-0" TYPICAL MONUMENT STYLE ENTRY SIGNAGE



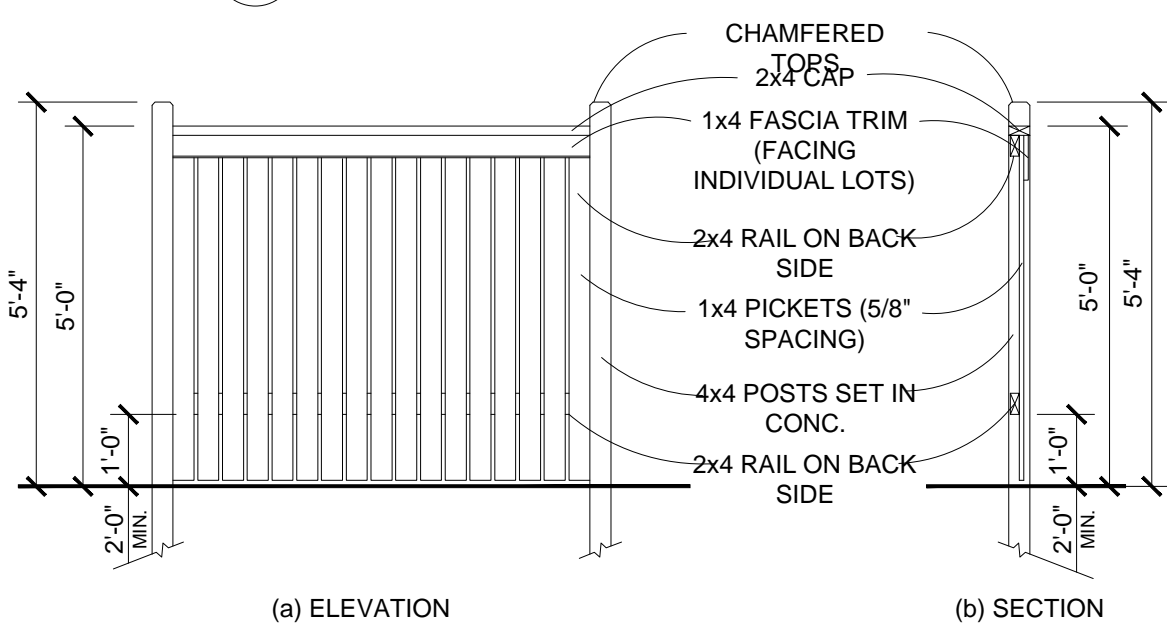
2  
SD2 STREET SECTION  
1/16" = 1'-0" TYPICAL FFE CT. LOCAL STREET



1  
SD2 STREET SECTION  
1/16" = 1'-0" TYPICAL FUTURE CASCADE AVENUE



6  
SD2 FENCE DETAILS  
3/8" = 1'-0"



7  
SD2 FENCE DETAILS  
3/8" = 1'-0"



PROJECT NARRATIVE

Overall Development Concept

Giuliano First Subdivision is the first Final Development Plan for the Giuliano Addition P.U.D., a 159.6-acre mixed-use planned unit development located in northwest Loveland recently annexed into the city with the approval of the General Development Plan and Annexation Map. The development concept is to provide a mix of residential and commercial uses within the same neighborhood, provide a variety of housing types from affordable housing to fine custom homes, and develop a coordinated and cohesive design approach for the overall development.

Affordable Housing Eligibility and Commitments

Giuliano & Father Construction, one of Loveland's larger entry-level homebuilders, has constructed hundreds of homes over the past fifteen years. In maintaining this tradition, the development of Giuliano Addition P.U.D. will be an opportunity for Giuliano & Father Construction to reexamine, improve and refresh their approach to building affordable homes, as well as utilize the new strategies that City Council and staff are implementing to reduce the time and cost of producing housing. Giuliano First Subdivision meets the city's definition of an affordable housing project as over 20% of the lots, a total of 72 out of 356 lots, will be sold to households with income at 70% or less than the Median Family Income (MFI) for Loveland, as determined by the U.S. Department of Housing and Urban Development.

Giuliano & Father Construction Inc., can construct and sell homes to affordable households through a combination of methods:

- 1) Creative Planning: By developing Giuliano Addition P.U.D. jointly with Buck Addition P.U.D. though they are separate projects, Giuliano & Father Construction is planning to develop a progressive community with housing encompassing multiple price levels. Although these two pieces of land were purchased at current market price with no raw water, because of their size, they can be developed to provide multiple land uses including affordable housing. When completed, this project will be an example of construction efficiency and creative planning.
- 2) Fast-Track Review: The timely development and construction of Giuliano First Subdivision will be dependent on the ongoing utilization of the City's 'fast-track' development review process. The projects momentum will rely heavily on open communication and flexibility among all parties involved. We foresee December 2001 as our target date for a final plat, a final development plans and final construction plans.
- 3) Use Tax Credit: Giuliano & Father Construction will use the Use Tax Credit to reduce the cost of the homes. Additionally, Giuliano & Father Construction will follow the CHAFA requirement ensuring that Giuliano Addition's affordable homes are sold to first time, entry-level buyers.
- 4) Development Standards Modifications: Based on the assumption that the homebuyers of the larger homes are more likely to be financially able to upgrade their own landscaping, Giuliano & Father Construction proposes to use the 20% reduction in overall landscape requirements and the 50% reduction of the sizes of the trees and bushes in the parcels that are not classified as affordable housing.

As an established developer and builder of entry-level housing, Giuliano & Father Construction's incentive to build affordable housing is to develop a community where people from all walks of life and income levels live together and to assure the citizens of Loveland that their need for affordable housing is being met.

Land Use

The limits of the Giuliano First Subdivision include the entire 159.6 acres of Giuliano Addition P.U.D. The overall concept is to locate the lower density areas on the west end of the property and the higher density areas on the east. Within this concept we have also integrated other land uses such as open spaces and the school site into the higher density areas, thus changing its overall density.

The highest density/use areas of this project are the multi-family and the neighborhood commercial center. These areas are located on the far east end of the property and will be developed in future PDP applications.

Included in this FDP application are three separate single-family areas all of which have different densities. These single-family areas are referred to as SF-1, SF-2, SF-3, and SF-4 (see sheet CVR, "GDP designation map"). A detailed breakdown of the residential land uses is as follows, and on sheet CVR of the FDP drawing set:

- SF-4 will be constructed in Phase 1. SF-3 will be constructed in Phase 2, along with an interim detention pond. Combined, SF-3 and SF-4 have a total of 162 lots resulting in a net density of 6.57 units per acre.
- SF-2 will be constructed in Phases 3, 4 and 5 with the total of 176 lots, resulting in a net density of 4.63 units per acre
- SF-1 will be constructed in Phase 6 with the total of 36 lots, resulting in a net density of 1.34 units per acre.

Circulation

West 43rd Street provides all of the current accesses into the project, and future accesses will be provided via. Florence Drive to 50th Street. A series of local collector streets will connect the various mixed-use components of the neighborhood. Local residential streets throughout the single-family areas will connect to the collector streets. Convenient access to Buck First Subdivision, the proposed Wilson Commons and adjacent undeveloped land has also been incorporated in the vehicular circulation design. A number of traffic calming devices are proposed throughout the neighborhood including several raised pedestrian crossing/speed humps located to break streets and to connect bicycle and pedestrian trails and green belts. Additionally, a raised intersection has been included in the design where there is a 4-way intersection.

The dedication of the R.O.W. and the design of the improvements for W. 43rd Street to minor arterial standards have been designed in conjunction with the annexation of Giuliano Addition FDP and the phased FDP development of the Buck Addition PUD. Similarly, with the development of adjacent properties, N. Wilson Ave. will be improved to major arterial standards. Refer to sheet SD-4 for typical street cross sections and final street construction plans by others.

Bicycle/pedestrian circulation has been planned to run through the single-family residential development, connecting into the private neighborhood park/play areas and the larger bicycle path network. Giuliano First Subdivision is providing a widened trail to serve as the connection between the Larimer County park trail and the City of Loveland's trail system. This trail is located along W. 43rd Street from N. Wilson Avenue to Sullivan Avenue, then north between the parcel designated SF-4 and the detention pond and the school, then west ending at Florence Drive. The recreation trail east of Florence Drive is to be maintained by the H.O.A. until the city determines and constructs the final connection alignment from the west. A 15-foot public trail easement shall be dedicated and conveyed to Larimer County west of Florence Drive along the north property line for bike, equestrian and pedestrian recreational purposes, see plat for reception number. This trail easement shall be maintained by the H.O.A. until Larimer County begins trail construction, and thereafter Larimer County shall assume maintenance o the trail and trail easement. Other trails encircle the development with the buffer yard /greenbelt areas and provide points of connection into the proposed Wilson Commons and adjacent undeveloped land parcels.

Public & Private Utilities

All public utilities and services are immediately available to the site along W. 43rd Street, or otherwise adjacent to the site, and will be installed underground. Refer to the final utility drawings for specific details regarding the exact line locations and sizes.

Water service will be provided by a westward extension of the existing 16" waterline in W. 43rd Street with a new 24" waterline. Cascade Avenue will also house a new 24" waterline that will tie into the westward extension. Three new services will be installed off of W. 43rd Street as follows:

- 8" waterline along Sullivan Avenue to serve MF-1, C-1, and SF-4.
- 12" waterline along Florence Drive to serve SF-2, SF-3, and SC-1.
- 8" waterline along Fire Court to serve SF-1.

Sanitary sewer will be provided by the extension of a new 8" service line along Sullivan Avenue from the existing 12" main located in W. 43rd Street. The remainder of the development will serviced with new 8" sewer lines.

All utility lines shall be coordinated with the required street trees and other landscaping materials to be in conformance with the following City of Loveland standards:

- Trees shall be planted no closer than 4'-5' from existing or proposed electrical lines.
- Trees shall be planted no closer than 4' from existing or new natural gas lines.
- Trees shall be planted no closer than 10' from water/wastewater lines.
- Trees shall be planted no closer than 10' from storm sewer lines.

A SWMM model has been developed for the Caddoa Basin in which Giuliano Addition PUD is located. The proposed regional pond will be sized in accordance with the revised SWMM. All flows from this property will be to the regional pond, with the exception of the parcels designated MF-1 & C-1, and the back of the lots of SF-4, which will flow undetained to the existing regional pond in the Buck First Subdivision. The construction of the regional pond will be included in Phase 2 of this project. Refer to the Final Drainage and Erosion Control Report and plans for details on this Regional Pond.

Public utility services will be provided by the flowing jurisdictions:

- Water: City of Loveland
- Sanitary Sewer: City of Loveland
- Electric: City of Loveland
- Gas: Public Service of Colorado
- Telephone: Qwest
- Cable TV: Comcast Communications

Public Facilities Providers

The City of Loveland will provide police and fire protection, with street widths and other infrastructure designed to accommodate emergency vehicles. The entire site lies outside of the 1-1/2 mile response diamonds for all existing or planned fire stations, therefore all multi-family and commercial structures over 5,000 gross sq. ft. in area will be required to be protected by automatic fire sprinkler systems.

Several private neighborhood park/play areas are proposed in conjunction with the single-family residential areas of the development. These parks will be centrally located and be connected by bicycle/pedestrian trails. Bicycle pedestrian facilities are discussed in another section of this narrative.

School facilities serving the development will be Centennial Elementary, Lucile Erwin Middle School and Loveland High School. The proposed elementary school in this development will result in some school boundary realignment. An impact report will be prepared by the school district for this development.

Landscaping

A traditional, tree-lined streetscape has been designed for the Giuliano First Subdivision along N. Wilson Avenue, W. 43rd Street, and along the local collector streets. Clusters of same species large shade trees, including varieties of Honey locust, Ash, and Maple will be planted at approximately 40' o.c., providing variety in the branching and color. Minimum size for all street trees will be 2" caliper.

The developer will provide full front yard landscape packages with the sale of the lot, in the parcels designated SF-3 and SF-4. This landscaping will be constructed prior to the issuance of a Certificate of Occupancy.

Lots in area SF-2 will receive sod certificates for the front yards and a certificate for a 2" caliper tree at closing, to be installed by the homeowner. Similarly, homeowners in area SF-1 will also receive a certificate for a 2" caliper canopy tree at closing to be installed by the homeowner. The landscaping in these areas will be constructed within 90 days after the issuance of a Certificate of Occupancy by the homeowner. If weather prohibits installation, all landscaping shall be installed during the next growing season, no later than June 20th of the following year. All residential lots will be required to landscape the front yard with a minimum of 70% living plant materials.

The private neighborhood parks will be sodded, and the greenbelts and other common open spaces will be seeded with a blend of Kentucky Bluegrass. Both areas will be irrigated with an underground automatic sprinkler system. The detention pond will be non-irrigated and seeded with native dryland grasses. A temporary irrigation system will be installed for this area, tract 'G' and shall remain until all vegetation is healthy and grasses have germinated. Phasing of the landscaping is consistent with the proposed construction phasing. The landscaping in each phase will be installed prior to the issuance of any building permits for that phase. Refer to phasing map on sheet CVR of the FDP drawing set. Landscaping in the playfields shall be maintained by the homeowner's association and shall have no effect to the amount of capital expenditures fees otherwise imposed under Chapter 16.38.

An architectural fence has been exclusively designed for this neighborhood. The design for the typical neighborhood edge wood fences will be used to delineate the single-family residential development parcels along the W. 43rd Street frontage. Brick masonry piers will break up edge fences. A maximum of 5' high privacy/lot line fences of the same design will be permitted on the lot lines of individual lots except that the brick piers will not be required. Fences in the residential areas will not extend beyond the front building line. Refer to fence details on sheet SD4 of the FDP drawing set. Phasing of the fencing installation is consistent with the proposed construction phasing. The fencing, in which the developer is responsible for in each phase, will be constructed prior to the issuance of any building permits for that phase. Individual lot fencing, should the homeowner choose to install a fence, will be constructed after the issuance of a Certificate of Occupancy. Refer to phasing map on sheet CVR of the FDP drawing set.

Architectural Concept

Giuliano First Subdivision will be traditional in character and designed in an architecturally consistent manner. Through the use of respective design elements, and recurring architectural details, the design of the structures will be similar in character, unifying the community. Additionally, each lot will have set building envelope with staggered front yard setbacks to add variety in the streetscape. Refer to the lot setback matrix on sheet CVR of the FDP drawing set. Color schemes will also be consistent throughout the project using primarily earth tones, and accent colors will be used in limited amounts for other architectural elements. All architectural standards will be incorporated into covenants for the neighborhood, and then used to evaluate proposed housing designs by the City of Loveland and the Architectural Control Committee. A draft copy of the Architectural Control Committee and Homeowners Association covenants accompanies this FDP submittal.

The following are architectural guidelines as required by the GDP conditions of approval and will in turn substantially bring this project into conformity with the proposed Title 18 requirements:

Except as specified in the GDP for single-story garages in areas SF-3 and SF-4, the standard for side yard building setbacks shall be as specified in the Municipal Code, which is one foot for every 3 feet of building height, or fraction thereof, but a minimum of 5 feet.

Lots with less than 65 feet of frontage on a public street right-of-way shall not be permitted a garage where garage doors for more than two cars are visible as part of the front building elevation.

On lots with less than 65 feet of frontage on a public street, garage doors that are visible as part of front building elevations shall be recessed behind either the front façade of the living portion of the dwelling or a covered porch by at least four (4) feet. Covered porches referred to in this condition shall measure at least eight (8) feet, measured across the front of the building, by six (6) feet.a

Garage doors visible, as part of front building elevations shall not comprise more than forty (40) percent of the ground floor street-facing linear building frontage. Alleys and corner lots are exempt from this condition.

Front facades of single-family dwellings shall include at least two of the following features: bay window, porch, or gabled or hipped roof element.

Minimum front yard building set backs for residential portions of single-family dwellings in areas SF-3 and SF-4 shall be varied and the minimum setbacks shall be 15 feet except for garage door which shall require a minimum setback of 20 feet.

The single-family residential parcels SF-3 & SF-4 are unique in that the construction of the entire areas will be planned. Tentatively, eight different house plans, without basements have been designed for this area. These housing designs will be conventional wood frame construction one and two story floor plans with 2 to 4 bedroom and a two-car garage. Plans will range from 850 to 1800 square feet, exclusive of garage, and marketed to first time homebuyers. Exteriors will be constructed of painted wood/cement composite siding, masonry, or stucco. Gable and or hipped roofing with a consistent type and color of shingles will be used throughout the area. Front facades shall have at least two of the following features: bay window, porch, gabled or hipped roof element, or front entry door.

Basements will be an option for the residential lot in parcel SF-2. The submitted house plans will be equal to or better than the plans in the Buck Addition and Windsong/Sun Disk Village developments. Front yard setback will be staggered with a minimum of 20 feet and a maximum of 24 feet as measured from the back of the sidewalk, depicted by lot and block number in a matrix and submitted with the FDP application. Conventional wood frame construction for the one and two story floor plans with 2 to 4 bedrooms and a two-car garage will range from 850 to 2000 square feet. Additionally 75% of the front elevation options shall include one of the following elements: aside load garage, the garage doors recessed a minimum of (4) four feet behind the front façade, a covered porch, or a box bay window. Front facades shall also include a minimum of 2 windows for a single story house and a minimum of 3 windows for a two-story dwelling.

The single-family residential parcel SF-1 will be one and two story floor plans with 2 to 5 bedrooms and an attached garage housing a minimum of three vehicles. Plans will range from 1400 to 4000 square feet exclusive of garages. Utility infrastructure and grading designs should allow the construction of basements or walkout levels in all of these residences. Setbacks shall be consistent with the City of Loveland's requirements. All exteriors will be constructed of painted wood/cement composite siding, various forms of masonry or stucco. Roofing materials will be, but not limited to, clay tiles and laminated fiberglass shingles with a minimum roof pitch of 5/12 for primary roof planes. The location of the garage will be even with or behind the front facade. Front facade shall also include a minimum of 3 windows for a single story house and a minimum of 4 windows for a two-story dwelling.

Landform Modification

The existing land use on the Giuliano Addition PUD is dryland farming, and the land slopes in a south/southeasterly direction. No significant landform modifications are necessary as a part of this development, other than for construction of the regional stormwater detention facility. Cut and fill will be required to provide adequate drainage away from proposed structures and to create the necessary drainage swales to the on-site detention facility.

Vesting/Construction Phasing

Vesting of property rights for Giuliano First Subdivision will occur upon final approval of this FDP.

The single family residential development will be built in multiple phases, with total build out planned for 7-8 years.

Construction phasing is proposed as follows: (see also the phasong map on sheet CVR of the FDP drawing set)

- Phase 1: Single-family area SF-4, including 105 lots and the development of the private park/play area tract B
- Phase 2: Single-family area SF-3, including 57 lots, the regional pond, the, development of the private park/play area tract H, and the School Site area SC-1
- Phase 3: A portion of Single-family area SF-2, including 69 lots and the development of the private park/play area tract N
- Phase 4: A portion of Single-family area SF-2, including 69
- Phase 5: A portion of Single-family area SF-2, including 38 lots and the development of the private park/play area tract O
- Phase 6: Single-family area SF-1, including 36 lots

Implementation/Regulatory Procedures

A homeowner's association will be established to govern the residential areas of the community and to maintain the neighborhood edge fencing, signage, neighborhood parks, and landscaping in the common open spaces as well as the landscaping in the public right-of-ways throughout the residential development. Protective covenants will be developed to insure the quality of all construction, establishing design standards for all residential building types. The Architectural Control Committee will also review and approve all proposed housing of the Giuliano Addition PUD prior to the issuance of building permits. A letter of this approval will be submitted with each building permit application.

The current annexation and zoning status of Giuliano Addition PUD allows the developer the necessary design freedom and flexibility to ensure the economic feasibility of the project as long as the conditions of approval are met.

Notwithstanding any information presented graphically in the FDP or accompanying document, all public improvement shall conform to the standards adopted June 1, 1999 unless specifically approved in writing a variance.

issue date

10/27/14

REVISIONS

- 1 02/09/15
- 2 03/09/15

sheet title

PROJECT NARRATIVE

GDP CONDITIONS OF APPROVAL

ENGINEERING		VESTING (added by City Council)	
1.	43rd Street - Right-of-way and Improvements. If determined necessary by the City Engineer, the developer shall acquire and dedicate to the City, at no cost to the City, sufficient right-of-way along the southern portion of the westernmost 900 feet of the proposed addition to construct all required standard improvements for 43rd Street along the entire southern boundary of the addition. This condition shall be effective upon submittal of the appropriate FDP application, as determined by the City Engineer.	13a.	Waiver of Damages. In the future, the Developer may be granted vested property rights associated with the approval of a site specific development plan within the property. In the event that such vested property rights are granted, and the City applies an initiated or referred measure to the property which would (a) change any term or condition of this Agreement, (b) impose a moratorium on development within the property, or otherwise materially delay the development of the property, or (c) limit the number of building or utility permits to which the Developer would otherwise be entitled, the Developer agrees to waive any right to damages against the City to which the Developer may otherwise be entitled under the Vested Rights Statute.
		CURRENT PLANNING	
2.	Cascade Avenue - Right-of-way and Improvements. If determined necessary by the City Engineer, the developer shall acquire and dedicate to the City, at no cost to the City, sufficient right-of-way along the western boundary of the proposed addition to construct all required standard improvements for Cascade Avenue. This condition shall be effective upon submittal of the appropriate FDP application, as determined by the City Engineer.	14.	FDP applications within 100 feet of the base limestone ridge shall include the results of the Bell's Twinpod survey. If the plant is located on the site, an appropriate buffer as determined by the City shall be preserved as open space.
3.	Wilson Avenue - Right-of-way and Improvements. The developer shall acquire and dedicate to the City, at no cost to the City, sufficient right-of-way along the eastern boundary of the proposed addition to construct all required standard improvements for Cascade Avenue. This condition shall be effective upon submittal of the first FDP application.	15.	Except as specified in the GDP for single-story garages in areas SF-3 and SF-4, the standard for side yard building setbacks shall be as specified in the Municipal Code, which is one foot for every 3 feet of building height, or fraction thereof, but a minimum of 5 feet.
4.	Transportation Master Plan. Future development applications within this project shall fully comply with any updates to the City of Loveland Transportation Master Plan adopted by the City Council for any portion of this project.	16.	Lots with less than 65 feet of frontage on a public street right-of-way shall not be permitted a garage where garage doors for more than two cars are visible as part of the front building elevation.
5.	Unless constructed by others, the developer shall design and construct a raised median for Wilson Avenue extending from the intersection of 43rd Street to 200 feet south of the intersection of Glen Isle Drive. The timing of this improvement shall be based on which FDP application is shown to cause the need for mitigation, based on level-of-service standards as applied to the intersection of Wilson Avenue and Glen Isle Drive. Other mitigation measures may be substituted for this improvement if accepted by the City Engineer.	17.	On lots with less than 65 feet of frontage on a public street, garage doors that are visible as part of front building elevations shall be recessed behind either the front facade of the living portion of the dwelling or a covered porch by at least four (4) feet. Covered porches referred to in this condition shall measure at least eight (8) feet, measured across the front of the building, by six (6) feet.
6.	At the time of each FDP application, a Traffic Impact Study shall be submitted which includes a trip distribution based on the gravity model or an alternative distribution method acceptable to the City Engineer.	18.	Garage doors visible as part of front building elevations shall not comprise more than forty (40) percent of the ground floor street-facing linear building frontage. Alleys and corner lots are exempt from this condition.
7.	Notwithstanding conceptual information presented in the GDP, access locations and operations shall be determined at the time of application for FDP approval.	19.	Future FDP applications shall include useable open space that is easily accessible to residential lots throughout a development. Open space shall be thoroughly integrated into residential projects.
PARKS AND RECREATION		20.	To address noise impacts on residential areas, FDP applications for development within zoning area C-1 shall include a noise study demonstrating compliance with City noise standards for residential areas. This study shall include analysis of loading, delivery, and operational noise sources. Potential restrictions on these operations including limitations on hours of operation may be imposed to achieve compliance with City standards.
8.	Within the annexation parcel, the applicant shall dedicate to the City of Loveland a 25-foot wide tract of land to be used for a recreation trail. The parcel shall provide a connection from a location near the northwest corner of the annexation parcel to a currently undetermined point on 43rd Street. The trail installation shall be the responsibility of the developer and shall be built to City of Loveland standards. The location of the trail shall be determined by the City after consultation with the developer at the time of the first PDP/Preliminary Plat within the annexation parcel. The location of the trail shall not unreasonably interfere with the developers preferred subdivision.	21.	The annexation map shall be revised to include the following statement in the Notes section: This annexation is subject to an annexation agreement which has been recorded in the real property records of Larimer County.
WATER/WASTEWATER/STORM DRAINAGE		22.	The GDP narrative shall be revised to reflect that fences other than the neighborhood edge fence shall be a maximum of five feet in height and shall match the lot line fence shown on SD1.
9.	The Developer shall design and construct a 24" water line along the south property line, from the west boundary of Buck 2nd Subdivision, along West 43rd Street, to the west boundary of Giuliano Addition then north along the west property line to the north boundary of this addition. The City may approve alternate alignments at the time of a FINAL Development Plan submittal.	23.	In area MF-1 the maximum number of congregate care units shall not exceed 50 dwelling units.
10.	The Developer shall design and construct an extension of the existing 16" waterline in W. 43rd Street, from its existing terminus, to connect with the aforementioned 24" waterline long the south property line.	24.	Front facades of single-family dwellings shall include at least two of the following features: bay window, porch, or gabled or hipped roof element.
11.	The Developer shall design and construct waterlines within the subject annexation that connect to the aforementioned 24" waterline along the west boundary and to the existing 16" waterline in W. 43rd Street and existing 30" waterline in N. Wilson Avenue. Specific alignments, connection points and sizing requirements will be determined by the City at the time of a FINAL Development Plan.	25.	Notwithstanding the typical street cross-sections shown on SD-1, the street sections proposed with subsequent FDP applications shall comply with City street standards.
12.	The Developer shall complete an Engineering Analysis of the surrounding and other applicable water system facilities in order to determine the need for additional water system improvements. The City will determine the content and specific parameters of the Analysis at the time of a FINAL Development Plan submittal.	26.	Minimum front yard building set backs for residential portions of single-family dwellings in areas SF-3 and SF-4 shall be varied and the minimum setbacks shall be 15 feet except for garage door which shall require a minimum setback of 20 feet.
POWER		27.	Language in the GDP narrative indicating that in area SF-1 the minimum lot width may be 60 feet and corner lots must be at least 70 feet shall be deleted.
13.	A 5% surcharge will be assessed on the sale of electricity to any development for a period of 10 years from the date of annexation. A note will be required on each final plat regarding this requirement as follows: "A surcharge of 5% will be added to all bills for the sale of electric power to additional services which came into the existence after January 31, 1987, within the territory herein annexed, which surcharge will expire ten years after effective date of this annexation."	28.	In areas SF-3 and SF-4, the maximum gross density shall be 5.5 dwelling units per acre instead of 6.0.

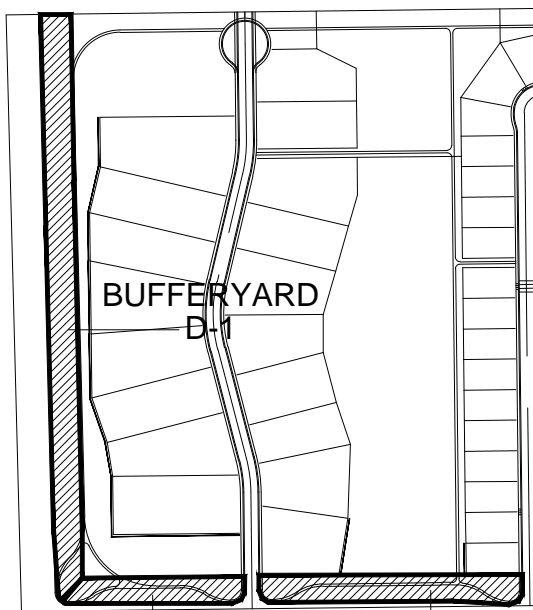
PDP CONDITIONS OF APPROVAL

Stormwater:		Current Planning:	
1.	The Developer shall design the stormwater drainage plan for this FDP to provide 100 year level detention with a 2-year historic release rate, as identified in the master plan update, until such time that the regional detention pond is constructed. Prior to issuance of the first building permit in this FDP, the Developer shall dedicate to the City the land for the future regional detention pond as depicted on the FDP. The Developer shall be eligible for reimbursement through standard City reimbursement policies, based on undeveloped land cost.	14.	In accordance with the requirements set forth in the Giuliano Addition GDP, the landscape plan of the FDP shall include a habitat enhancement plan designed to mitigate the loss of wetlands on the project site. The landscape plan shall also include a temporary irrigation system for the area in and around the regional stormwater detention facility and such system shall remain until all vegetation planted in and around the regional stormwater detention facility is healthy and grasses have germinated. Such plan must also include the variety of riparian species, as listed in the GDP. No building permits shall be issued once construction of the regional stormwater facility is completed unless the Developer posts a cash security with the City for the landscaping, mitigation measures, and irrigation system required under this condition. In no case shall building permits be issued for Phase 6 until such security has been posted with the City. The City shall install the landscaping, mitigation measures and irrigation system required under this condition as part of its design and construction of the regional stormwater detention facility. The Developer's security required under this condition shall be transferred to the City, which shall use the funds to install the landscaping, mitigation measures and irrigation system required under this condition.
Water / Wastewater:		15.	Prior to issuance of any building permits in Phase 1, a native seed mix shall be planted to cover the total area of Tract F, including the temporary stormwater detention facility. The FPD shall identify the type of native seed mix as well as the quantity and method that will be used for seeding. A temporary irrigation system shall be installed and remain until all grasses have germinated. The landscape security for Phase 1 shall include the costs of planting the seed mix and installing the irrigation system required under this condition. That portion of the security for the grasses and temporary irrigation system shall not be released until the City has determined that all grasses have germinated and are in a health condition.
2.	The Developer shall coordinate the design and installation of the project related utilities with the City's proposed waterline in West 43rd Avenue. The final construction plans shall reflect specific alignments and connection points.	16.	The smaller detention areas within area SF-1, shall be landscaped with native grasses (wetland mix), shrubs and trees compatible for prairie riparian areas below 6,000 feet.
3.	The Developer shall design and construct the proposed sanitary sewer main and manholes to be entirely outside any portion of the 100-year water surface elevation "limits" for the proposed storm water detention pond. Additionally, access for heavy maintenance vehicles along the entire reach of sanitary sewer main and manholes, within the proposed storm water detention pond area, must be provided by a minimum 20' all weather access roadway. The specific alignment and configuration of the sanitary sewer main and access roadway can be determined with the Final Development Plan.	17.	Overlot grading within Block 16 of the plat shall be minimized and building envelopes depicted on the FDP and plat to avoid disturbance to the limestone ridge located on Outlot 3.
4.	At no cost to the City, the Developer shall design and install a public water booster station. The station shall be installed prior to issuance of any building permit within that portion of the FDP situate west of Florence Drive. At a minimum, the station shall be sized to accommodate all proposed lots west of Florence Drive as well as the entire adjacent development known as the Buck 2nd Subdivision. The specific location of the water booster station shall be shown on the final development plan. The Developer shall be responsible for the cost, creation and/or acquisition of any public easements necessary to accommodate the herein mentioned water booster station and all associated main line extension(s) and related appurtenances.	18.	The location of the required water booster station shall be approved by all departments at time of FDP.
Fire Prevention:		19.	In accordance with the requirements set forth in the Giuliano Addition GDP, a cluster mailbox system to service groups of single family homes shall be included with any development proposal. The FDP shall depict the specific architectural treatment and landscaping for these clustered mailboxes.
5.	All dwellings on Fife Court shall be protected with automatic sprinkler systems in accordance with NFPA 13R.	20.	In accordance with the requirements set forth in the Giuliano Addition GDP, private neighborhood park/play areas and greenbelts shall be landscaped in a softer, less formal manner, using groupings or clustered trees and shrub masses to delineate a variety of active and passive activities. The FDP landscape plan shall be consistent with this requirement.
6.	A temporary emergency access roadway shall be provided from Kincaid Drive to Wilson Avenue prior to issuance of any building permits in Phase 2. The construction plans for such temporary roadway shall be subject to approval by the City of Loveland Fire Department and City Engineer.	21.	In accordance with the requirements set forth in the Giuliano Addition GDP, a homeowner's association shall be established to govern the residential areas of the community and to maintain the landscaping, fencing, signage, neighborhood parks, and common open space throughout the residential development. Protective covenants shall also be developed to insure the quality of all construction and establishing design standards for all residential building types. The developer shall also create an architectural review committee to review and approve all proposed housing for the Giuliano Addition GDP, to be established prior to the issuance of the first building permit within the GDP. Notwithstanding the foregoing, the City shall not be bound by the decisions of the architectural review committee and shall incur no liability for the issuance of building permits approved without approval from the architectural review committee.
Parks and Recreation:		22.	A separate sign permit shall be required for the entry monument sign.
7.	The trail locations as proposed on the FDP shall conform to Larimer County and City of Loveland requirements and shall be subject to review and approval by the City of Loveland Parks and Recreation Department. All such trails shall be depicted on the FDP and all final grades and width requirements shall be depicted on the final construction drawings. The final design of all such trails shall be coordinated with the design of storm water or detention channels and basins proposed within this FDP and on the adjoining property to the north. Detailed cross sections of the trail alignment along the north property line of this FDP, adjacent to the school site and west of Florence Drive, shall be submitted for review with the FDP. Such alignment and cross sections shall be subject to approval by the City of Loveland Parks and Recreation Department.	23.	In accordance with the requirements set forth in the Giuliano Addition GDP, a minimum of 20% of the single family detached dwellings in this FDP shall be sold to households with incomes at 70% or less of the Median Family Income (MFI) for Loveland, as determined by the U.S. Department of Housing and Urban Development.
8.	The recreation trail shall not be located within any part of the drainage conveyance system (north of the school or in the County trail area) and shall be located above and outside the surface elevation of the 20 year storm event. The surface elevation of the 20 year storm event shall be depicted on the FDP.		
9.	The recreational trail shall be designed and constructed by the Developer according to City of Loveland recreation trail standards and shall not exceed ADA standards for maximum grades. Construction drawings for said trail shall be subject to approval by the City of Loveland Parks and Recreation Department prior to approval of the FDP.		
Engineering:			
10.	All public improvements within the Preliminary Development Plan (PDP) and Preliminary Plat shall comply with the Larimer County Urban Area Street Standards (LCUASS).		
11.	Unless designed and constructed by others, the Developer shall design and construct the following public improvements prior to the issuance of any building permits within the PDP, unless otherwise approved pursuant to the provisions in Section 16.40.010.B of the Loveland Municipal Code:  a. 43rd Street. The Developer shall construct the portion of West 43rd Street adjacent to the PDP to the LCUASS 2-lane arterial street standard.  b. Cascade Avenue. The Developer shall design and construct that portion of Cascade Avenue adjacent to the PDP to the LCUASS major collector street standard. The City may accept cash in-lieu for all or a portion of the construction costs for such improvements, in a form acceptable to the City Attorney, if approved in writing by the City Engineer.		

REVISIONS	
1	02/09/15
2	03/09/15



BUFFERYARD MAP & PLANT LIST



Bufferyard D-1

Linear Feet x Depth  
1,200' x 55'

Canopy Trees	24
Flowering Trees or Large Shrubs	24
Shrubs	150
Evergreens/Conifers	18

Bufferyard D-2

Linear Feet x Depth  
400' x 45'

Canopy Trees	8
Flowering Trees or Large Shrubs	8
Shrubs	50
Evergreens/Conifers	6

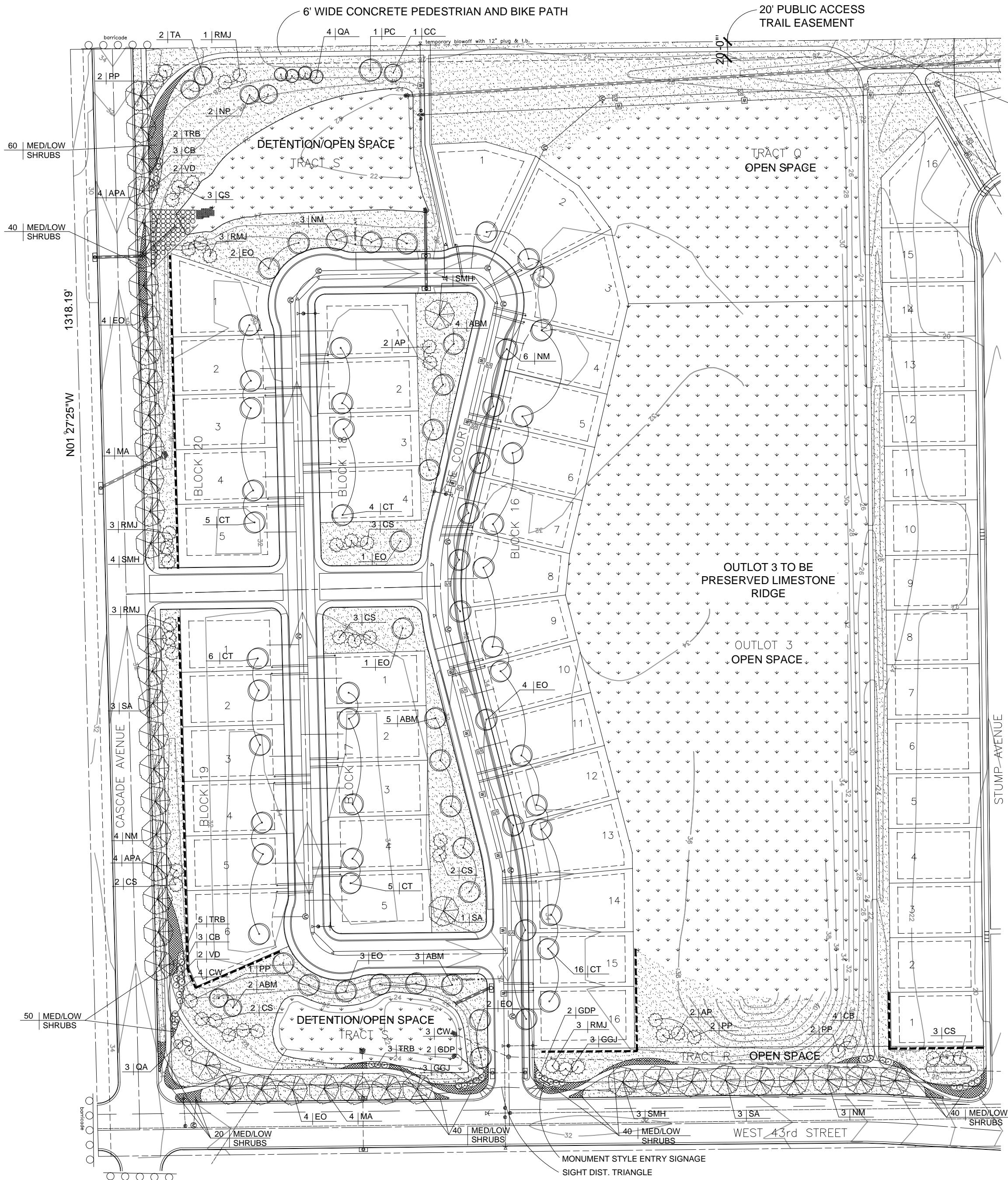
Bufferyard D-3

Linear Feet x Depth  
550' x 45'

Canopy Trees	11
Flowering Trees or Large Shrubs	11
Shrubs	69
Evergreens/Conifers	9

GENERAL LANDSCAPE NOTES

- STREET CANOPY TREES FOR THE SINGLE FAMILY LOTS ARE INDICATED BY THE MARK 'CT' ON THE LANDSCAPE PLAN AND THE PLANT LIST. ONE TREE PER LOT, AND TWO TREES PER CORNER LOTS ALONG LOCAL STREETS WILL BE PROVIDED BY THE DEVELOPER THROUGH THE ISSUANCE OF A CERTIFICATE TO THE LOT OWNER FOR THE INSTALLATION OF THE TREE OF THEIR CHOICE FROM THE CITY OF LOVELAND'S APPROVED LIST OF SPECIES.
- PLAY AREAS SHALL BE MAINTAINED BY THE HOMEOWNERS ASSOCIATION AND SHALL HAVE NO EFFECT TO THE AMOUNT OF CAPITAL EXPENDITURES FEES OTHERWISE IMPOSED UNDER CHAPTER 16.38.
- LANDSCAPE PLAN IS DIAGRAMMATIC; TREE AND SHRUB LOCATION MAY BE ADJUSTED, AS NEEDED, AT TIME OF PLANTING TO MAINTAIN A MINIMUM SEPARATION FROM UTILITY LINES AS FOLLOWS:
  - \* TREES NO CLOSER THAN 4'-5' FROM ELECTRICAL LINES.
  - \* TREES NO CLOSER THAN 4' FROM NATURAL GAS LINES.
  - \* TREES NO CLOSER THAN 10' FROM WATER/WASTEWATER LINES.
  - \* TREES NO CLOSER THAN 10' FROM STORM SEWER LINES
  - \* SHRUBS 4' MINIMUM SEPARATION.
- ALL LANDSCAPING ILLUSTRATED BY THIS PLAN SHALL BE INSTALLED AND MAINTAINED BY THE DEVELOPER UNLESS OTHERWISE INDICATED.
- SCHEDULED SIZES STATED REPRESENT MINIMUM CALIPER AND HEIGHT. SIZES PROVIDED MAY EXCEED MINIMUM REQUIRED, BUT IN NO EVENT SHALL SMALLER PLANT MATERIAL SIZES BE PROVIDED.
- ALL LANDSCAPE AREAS SHALL BE IRRIGATED WITH AN AUTOMATIC UNDERGROUND IRRIGATION SYSTEM, AND IRRIGATED WITH DOMESTIC WATER, UNLESS NOTED OTHERWISE.
- UNDERGROUND UTILITIES SHALL BE LOCATED PRIOR TO INSTALLATION OF LANDSCAPE AND IRRIGATION.
- SIGHT DISTANCE EASEMENTS RESTRICTS PERMANENT OBJECTS TALLER THAN 24' ABOVE THE PAVEMENT, WITHIN THE EASEMENT. STREET TREES SHALL BE PERMITTED AS REQUIRED BY SITE DEVELOPMENT STANDARDS AND SHALL BE LIMBED UP WITH NO BRANCHES EXTENDING BELOW 7' ABOVE PAVEMENT.
- STEEL EDGERS SHALL BE PROVIDED AROUND ALL PLANTING BEDS ADJACENT TO TURF AREAS. STEEL EDGERS SHALL BE GALVANIZED, 1/8" X 4" DEEP.
- GRAPHIC SYMBOLS PRESIDE OVER WRITTEN PLANT QUANTITIES.
- THIS LANDSCAPE PLAN INCLUDES (NOT SHOWN ON PLANS) A TEMPORARY IRRIGATION SYSTEM FOR THE AREA IN THE REGIONAL DETENTION POND TRACT G. THIS SYSTEM SHALL REMAIN UNTIL ALL VEGETATION IS HEALTHY AND GRASSES HAVE GERMINATED.
- DEVELOPER SHALL ENSURE THAT THE LANDSCAPE PLAN IS COORDINATED WITH THE PLANS DONE BY OTHER CONSULTANTS SO THE PROPOSED GRADING, STORM DRAINAGE, OR OTHER CONSTRUCTION DOES NOT CONFLICT NOR PRECLUDE INSTALLATION AND MAINTENANCE OF LANDSCAPE ELEMENTS ON THIS PLAN. CHANGES IN THE LANDSCAPE PLACEMENT MAY BE ALLOWED DUE TO CHANGES IN SITE CONSTRUCTION.
- DECIDUOUS TREES SHALL BE PLANTED A MINIMUM OF 8' FROM THE EDGE OF THE RECREATIONAL TRAIL. EVERGREEN TREES SHALL BE PLANTED A MINIMUM OF 12' FROM THE EDGE OF THE RECREATIONAL TRAIL.



- A. All trees adjacent to the easement shall adhere to the following standards:
- Minimum offset for coniferous trees from edge of easement to tree center is 12 feet.
  - Minimum offset for deciduous trees from edge of easement to tree center is 6 feet.
  - Minimum clear zone defined as the vertical height from trail to lowest branch is 10 feet.
- B. All plant beds shall utilize wood fiber mulch only. Rock mulch shall not be permitted adjacent to the trail.
- C. The easement can be maintained with dryland grasses if the developer or HOA prefer this surfacing solution.

PLANT SELECTION LIST

MRK.	QTY.	COMMON NAME	LATIN NAME	PLANTING SIZE
<b>Large Shade Trees</b>				
APA	8	Autumn Purple Ash	Fraxinus americana 'Autumn Purple'	2' CAL.
MA	8	Marshall Seedless Ash	Fraxinus pennsylvanica lanceolata 'Marshall'	2' CAL.
SA	6	Summit Ash	Fraxinus pennsylvanica lanceolata 'Summit'	2' CAL.
NM	15	Emerald Queen Norway Maple	Acer platanoides 'Emerald Queen'	2' CAL.
ABM	11	Autumn Blaze Maple	Acer x freemanii 'Jeffersred'	2' CAL.
EO	14	English Oak	Quercus robur	2' CAL.
SMH	8	Shademaster Honeylocust	Gleditsia tri inermis 'Shademaster'	2' CAL.
CT	32	CANOPY TREE SELECTED BY LOT OWNER - SEE GENERAL NOTE #1		
<b>Ornamental Trees</b>				
NP	2	Newport Plum	Prunus speciosa	1 1/2' CAL.
QA	7	Quaking Aspen	Populus tremuloides	1 1/2' CAL.
TA	2	Thrinax Alder	Alnus tenuifolia	1 1/2' CAL.
<b>Coniferous / Evergreen Trees</b>				
AP	2	Austrian Pine	Pinus nigra	6H
PP	7	Ponderosa Pine	Pinus ponderosa	6H
RMJ	17	Rocky Mountain Juniper	Juniperus scopulorum	6H
CS	18	Colorado Spruce	Picea pungens	6H
<b>High Deciduous and Coniferous Shrubs (4'-15' in height)</b>				
VD	4	Variegated Dogwood	Cornus alba argentea-marginata	5 GAL.
CB	10	Black Chokeberry	Aronia melanocarpa elata	5 GAL.
GGJ	6	Gray Glean Juniper	Juniperus scopulorum	5 GAL.
GDP	4	Gold Drop Potentilla	Potentilla fruticosa 'Gold Drop'	
<b>Low Medium Deciduous and Coniferous Shrubs (18"-4' in height)</b>				
CC	1	Native Chokeberry	Prunus virginiana melanocarpa	5 GAL.
CW	4	Coyote Willow	Salix exigua	5 GAL.
TRB	10	Tall Blue Rabbitbrush	Chrysothamnus sp. albicaulis	5 GAL.
PC	1	Plains Cottonwood	Populus sargentii	5 GAL.

SEED MIXES

Drought Tolerant Turf Seed Mix

Common Name	% of Total
Lincoln Smooth Brome	40%
Alta Tall Fescue	60%
Seeding Rate:	
Irrigated: 4 lbs / 1000 S.F.	

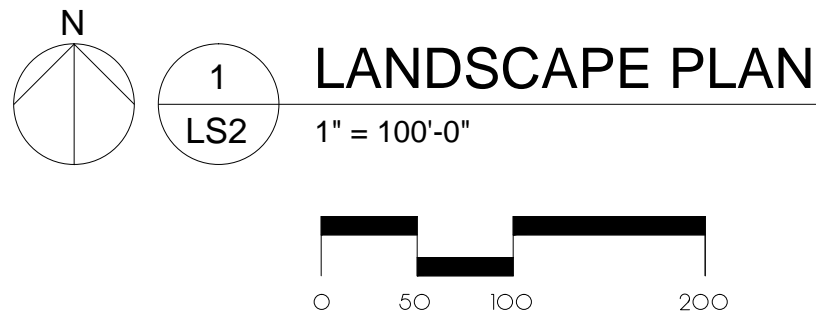
Footfills Native/ Wetland Seed Mix

Common Name	% of Total
Crested Wheatgrass	25%
Mountain Brome	20%
Hard Fescue	15%
Annual Ryegrass	15%
Kentucky Bluegrass	10%
Sideoats Grama	5%
Little Bluestem	5%
Blue Grama	4%
Sand Dropseed	1%

Seeding Rate:  
Broadcast: 20-25 lbs./ acre  
Drilled: 15-20 lbs./ acre

LANDSCAPE LEGEND

- PROPOSED DECIDUOUS CANOPY TREES
- PROPOSED DECIDUOUS STREET TREES
- PROPOSED CONIFEROUS/EVERGREEN TREES
- PROPOSED ORNAMENTAL TREES
- PROPOSED HIGH DECIDUOUS & CONIFEROUS SHRUBS
- PROPOSED SHRUB BEDS (to contain a mix of medium and low deciduous & coniferous shrubs)
- IRRIGATED SOD
- IRRIGATED TURF SEED MIX
- NON-IRRIGATED NATIVE/ WETLAND SEED MIX
- AREA OF NON-IRRIGATED RE-ESTABLISHED WETLANDS



LANDSCAPE PLAN

1" = 100'-0"

issue date

10/27/15

REVISIONS

1	02/09/15
2	03/09/15

sheet title

LANDSCAPE PLAN,  
NOTES, DETAILS,  
SEED MIXES &  
PLANT LISTS

LS1

































PC ATTACHMENT 7













PC ATTACHMENT 7







GIULIANO FOURTH SUBDIVISION  
BEING A SUBDIVISION OF BLOCKS 16 & 17, TRACTS S & U, AND OUTLOT 1 OF GIULIANO FIRST SUBDIVISION, AS RECORDED AT RECEPTION No. 2004008797  
SITUATE IN THE SOUTH 1/2 OF SECTION 33, TOWNSHIP 6 NORTH, RANGE 69 WEST OF THE SIXTH P.M.,  
CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO

KNOW ALL MEN BY THESE PRESENTS: That the undersigned being all the owners and lienholders of the following described property, except any existing public streets, roads, or highways, which property is located in the South 1/2 of Section 33, Township 6 North, Range 69 West of the Sixth P.M., containing 159.9597 acres, more or less, and being a subdivision of Giuliano First Subdivision to the City of Loveland, County of Larimer, State of Colorado, as recorded at Reception No. 2004008797, being more particularly described as follows: considering the North line of the South 1/2 of the South 1/2 of said Section 33 as bearing N89°18'33"E and with all bearings contained herein relative thereto, is contained within the boundary lines which begin at a point on said North line which bears N89°18'33"W 90.00 feet from the South 1/16 Corner on the West line of said Section 33, and run thence along the North line of the South 1/2 of the South 1/2 of said Section 33, N89°18'33"E 962.36 feet; thence departing said North line, S00°41'27"E 69.91 feet to a point on the boundary line of Block 15 of said Giuliano First Subdivision; thence along said boundary line, S29°30'42"E 105.00 feet, and again along the arc of a 68.00 foot radius curve concave to the Southeast a distance of 31.07 feet, whose central angle is 26°10'43", the long chord of which bears S47°23'56"W 30.80 feet, and again N29°30'42"W 101.90 feet, and again S42°44'06"W 97.67 feet, and again S00°39'18"E 1035.00 feet to a point on the boundary line of Tract T of said Giuliano First Subdivision; thence along said boundary line, S89°37'51"W 270.59 feet, and again S89°20'42"W 160.00 feet, and again S00°39'18"E 27.88 feet, and again along the arc of a 12.00 foot radius curve concave to the Northeast a distance of 18.85 feet, whose central angle is 90°00'00", the long chord of which bears S45°39'27"W 16.97 feet to a point in the existing North right-of-way line of West 43rd Street; thence along said existing North right-of-way line, S89°20'42"W 426.16 feet; thence along the arc of a 12.00 foot radius curve concave to the Northeast a distance of 18.68 feet, whose central angle is 89°11'53", the long chord of which bears N46°03'22"W 16.85 feet; thence N01°25'25"W 1225.40 feet to the point of beginning; do hereby subdivide the same into lots, blocks, tracts, right-of-ways and easements as shown on this plat; and do hereby designate and dedicate: (1) all such right-of-ways and easements as shown on this plat, and other private easements to and for public use, except where indicated otherwise on this plat; and (2) all such utility easements to and for public use for the installation and maintenance of utility, irrigation and drainage facilities; and do hereby designate the same as GIULIANO FOURTH SUBDIVISION to the City of Loveland, Colorado. All expenses involving necessary improvements for a water system, sanitary sewer system, curbs and gutters, sidewalks, street signs, street improvements, traffic control signs, alley grading and surfacing, gas service, electric service, grading and landscaping shall be paid by the owner.

OWNER: GIULIANO AND FATHER CONSTRUCTION, INC. A COLORADO CORPORATION  
also known as GIULIANO AND FATHER CONSTRUCTION CO., INC. A COLORADO CORPORATION

BY \_\_\_\_\_  
JOHN G. GIULIANO, President

State of Colorado } S.S.  
County of Larimer }  
The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2014, by John G. Giuliano as President of Giuliano and Father Construction, Inc..

My notarial commission expires \_\_\_\_\_ NOTARY PUBLIC \_\_\_\_\_

GIULIANO ADDITION LLLP, a Colorado Registered Limited Liability Partnership

BY \_\_\_\_\_  
JOHN G. GIULIANO, Partner

State of Colorado } S.S.  
County of Larimer }  
The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2014, by John G. Giuliano as Partner of Giuliano Addition LLLP, a Colorado Registered Limited Liability Partnership.

My notarial commission expires \_\_\_\_\_ NOTARY PUBLIC \_\_\_\_\_

J & P FAMILY, LLLP, a Colorado Registered Limited Liability Partnership

BY \_\_\_\_\_  
JOHN G. GIULIANO, Partner

State of Colorado } S.S.  
County of Larimer }  
The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2014, by John G. Giuliano as Partner J & P FAMILY, LLLP, a Colorado Registered Limited Liability Partnership.

My notarial commission expires \_\_\_\_\_ NOTARY PUBLIC \_\_\_\_\_

OWNER:

BY \_\_\_\_\_  
JOHN T. GIULIANO, Owner

State of Colorado } S.S.  
County of Larimer }  
The foregoing instrument was acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 2014, by John T. Giuliano as Owner.

My notarial commission expires \_\_\_\_\_ NOTARY PUBLIC \_\_\_\_\_

I, \_\_\_\_\_, an attorney licensed to practice law in the State of Colorado, certify that I have examined the title to the above described land dedicated to the City of Loveland, Colorado, and that the parties executing the dedication are the owners thereof in fee simple, and the dedicated land is free clear of all liens and encumbrances, except as set forth herein.

So sworn this \_\_\_\_\_ day of \_\_\_\_\_, 2014

\_\_\_\_\_  
ATTORNEY AT LAW

I, Jay S. Robinson, being a registered Professional Land Surveyor in the State of Colorado, do hereby certify that the survey of GIULIANO FOURTH SUBDIVISION was made by me or under my supervision and that the survey is accurately represented on this plat and that the statements contained hereon were read by me and the same are true to the best of my knowledge.

JAY S. ROBINSON  
Colorado registered P.L.S. No. 37899



This plat is approved by the Director of Community Services of the City of Loveland, Larimer County, Colorado, this \_\_\_\_\_ day of \_\_\_\_\_, 2014, for filing with the Clerk and Recorder of Larimer County and for conveyance to the City of the public dedications shown hereon, which are accepted; subject to the provision that approval in no way obligates the City of Loveland, for the financing or construction of improvements on land, streets or easements dedicated to the public except as specifically agreed to by the Director of Community Services.

Witness our hands and seal of the City of Loveland.

BY \_\_\_\_\_  
DIRECTOR OF COMMUNITY SERVICES

ATTEST \_\_\_\_\_  
CITY CLERK

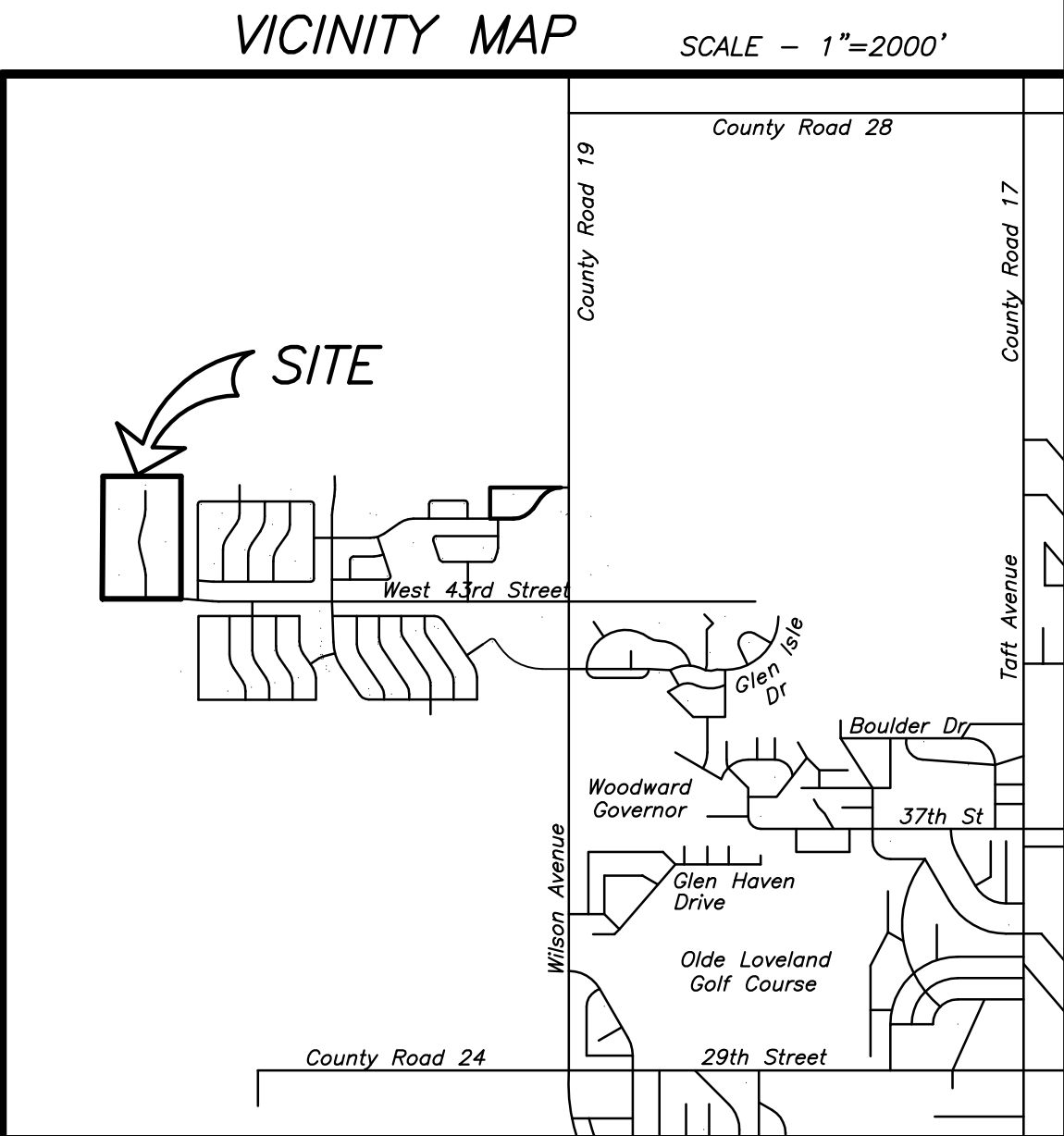
According to Colorado law, you must commence any legal action based upon any defect in this survey within three years after you first discover such defect. In no event, may any action based upon any defect in this survey be commenced more than ten years from the date of the certification shown hereon.

PLAT NOTES:

- There is a 14 foot utility and postal easement along all street rights-of-way, a 10' utility and drainage easement along all rear lot lines, and a 5' utility and drainage easement along all side yard lot lines unless otherwise shown.
- Construction of public improvements and issuance of building permits within this subdivision are governed by a separate agreement between the City of Loveland and the owner, which agreement constitutes a covenant running with the land.
- All Tracts and Outlots are open space, utility, drainage and pedestrian access easements.
- All Tracts and Outlots are to be owned and maintained by the Homeowners Association.
- All Tracts ARE to be irrigated with City of Loveland treated water. All Outlots will NOT be irrigated with City of Loveland treated water.
- This property is subject to a Development Agreement which has been recorded in the real property records of Larimer County.
- This property is subject to a Preliminary Development Plan which is on file in the City Building Division.
- Approval of this plan creates a vested property right pursuant to Section 24-68-103, C.R.S., as amended.
- A surcharge of 5% will be added to all bills for the sale of electric power to additional services which came into the existence after January 31, 1987, within the territory herein subdivided which surcharge will expire ten years after effective date of this subdivision.
- All residential side and rear yard drainage easements are to be privately maintained by the individual lot owners.
- All swales and detention pond drainage easements are to be privately maintained by the Homeowners Association.
- Bearings are based on the assumption that the North line of the South 1/2 of the South 1/2 of Section 33-T6N-R69W bears N89°18'33"E.
- ⊙ - Indicates set 1/2" rebar with orange plastic cap PLS No. 37899 unless otherwise noted.
- A 1/2" rebar with cap PLS No. 37899 will be placed in a monument box at all street intersections and points of curvature after pavement is in place if required by the City of Loveland at that time.
- The lineal unit of measurement used for the surveying of this property is U.S. Survey Feet.
- Total area in square footage of easements dedicated to the public or the City by this plat. Total area in square feet is 734,830.
- Total area in square footage of easements dedicated to the public or the City that is being vacated this plat. Total area in square feet is 29,010.
- No permanent structures or landscape shall be permitted within the dedicated twenty-foot (20') public access easement.
- The revised public trail easement and walk shall be maintained by the HOA or developer until such time as the City of Loveland or Larimer County determine the trail should be open and converted to a full public trail.

This preliminary plat of Giuliano Fourth Subdivision is hereby approved by the Planning Commission of the City of Loveland, Larimer County, Colorado, this \_\_\_\_\_ day of \_\_\_\_\_, 2015, for the preliminary conveyance to the city of the public dedications shown hereon, which are accepted; subject to the provision that approval in no way constitutes a final plat.

\_\_\_\_\_  
PLANNING COMMISSION CHAIR



STEWART & ASSOCIATES

CIVIL ENGINEERS AND LAND SURVEYORS

P.O. BOX 429, FORT COLLINS, COLORADO 80522  
PHONE: (970)462-9331, EMAIL: stewarts@trf.com

P.E. / L.S.  
J.S.R. 1

DESIGNED:

DRAWN:

J.S.R. 1

CHECKED:

SURVEY:

D.H.K.

SCALE:

REVISIONS

NO.	DATE	BY	REMARKS

CLIENT: GIULIANO & FATHER CONSTRUCTION  
308 COMMERCE DRIVE No. 4, FORT COLLINS, CO 80524

PROJECT: PORTION OF GIULIANO FIRST SUBDIVISION  
CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO

TYPE: SUBDIVISION PLAT

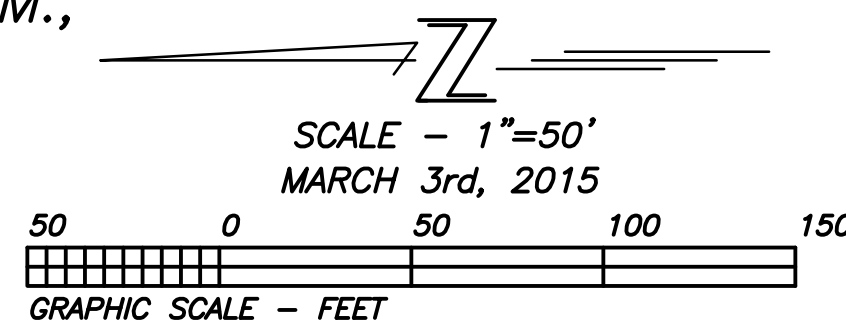
JOB NUMBER 04-PLAT1 Subd plat disk #10	DATE 03/03/15 PLAT No.
---	------------------------------

SHEET NUMBER  
ONE OF TWO



# GIULIANO FOURTH SUBDIVISION

BEING A SUBDIVISION OF BLOCKS 16 & 17, TRACTS S & U, AND OUTLOT 1 OF GIULIANO FIRST SUBDIVISION, AS RECORDED AT RECEPTION No. 2004008797  
SITUATE IN THE SOUTH 1/2 OF SECTION 33, TOWNSHIP 6 NORTH, RANGE 69 WEST OF THE SIXTH P.M.,  
CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO



CURVE	RADIUS	LENGTH	DELTA	CHORD	CH. BEARING
C1	300.00'	72.63'	13°52'19"	72.46'	N07°35'27"W
C2	300.00'	72.63'	13°52'19"	72.46'	N07°35'28"W
C3	300.00'	72.63'	13°52'19"	72.46'	N06°16'51"E
C4	37.00'	67.08'	103°52'18"	58.28'	N38°43'10"W
C5	37.00'	58.64'	90°48'07"	52.69'	S43°56'38"W
C6	37.00'	57.60'	89°11'53"	51.96'	S46°03'22"E
C7	325.00'	26.34'	4°38'39"	26.34'	N05°59'37"W
C8	325.00'	52.34'	9°13'41"	52.29'	N09°54'47"W
C9	275.00'	39.03'	8°08'08"	39.01'	N10°27'33"W
C10	275.00'	94.11'	19°36'30"	93.65'	N03°24'46"E
C11	32.00'	11.14'	1°56'54"	11.09'	N23°11'28"E
C12	68.00'	20.81'	1°35'13"	20.78'	N24°22'19"E
C13	68.00'	50.76'	42°46'00"	49.59'	N05°48'17"W
C14	68.00'	44.90'	37°50'07"	44.03'	N46°09'20"W
C15	68.00'	44.75'	37°42'24"	43.99'	N85°53'35"W
C16	68.00'	9.35'	7°52'28"	9.34'	S73°19'59"W
C17	32.00'	11.14'	1°56'54"	11.09'	S79°22'13"E
C18	32.00'	11.14'	1°56'54"	11.09'	N80°40'51"W
C19	68.00'	108.04'	91°02'01"	97.03'	S63°46'36"W
C20	68.00'	47.08'	39°39'54"	46.14'	S01°54'22"E
C21	32.00'	11.14'	1°56'54"	11.09'	S11°28'52"E
C22	12.00'	19.02'	90°48'07"	17.09'	S43°56'38"W
C23	12.00'	18.68'	89°11'53"	16.85'	S46°03'22"E
C24	32.00'	11.14'	1°56'54"	11.09'	S08°31'02"W
C25	68.00'	50.00'	42°07'46"	48.88'	S02°54'24"E
C26	68.00'	103.21'	86°57'56"	93.59'	S67°07'15"E
C27	32.00'	11.14'	1°56'54"	11.09'	N79°22'15"E
C28	12.00'	18.85'	90°00'00"	16.99'	S45°39'18"E
C29	12.00'	18.85'	90°00'00"	16.99'	S44°20'42"W
C30	12.00'	18.85'	90°00'00"	16.99'	S44°20'42"W
C31	12.00'	18.85'	89°11'53"	16.85'	N46°03'22"W
C32	12.00'	19.02'	90°48'07"	17.09'	N43°56'38"E
C33	12.00'	17.53'	83°41'48"	16.01'	S48°48'24"E
C34	325.00'	26.34'	4°38'39"	26.34'	S10°44'33"E
C35	275.00'	66.58'	13°52'19"	66.42'	S07°35'27"E
C36	12.00'	17.53'	83°41'48"	16.01'	S47°28'48"W
C37	12.00'	18.68'	89°11'53"	16.85'	N46°03'22"W
C38	12.00'	19.02'	90°48'07"	17.09'	N43°56'38"E
C39	12.00'	21.76'	103°52'19"	18.90'	S38°43'12"E
C40	325.00'	42.93'	7°34'07"	42.90'	S09°25'57"W

## SYMBOL LEGEND:

⊙ - Indicates set 1/2" rebar with orange plastic cap PLS No. 37899 unless otherwise noted.

△ - Indicates government section control corner, marked as shown hereon.

○ - Indicates centerline point, lot corner, point of curvature, point of tangent, or tract corner. No monument set at these locations at the time of this plat.

STEWART & ASSOCIATES  
CIVIL ENGINEERS AND LAND SURVEYORS  
P.O. BOX 429, FORT COLLINS, COLORADO 80522  
PHONE: (970)462-9331, EMAIL: stewart@stfr.com

CLIENT: GIULIANO & FATHER CONSTRUCTION  
308 COMMERCE DRIVE No. A, FORT COLLINS, CO 80524  
PROJECT: PORTION OF GIULIANO FIRST SUBDIVISION  
CITY OF LOVELAND, COUNTY OF LARIMER, STATE OF COLORADO  
TYPE: SUBDIVISION PLAT

JOB NUMBER: 04-PLAT2  
DATE: 03/03/15  
SUBDIVISION: Subdiv plot  
PLAT No.

SHEET NUMBER  
TWO OF TWO

According to Colorado law, you must commence any legal action based upon any defect in this survey within three years after you first discover such defect. In no event, may any action based upon any defect in this survey be commenced more than ten years from the date of the certification shown hereon.



# **Draft Proposal for the Processing of Thompson Schools site plans**

## **Introduction:**

This document outlines a working draft proposal for the City's review of site plans for new Thompson School District schools located within Loveland, including charter schools. The proposal reflects a shared interest of the Development Services Department and School District representatives in developing an agreed upon review process. While this effort is still in a preliminary stage, Planning staff and District representatives are seeking the input of the Planning Commission. At this juncture, nothing contained herein should be construed to represent a commitment by either the City or the District.

Because the School District is a state government agency, the relationship between the City and the District is unlike the relationship the City has with private developers; specifically, the City does not have the full review and approval authority for development and building projects. Given this situation, it is beneficial for the City and the District enter into an agreement that clarifies the review process with the respect to school facilities.

## **Purpose:**

The intent of this proposal is to outline a clear, predictable and efficient process that is understood and agreed to by City staff, the Planning Commission, School District officials and their project teams. These shared expectations will enable projects to move smoothly through the City review process.

Once an agreement regarding the review process is reached, a memorandum of understanding (or similar document) can be completed to formalize the agreement. In addition the review of site plans for new schools, the formalized agreement will need to address the review process for other school-related projects including:

- site or building improvements to existing District sites that impact city street, utility, stormwater services
- non-school uses like telecommunications facilities to be located on school sites
- signage requests from the School District

Other projects like those listed above would not be subject to the full list of procedures specified herein as such projects tend to be more limited in scope and require much less processing and coordination.

## **Goals:**

1. Create a clear and repeatable process that has been agreed to by both parties
2. Maintain open communication and collaboration as projects undergo review
3. Bring projects to the Planning Commission as quickly as possible
4. Move projects through the review process efficiently to conclusion
5. Assure efficient and coherent interaction between the City and the District for new construction planning and permit approvals.



## Draft Proposal for the Processing of Thompson Schools site plans

6. Differentiate the District from private developers by identifying the appropriate codes that are applicable to school construction under State law.
7. Create a replicable model for use in all communities served by the District with the recognition that the District interacts with five (5) cities and three (3) counties. Procedures established here should be developed with an eye for compatibility with State laws and assure usability with other agencies in the area.
8. Maximize efficiency in school construction planning to focus tax-payer funds toward the completion of excellent public schools.

### Strategies:

1. Utilize the City's existing review process with adjustments that facilitate expeditious review.
2. Collect the review processes of all five (5) cities and three (3) counties to identify common methodologies.
3. Designate primary points of contact for both project teams, ensuring that all communications flow through these individuals.
4. Alert the applicant (School District) to project-related issues as early as possible, including any possible Planning Commission concerns.
5. Conduct collaborative meetings as needed before and during the review process to achieve project clarity and progress.
6. Assure that City department comments are clearly delineated between what is required by applicable code and what is requested or preferred so as to avoid unnecessary tax-payer expense.
7. Assure that City department comments are coordinated assuring that adjustments made in response to one department will not invalidate comments from another department.
8. Revise District policies to require inclusion of City planning staff in District citizen oversight committees in an ex-officio capacity.
9. All City department feedback must include written statements for required changes and may be supplemented with redline drawings to help clarity.
10. Each District submittal after receipt of City comments must include written responses to each itemized statement from the City departments detailing the changes made.
11. Each round of feedback must be additive to the same document to track comments, requests, and revisions.

### Submittal Components:

1. **Site plan** meeting agreed upon submittal content
2. **Public Improvement Construction Plans (PICPs)** specifying utility, street and stormwater design meeting agreed upon submittal content
3. **Subdivision plats**, easements and other associated applications/materials will be submitted as applicable



## Draft Proposal for the Processing of Thompson Schools site plans

### Review Process:

1. **School Construction Checklist** - develop a checklist of applicable codes so as to clearly differentiate school construction from private development. Identify the relevant codes as "required" when provided for by State law, "preferred" when codes are not required but still valuable to the City and, "not applicable" for any codes that the City does not wish to be included in the planning process. The District will comply with required codes and make reasonable effort to adopt guidance from preferred City codes so long as student safety is not compromised, educational programming needs are not sacrificed, or undue tax-payer expense is not required.
2. **Master Planning** - the City and the District shall assure that staff representatives from both agencies are always included as active participants in master planning activities and shall have standing membership on planning committees engaged with capital planning. This mutual representation agreement shall assure that neither the City's nor the District's planning processes will proceed without mutual awareness and participation of both agencies.
3. **Significant Development** - when the Planning Commission approves private development of significant property that will require the District to construct a school to meet the educational needs of the future child residents, the Planning Commission will hold a discussion with the senior Operations official from the District and the senior Public Works official from the City to outline school construction needs. The District or the City may request such a discussion in the event that rapid growth is detected, which may call for school construction.
4. **School Planning initiation** - when the Board of Education commissions an architect to begin designing a new school, the City Planning office shall be alerted and designate a representative to participate in the design charrette process. The planner shall assist the District's contracted architect in identifying the relevant codes applicable to school construction and provide guidance on how City departments interpret them.
5. **Concept Review**—use this pre-application process to clarify procedures and to identify design issues and challenges early in the process. The District will provide an electronic copy of the Design Document at this stage. A meeting will be held by the Current Planning Manager to assemble and normalize the comments from the departments in writing. Schedule a Planning Commission meeting for presentations on the District's concept and the City's advisement.
6. **Department interactions** - the assigned City planner shall be involved in any interactions between the District's contracted architect and any City departments to document feedback and help assure coherence.
7. **Pre-Submittal Meeting**—conduct a pre-submittal meeting with City development review staff and the school's consultant team to review submittal requirements and to clarify review process expectations. . The District will provide a copy of the 50% Construction Document at this stage.
8. **Submittal** - conduct a submittal meeting with the assigned City planner to review completeness of District materials and to orient City department representatives on the applicable codes involved with school construction. The City planner will assure that comments related to codes not applicable to school construction are clearly identified as advisory to the District. The District will provide the completed Construction Document along with required reports from the school site development checklist.



## Draft Proposal for the Processing of Thompson Schools site plans

9. **Submittal presentation** —upon acceptance of a complete electronic submittal, schedule the Planning Commission meeting in order to facilitate progress.
10. **Review process**—conduct reviews in an efficient timeframe (2 weeks when possible) and conduct a review meeting following each round of comments to ensure clarity; target 2 rounds of review for completion. Through this process, staff reviewers will provide clear and internally-coordinated feedback to the applicant’s team. Reviewers will not “revisit” design comments or revise design directions unless necessary due to redesign by the applicant.
11. **Completion / Approval** —Once the PICPs are complete, all safety-related design provisions have been specified, applicable easements have been recorded, and the site plan is consistent with these plans, then the project is ready to be finalized. The final documents, as reflected on the signature blocks, will clearly indicate which departments have approval authority and which departments are “commenting departments only.”
12. **Follow-up with the Planning Commission**—At an appropriate point following the Planning Commission’s review of a project, it is useful to have the School District provide an update to the Commission project progress. This process helps to maintain a positive working relationship.

### Planning Commission Review:

1. The Planning Commission meeting to review the site plan will be scheduled once a complete submittal has been made to the City. The Commission will not review PICPs.
2. Staff will work to identify Planning Commission interests/concerns prior to the meeting in order to avoid surprise issues and provide thorough information.
3. The two teams will work to provide the Commission with a clear understanding of the project and to gain the Commission’s support.
4. The senior Operations official from the District and the senior Public Works official from the City shall appear together to receive the comments from the Planning Commission.

### No Change in Authority:

Nothing contained in this process shall be construed to limit or otherwise change the authority of the Board of Education nor to extend the authority of any agency, committee or body beyond what is provided for by State law concerning school construction.